

Agenda

Strathfield Local Planning Panel Meeting

Notice is hereby given that a Strathfield Local Planning Panel Meeting will be held at Council Town Hall (Supper Room), 65 Homebush Road, Strathfield on:

Thursday, 4 October 2018

Commencing at 10:00am for the purpose of considering items included on the Agenda

Persons in the gallery are advised that the proceedings of the meeting are being recorded for the purpose of ensuring the accuracy of the Minutes. However, under the Local Government Act 1993, no other tape recording is permitted without the authority of the Council or Committee. Tape recording includes a video camera and any electronic device capable of recording speech.

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TO: Strathfield Local Planning Panel Meeting - 4 October 2018
REPORT: SLPP – Report No. 1
SUBJECT: PLANNING PROPOSAL - NO 2-6 PILGRIM AVENUE, 9 ALBERT ROAD AND 11-13 ALBERT ROAD, STRATHFIELD
 TO AMEND THE HEIGHT OF BUILDINGS AND FSR IN STRATHFIELD LEP 2012
DA NO. N/A

PURPOSE

Summary of the Planning Proposal

Table 1 below provides a summary of the planning proposal, and its revisions. The subject site has an existing height of 35m and FSR of 3.5:1 under Strathfield LEP 2012:

Table 1: Summary of Planning Proposal Revisions

	Date of Lodgement	Proposed Height	Proposed FSR
Planning Proposal (as originally lodged)	July 2015	70m	8.3:1
Planning Proposal – Revision 1	March 2016	60m	6.1:1
Planning Proposal – Revision 2	September 2017	54m	5:1

Table 2 below provides a chronological overview of the Planning Proposal. Further detail is provided in the body of the report.

Table 2: Chronological Overview of the Planning Proposal

Date	Details
July 2015	Planning Proposal lodged with Strathfield Council. The Planning Proposal sought to amend Strathfield LEP 2012 as follows: Height – from 35m to 70m FSR – from 3.5:1 to 8.3:1 This is known as the original planning proposal. All other versions are referred to as Revisions.
March 2016	A revision to the Planning Proposal was submitted to Strathfield Council. Revision 1 sought to amend Strathfield LEP 2012 as follows: Height – from 35m to 60m FSR – from 3.5:1 to 6.1:1
6 June 2016	Revision 1 submitted to the Department of Planning & Environment (DPE) as a Pre-Gateway Review due to Council not indicating its support within 90 days.
21 June 2016	Report to Council on the original planning proposal

PLANNING PROPOSAL - No 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield
To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)

Date	Details
	<p>The report to Council was based on the original Planning Proposal scheme of 8.3:1 FSR and 70 metres</p> <p>Council resolved the following (Minute No 187/16):</p> <ol style="list-style-type: none"> 1. <i>That Council not support the Planning Proposal for 2-6 Pilgrim Avenue & 11-13 Albert Road Strathfield relating to 8, 12 and 18 storey building heights and 6.1:1 FSR, proceeding to the Department of Planning & Environment's Gateway assessment process for the reasons outlined in this report.</i> 2. <i>That Council endorse amending the Planning Proposal with maximum building heights of 8, 11 and 14 storeys and 4.5:1 FSR consistent with David Lock & Associates' Urban Design Review.</i> 3. <i>That Council Officers explore the potential to value capture from the Planning Proposal prior to the matter being notified to the Department of Planning and Environment.</i> 4. <i>That the Department of Planning & Environment be notified accordingly</i>
18 October 2016	<p>The Sydney East Joint Regional Planning Panel (JRPP) considered that the proposal had merit and recommended it should proceed to Gateway Determination subject to the following:</p> <ul style="list-style-type: none"> ▪ The proposal should extend over the whole street block including the adjoining service station; ▪ The permissible FSR over the whole street block should be 5:1; ▪ The maximum permissible height should be 54m; ▪ A development control plan should be prepared and exhibited with the draft LEP showing the proposed distribution of height. That part of the site which adjoins the residential area should have the lowest height, while the part which is closest to the station should have the highest; ▪ A new traffic study should be prepared which applies to the whole street block which takes into account any public commuter parking provided; and ▪ Given that the planning proposal does not include any changes in permissible use, land contamination and hydraulic studies may be left to the development assessment stage.
July 2016	Formal comments were provided to the DPE outlining Council's position in relation to the Planning Proposal
20 July 2017	Council formally accepts the Relevant Planning Authority (RPA) role for the Planning Proposal
September 2017	<p>Proponent submits an updated Planning Proposal (Revision 2) which seeks to amend Strathfield LEP 2012 as follows:</p> <p>Height – from 35m to 54m</p>

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To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)**

Date	Details
	FSR – from 3.5:1 to 5:1 This is in accordance with the recommendation of the JRPP
11 September 2017	DPE submits the Planning Proposal (Revision 2) for a Gateway Determination. This is the Planning Proposal that has been assessed as part of this report.
6 November 2017	Gateway Determination issued for the Planning Proposal (Revision 2). The Gateway Determination included a number of conditions which had to be addressed prior to exhibition of the Planning Proposal The Gateway Determination requires Council to finalise the amending LEP by 6 November 2018
31 August 2017	Council forwards the Planning Proposal for a Gateway Determination. Council raised a number of concerns
February 2018	Amended Planning Proposal submitted to the Department of Planning & Environment satisfying the conditions of the Gateway Determination
8 March 2018	DPE advises Council of compliance of the Planning Proposal with the Gateway conditions
1 May – 1 June 2018	Public exhibition of the Planning Proposal

Background

This report is based on Revision 2 which was lodged in September 2017 and proposes a height of 54m and a FSR of 5:1, as recommended by the Sydney East Joint Regional Planning Panel in October 2016

The original Planning Proposal was lodged in July 2015 and provided an indicative concept proposal incorporating three (3) buildings ranging in height from 10 to 22 storeys with a north-east facing internal courtyard and commercial frontage along Pilgrim Avenue and Albert Road, and the request to:

- Increase the maximum building height from 35m to 70m; and
- Increase the maximum FSR from 3.5:1 to 8.3:1;

Council sought independent urban design advice from *David Lock & Associates* (DLA) to conduct a peer review of the proposal lodged in July 2015 and to advise on what was considered to be an appropriate FSR and height for the site as part of undertaking a preliminary assessment of the proposal.

In December 2015, staff met with the Proponent to provide preliminary feedback and to present DLA's recommendations. The proponent was invited to submit a revised scheme in accordance with DLA's suggested changes.

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In March 2016, a revised proposal (Revision 1) was submitted to Council. This amendment proposed to:

- Increase the maximum building height from 35m to 60m (approximately 11 to 22 storeys); and
- Increase the maximum FSR from 3.5:1 to 6.1:1

Council considered a report on 21 June 2016. The report to Council was based on the original Planning Proposal scheme of 8.3:1 FSR and 70 metres in height and not Planning Proposal – Revision 1.

As a result, Council at its meeting on 21 June 2016 resolved the following (Minute No 187/16):

1. *That Council not support the Planning Proposal for 2-6 Pilgrim Avenue & 11-13 Albert Road Strathfield relating to 8, 12 and 18 storey building heights and 6.1:1 FSR, proceeding to the Department of Planning & Environment's Gateway assessment process for the reasons outlined in this report.*
2. *That Council endorse amending the Planning Proposal with maximum building heights of 8, 11 and 14 storeys and 4.5:1 FSR consistent with David Lock & Associates' Urban Design Review.*
3. *That Council Officers explore the potential to value capture from the Planning Proposal prior to the matter being notified to the Department of Planning and Environment.*
4. *That the Department of Planning & Environment be notified accordingly.*

Pre-Gateway Review

On 6 June 2016, the Planning Proposal (Revision 1) was submitted as a Pre-Gateway Review due to Council not indicating its support within 90 days.

On 18 October 2016, the Sydney East Joint Regional Planning Panel considered that the proposal had merit and recommended it should proceed to Gateway Determination subject to the following:

- The proposal should extend over the whole street block including the adjoining service station;
- The permissible FSR over the whole street block should be 5:1;
- The maximum permissible height should be 54m;
- A development control plan should be prepared and exhibited with the draft LEP showing the proposed distribution of height. That part of the site which adjoins the residential area should have the lowest height, while the part which is closest to the station should have the highest;
- A new traffic study should be prepared which applies to the whole street block which takes into account any public commuter parking provided; and
- Given that the planning proposal does not include any changes in permissible use, land contamination and hydraulic studies may be left to the development assessment stage.

Having regard to the Panel's advice, The Department of Planning & Environment (DPE) determined that the proposal should proceed to Gateway subject to conditions as suggested by the Panel. The DPE considered it appropriate that the Planning Proposal be amended to include the adjacent service station site to ensure a consistent urban design outcome across the whole block. The proposed amendments to the height and FSR were also considered by DPE to be a suitable response in relation to the entire block and ensure the transition of the controls to the adjacent residential locality.

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To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)**

An amended Planning Proposal (Revision 2) was submitted to the DPE which proposed to amend Strathfield LEP 2012 as follows:

- Increase the maximum building height from 35m to 54m; and
- Increase the maximum FSR from 3.5:1 to 5:1

A concept design was provided with the proposal and comprised the following:

- Building form across the site comprising a podium structure with two (2) residential towers of 11 and 15 storeys primarily aligned to the Pilgrim Avenue and Raw Square frontages;
- A central lower rise podium level comprising communal open space and landscaping;
- Ground floor commercial/retail tenancies; and
- Basement level car parking accessed from Pilgrim Avenue and Raw Square.

Component	Details
Height	Building A – 12 storeys (44m) Building B – 10 storeys (37m) Building C – 15 storeys (54m) Building D – 10 storeys (37m)
Residential apartments	280 dwellings
Commercial floor space	1,358sqm comprising 8 separate ground floor commercial tenancies



Figure 1: Proposed indicative design viewed facing north-east (Source: JBA)

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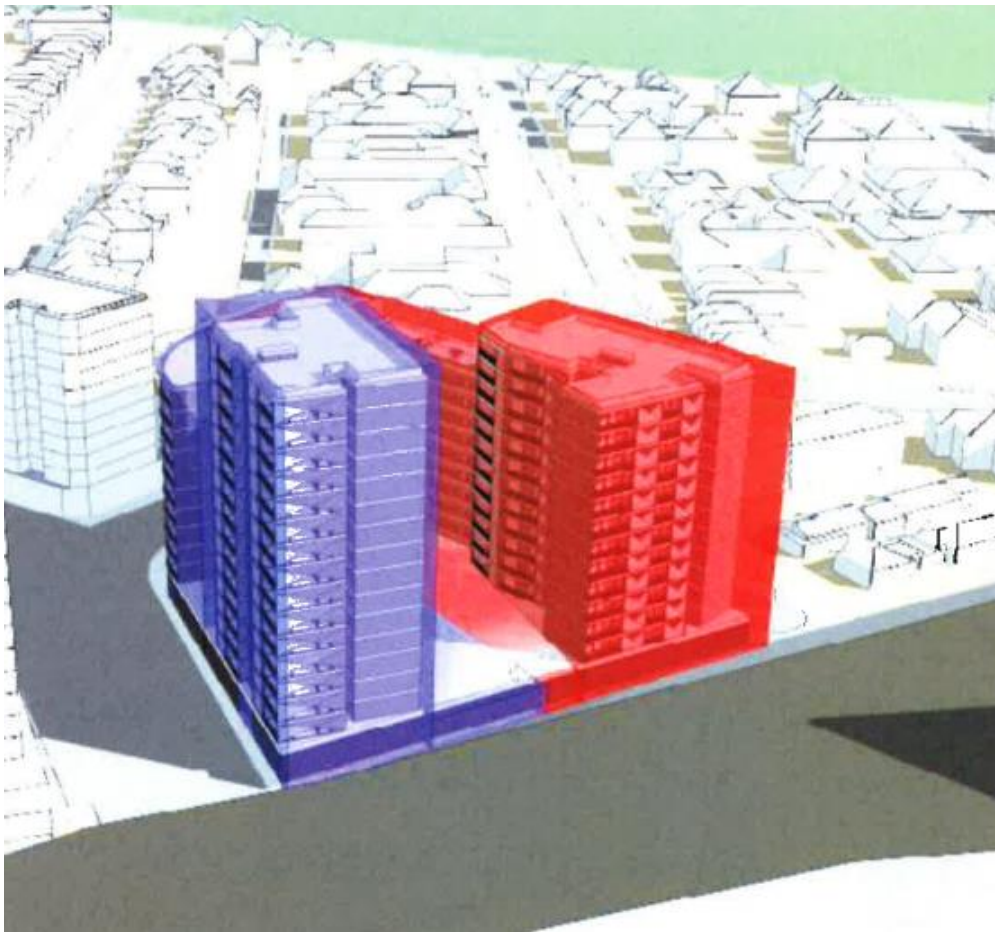


Figure 2: Proposed indicative design facing south (Source: JBA)

Gateway Determination

A Gateway Determination was issued on 6 November 2017 for the Planning Proposal (Revision 2). The Gateway Determination included a number of conditions, including the following conditions:

1. *Prior to community consultation, the Planning Proposal is to be updated and amended to:*
 - (a) *provide a project timeline, outlining the anticipated timeframes for the plan making process;*
 - (b) *reference and address all relevant priorities and actions in the draft Greater Sydney Region Plan;*
 - (c) *reference and address all relevant priorities and actions outlined in the draft Eastern City District Plan;*
 - (d) *provide updated overshadowing diagrams to state the correct date of the analysis (being the winter solstice at 21 June) and also show the overshadowing impacts of the existing built form;*
 - (e) *provide updated analysis of solar access and cross ventilation to the indicative design for the existing service station site at 9 Albert Road;*
 - (f) *address and justify the inconsistency with Section 117 Direction 4.1 Acid Sulphate Soils as the subject site is identified as being on Class 5 land in the*

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Strathfield LEP2012;

- (g) address and justify the inconsistency with Section 117 Direction 4.3 Flood Prone Land. In this regard, a flood study is to be provided to demonstrate the suitability of the site for redevelopment;*
- (h) provide an updated traffic and transport assessment to address the cumulative effect of traffic generation on the intersection at Raw Square and Everton Road;*
- (i) provide a preliminary contamination assessment of the extended site to demonstrate the suitability of the site for development;*
- (j) provide an acoustic report to demonstrate the suitability of the site for redevelopment in light of its proximity to the adjacent railway line and road network. The report should also consider the noise impacts of the operation of the existing service station should this use remain whilst the properties at 2-6 Pilgrim Avenue and 11-13 Albert Road are redeveloped*
- (k) provide an air quality report to demonstrate the impacts of the existing service station should it not be redeveloped; and*
- (l) prepare a site specific DCP reflecting the distribution of height across the entire site. This must ensure that the site which adjoins the residential area should have the lowest, while the part which is closest to the station should have the highest.*

The planning proposal is to be amended to include any relevant findings of the required studies and the amended proposal is to be forwarded to the Department for review and endorsement prior to community consultation.

The Gateway Determination required Council to finalise the amending LEP by 6 November 2018. A copy of the Gateway Determination is included at **Attachment 1**.

A copy of the Planning Proposal document submitted in response to the Gateway Determination conditions is included at **Attachment 2**

Relevant Planning Authority (RPA) and Council's Concerns

Following the Panel's recommendation, Council was invited to be the Relevant Planning Authority (RPA). At the time, Council raised a number of concerns with the proposal. Following discussions between Council and DPE, Council formally accepted the RPA role for the proposal on 20 July 2017.

Council, on 31 August 2017 forwarded the Planning Proposal to the DPE for a Gateway Determination. In its correspondence, Council also raised a number of concerns with respect to the Planning Proposal, including:

- Deferral of the Planning Proposal until such time as the Planned Precinct (formerly Priority Precinct) work for the Strathfield, Burwood and Homebush Precinct was completed so as to understand the opportunities and cumulative impacts of the proposed development;
- Concern with the Panel's recommendation to extend the planning proposal over the whole street block to include the service station site for which there is no owner's consent

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- Concern with the proposed density for the site which was inconsistent with the recommendations of a peer review undertaken by an independent consultant which recommended 14 storeys and an overall FSR of 4.5:1
- Adverse impacts from the proposed development on the amenity (privacy and solar access) and future compliance of the development with the ADG
- Lack of provision of commercial floor space in the overall development
- Site specific DCP and revised masterplan for the consolidated site to provide detailed planning and design guidelines
- The preparation of additional studies, including a:
 - Detailed flood study
 - Detailed contamination report
 - Acid sulphate soils study
 - Acoustic report
 - Detailed traffic assessment
- Provision of public benefit

A copy of Council's letter is included at **Attachment 3**.

In its report to the Panel, the DPE considered Council's concerns and provided responses to each of the points raised. Table 3 below provides a summary of the DPE's response:

Table 3 – Response by DPE in relation to Council's Concerns

Council Issue	DPE Response
Deferral of Planning Proposal	Not in keeping with the Panel's recommendation of 18 October 2016 and is not supported.
Owner's consent for the inclusion of the service station site	<p>No requirement under the EP&A Act that requires owner's consent to be part of a planning proposal.</p> <p>A Gateway condition has been included requiring that the proponent write to the landowners during the consultation period of the Planning Proposal.</p>
Density	<p>Proposed density has been reduced to 5:1</p> <p>Council's request for a range of feasible options to be developed for heights and densities is not supported. The updated concept design has responded to the distribution of heights recommended by the Panel to provide the highest buildings closest to the railway station which is considered acceptable.</p> <p>The preparation of a site specific DCP will provide the basis for the distribution of height and FSR across the site.</p>
Amenity impacts	Conditions of Gateway have been included requiring that updates be made to the proposal to address amenity concerns regarding solar access and ventilation and its response to the ADG.

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Council Issue	DPE Response
Inadequate commercial floor space	<p>The proposal does not seek to rezone the site from the existing B4 – Mixed Use zone. Council's concerns for additional commercial floor space will need to be considered further as part of any future development application.</p> <p>Clause 6.8 of the Strathfield LEP 2012 restricts the amount of residential floor space (should not exceed 35% of the FSR of the building) in the adjacent Strathfield Town Centre where a building contains more than 1,500sqm of floor space. The subject site is outside of the Strathfield Town Centre.</p>
Site specific DCP	Condition (l) of the Gateway Determination requires the preparation of a site specific DCP.
Additional Studies	<p>The Gateway Determination conditions require the proponent to:</p> <ul style="list-style-type: none"> - Address s117 Direction 4.1 – Acid Sulphate Soils; - Address s117 Direction 4.3 – Flood Prone Land and submit a flood study - Provide an updated traffic and transport assessment of the extended site - Provide an acoustic report to demonstrate the suitability of the site in light of its proximity to the adjacent railway line and road network - Provide an air quality report to demonstrate the impacts of the existing service station site

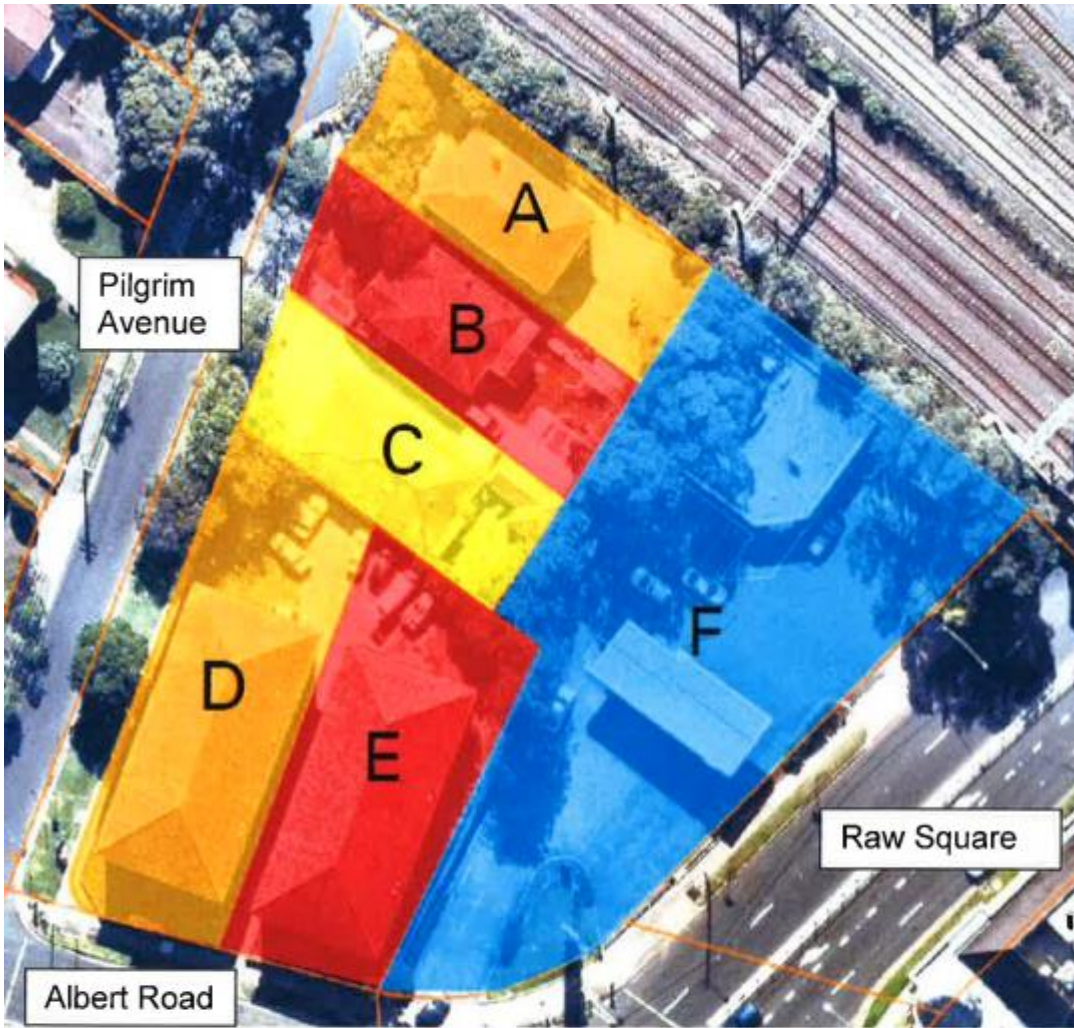
The Site and Locality

The site comprises six lots, is irregular in shape and has a total area of approximately 4,885sqm.

Three of the lots (Sites A-C) have a sole frontage to Pilgrim Avenue and contain detached single storey residential dwellings with separate driveway access. Two of the lots (Sites D and E) contain separate residential flat buildings with a central driveway access from Albert Road. A service station is located on the remaining lot (Site F) on the corner of Raw Square and Albert Road (Figure 3)

The site is largely covered with concrete and asphalt with a low level of vegetation. Street trees are located along the Pilgrim Street verge adjacent to the north-wester side of the site. This land is not part of the site and is identified as being owned by Railcorp. The site does not contain any heritage items and is not located within a heritage conservation area. The closest heritage conservation area is located approximately 100m to the south-west of the site.

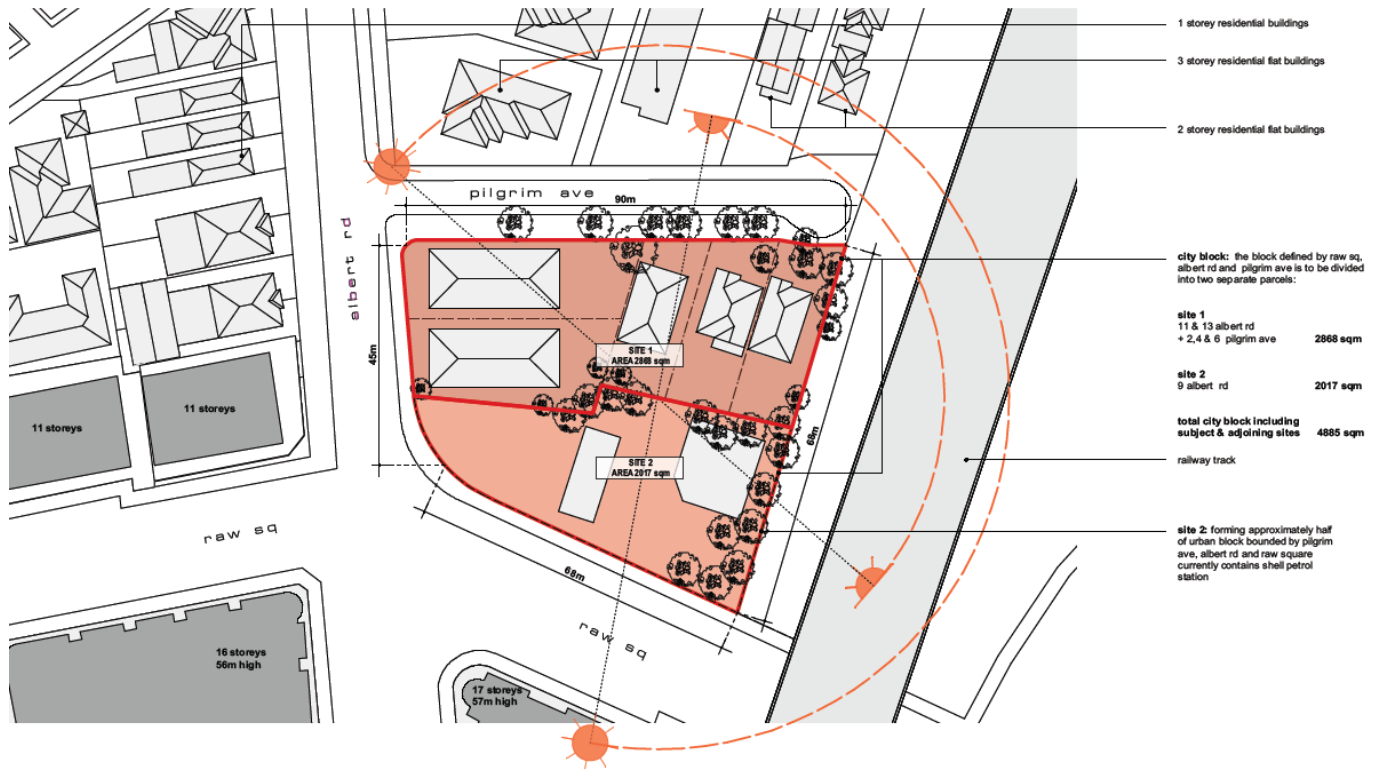
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Reference	Street address	Legal description	Area
Site A (proponent owned)	2 Pilgrim Avenue	SP8785	500 m ²
Site B (proponent owned)	4 Pilgrim Avenue	Lot 9 DP15917	472 m ²
Site C (proponent owned)	6 Pilgrim Avenue	Lot 8 DP15917	433 m ²
Site D (proponent owned)	13 Albert Road	Lot A DP100558	748 m ²
Site E (proponent owned)	11 Albert Road	Lot B DP100558	715 m ²
Site F	9 Albert Road	Lot 100 DP807807	2,017 m ²

Figure 3: Site Plan (Source: JBA)

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Site Plan – Existing context (Source: Kennedy Associates Architects/JBA)

Figure 4 below provides an overview of the surrounding land uses. Further to the east of the Service Station is the Sandalwood Apartments residential tower (approximately 18 storeys high) and the Strathfield Railway Station approximately 200m from the site.

Strathfield Railway Station is a key interchange on the Sydney Trains rail network with a number of key rail services stopping at the station including the T1 North Shore, Northern and Western Line and the T2 Airport, Inner West and South Line provide frequent services from the station into wider Sydney.

Across Albert Road to the immediate south of the site is a mix of single and double storey detached residential dwellings, with a new residential tower development on the corner currently under construction, to be approximately 11 storeys.

The Regal Court mixed use development (approximately 18 storeys high) is located to the site’s south-east and comprises a number of smaller ground level retail shops with residential above. Further to the south-east is the Strathfield Plaza shopping complex and a mix of commercial and residential buildings.

West of the site across Pilgrim Avenue are a number of older style 1970’s residential flat buildings that vary between two to four storeys in height. The higher residential unit blocks generally front Elva Street, and overlook the rail corridor. Further to the west the height of the residential flat buildings along Elva Street increases to approximately 10 storeys.

The site overlooks the rail corridor to the north. Additional residential areas, within the Canada Bay LGA, are located to the north-east, and generally comprise single storey detached dwellings. A light industrial and warehouse precinct is located approximately 400m to the north-west of the site (Refer to Figures 5 – 7).

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- The Site
- Existing Sydney Train Lines (T1, T2)

Figure 4: Subject site and surrounding land uses (Source: JBA)



Figure 5: View east along Pilgrim Avenue

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Figure 6: View north along Pilgrim Avenue



Figure 7: View south along Pilgrim Avenue

Current Planning Controls

The Strathfield LEP 2012 applies to the subject site. The following provisions are relevant to the Planning Proposal, extracts of which are shown in Figures 8 to 10 below:

Land Zoning	The site is zoned B4 Mixed Use under Strathfield LEP 2012. It is not proposed to change the zoning of the land. The current zoning is shown in Figure 8
Height	The site has a current height limit of 35m applied under the Strathfield LEP, as shown in Figure 9.
FSR	The site has a maximum floor space ratio of 3.5:1 applied under the Strathfield LEP, as shown in Figure 10.

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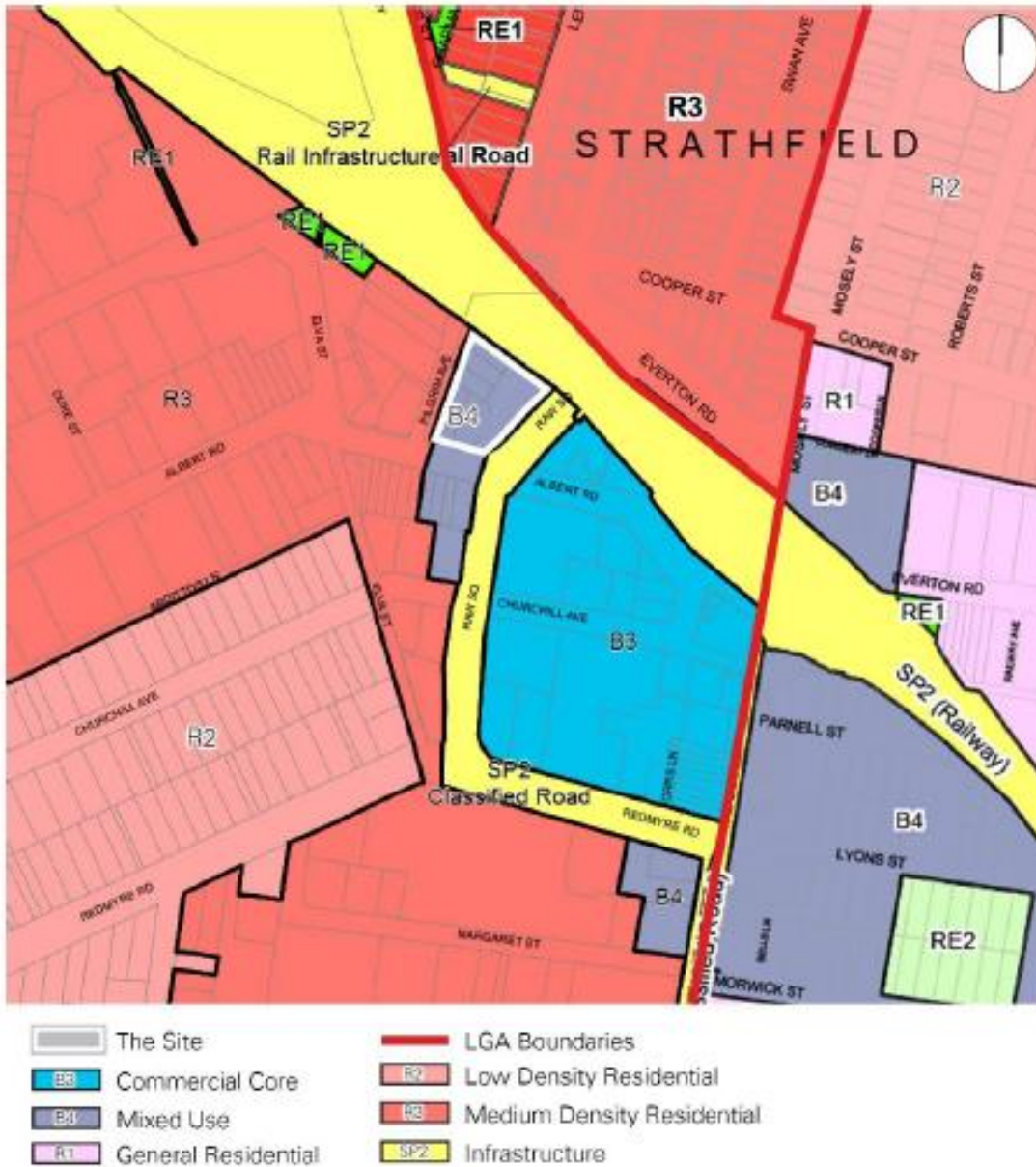


Figure 8: Strathfield LEP Zoning Map (Source: SLEP 2012)

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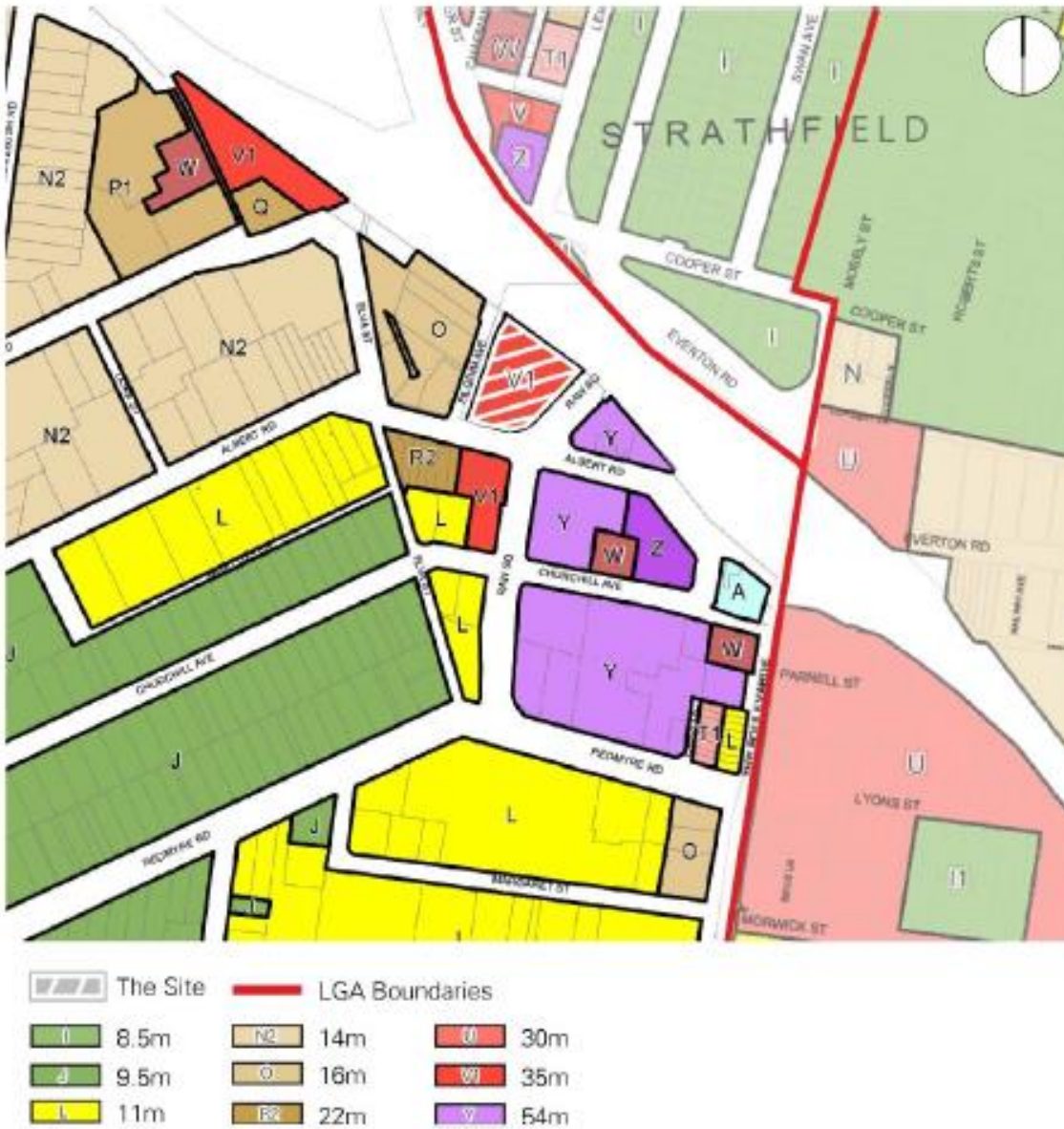


Figure 9: Strathfield LEP Building Height Map (Source: SLEP 2012)

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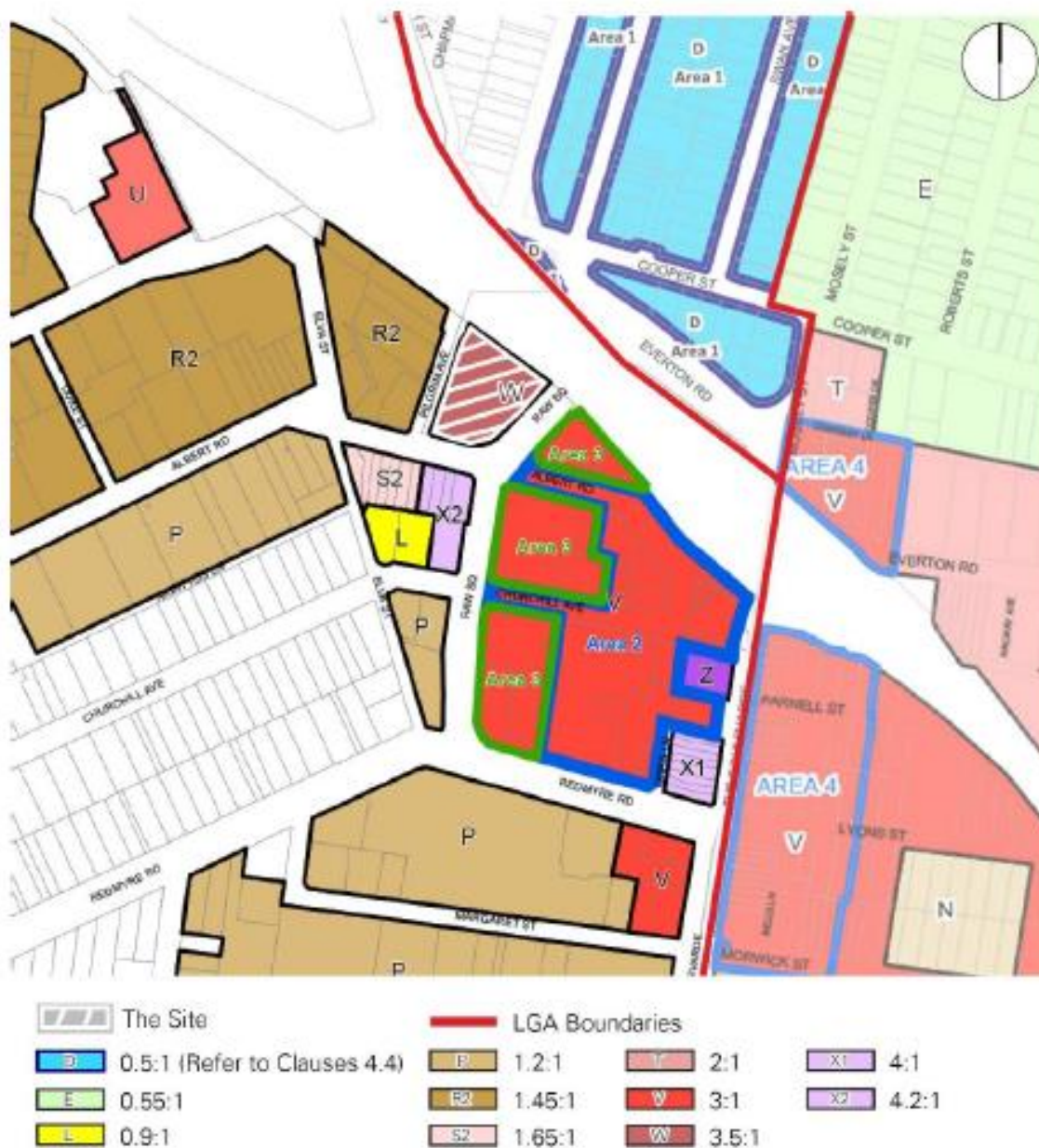


Figure 10: Strathfield LEP FSR Map (Source: SLEP 2012)

Adjoining sites to the east of the subject site have an existing height of 54m and a FSR of between 3:1 – 5:1 if the size of the lot exceeds 1,500sqm (Area 3 - Clause 4.4B of Strathfield LEP 2012)

Description of Current Planning Proposal

In summary, this planning proposal requests an amendment to Strathfield LEP 2012 as outlined in Table 4 below:

**PLANNING PROPOSAL - No 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield
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Table 4: Summary of current controls and controls in the Planning Proposal

Property	Current Controls in SLEP 2012	Planning Proposal
2-6 Pilgrim Avenue – 'Site 1' (1,405sqm)	B4 – Mixed Use 35m (10storeys) 3.5:1 (4,917.5sqm)	B4 – Mixed Use 54m (15 storeys) 5:1 (7,025sqm)
11-13 Albert Road – 'Site 1' (1,463sqm)	B4 – Mixed Use 35m (10storeys) 3.5:1 (5,120.5sqm)	B4 – Mixed Use 54m (15 storeys) 5:1 (7,315sqm)
9 Albert Road – 'Site 2' (2,017sqm)	B4 – Mixed Use 35m (10storeys) 3.5:1 (7,059.5sqm)	B4 – Mixed Use 54m (15 storeys) 5:1 (10,085sqm)

The ownership of five of the lots is with one owner with the adjacent lot (9 Albert Road) owned separately and listed in Table 5 below:

Table 5: Ownership of lots subject to Planning Proposal

Street Address	Owner
2 Pilgrim Avenue, Strathfield	Convertia Pty Ltd
4 Pilgrim Avenue, Strathfield	Convertia Pty Ltd
6 Pilgrim Avenue, Strathfield	Convertia Pty Ltd
9 Albert Road, Strathfield	Ver Custodian P/L
11 Albert Road, Strathfield	Convertia Pty Ltd
13 Albert Road, Strathfield	Convertia Pty Ltd

The proposed amendments to Strathfield LEP 2012 as identified in Table 4 above are shown in Figures 11 & 12 below:

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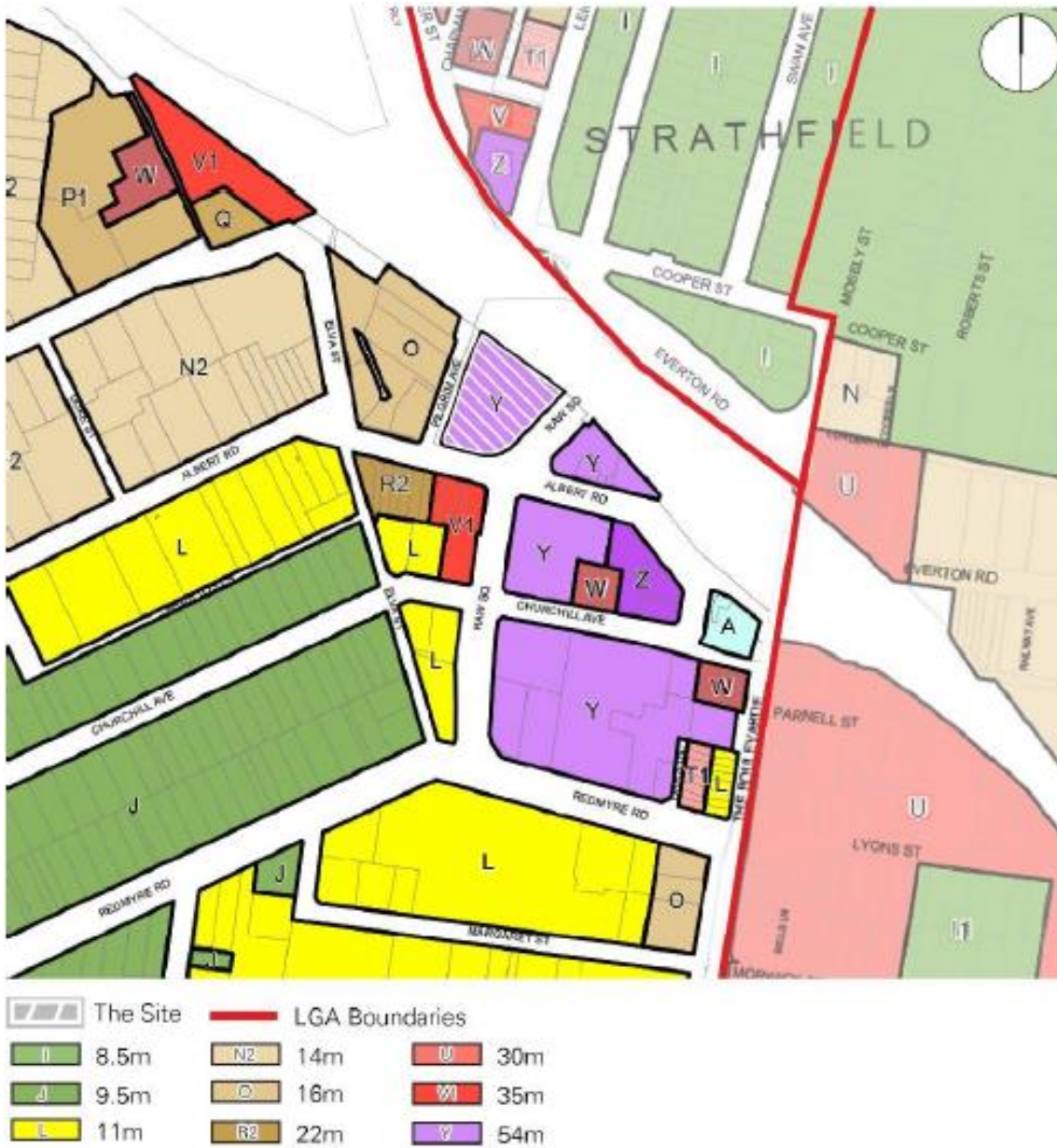


Figure 11: Planning Proposal Maximum Building Heights Map - Proposed

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Figure 12: Planning Proposal Maximum Floor Space Ratio Map - Proposed

This Planning Proposal comprises a block containing two ‘sites’ with a total urban area of approximately 4,885sqm.

‘Site 1’ comprises the amalgamation of 2-6 Pilgrim Avenue and 11-13 Albert Road Strathfield and is currently occupied by single dwellings and residential flat buildings. ‘Site 2’ comprises 9 Albert Road, Strathfield and is currently occupied by a service station and is in separate ownership.

The primary focus of the planning proposal is the future redevelopment of Site 1 however it has been prepared to also illustrate the long term development potential of the entire site.

As part of the Planning Proposal, an indicative scheme was prepared by Kennedy Associates Architects for the site. The indicative scheme demonstrates how the site could be redeveloped in the future under the proposed height and floor space ratio controls, while preserving future

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development rights on the adjacent service station site. A copy of the Urban Design Report is included under separate cover.

The key components of the scheme include:

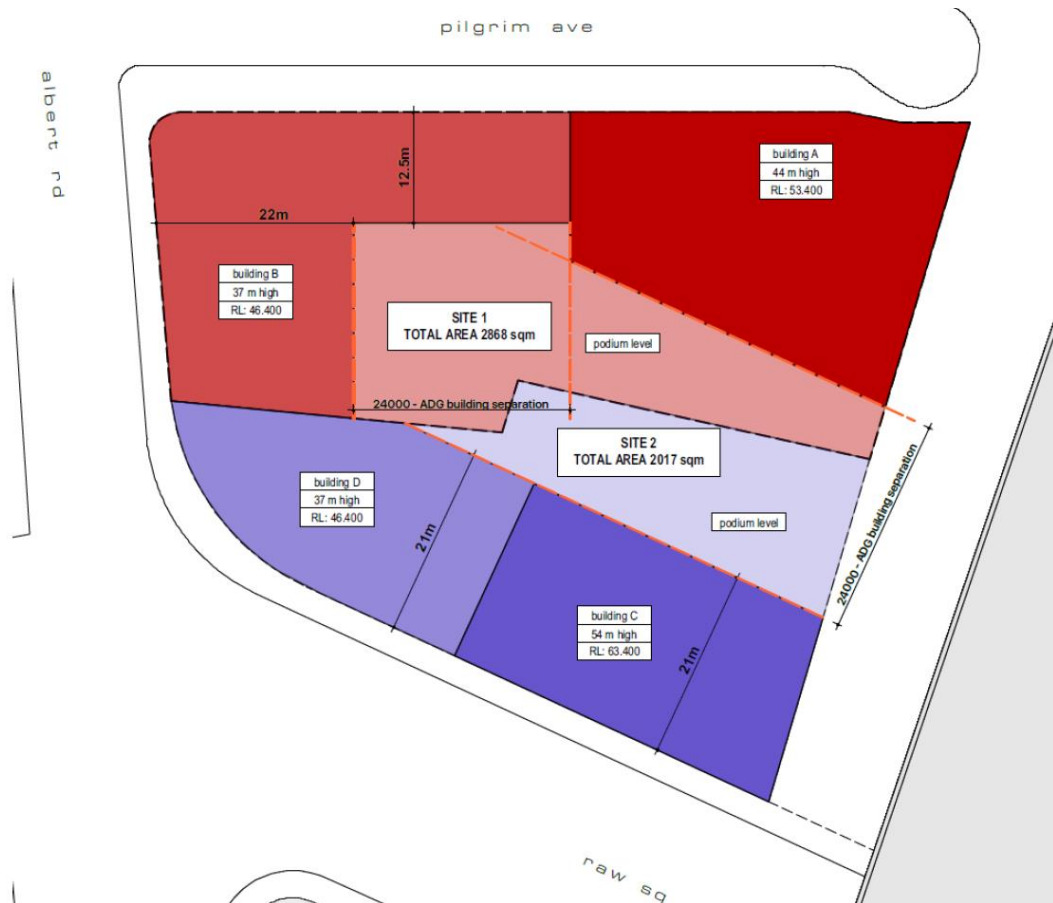
- A predominant street wall height of approximately 10 storeys that wraps around the site along all street frontages;
- Building form across the subject site comprising a podium structure with two residential towers of 11 and 15 storeys primarily aligned to the Pilgrim Avenue and Raw Square frontages and act as northern and southern bookends for the site;
- A central lower rise podium level that houses the primary communal open space area characterised by landscaping and recreational pool facilities;
- Ground floor commercial/retail tenancies; and
- Basement level car parking accessed from Pilgrim Avenue and Raw Square.

The Planning Proposal (Sites 1 & 2) proposes a mixed use development (predominantly residential development) with 24,077sqm of floorspace comprising the following:

Table 6: Indicative Scheme Development Statistics

Massing	Perimeter block massing consisting of a series of interconnected buildings facing Pilgrim Avenue Albert Road & Raw Square Strathfield
Use	Ground Floor – Commercial
	Upper Floors – Residential
Height	Site 1 – 11-13 storeys
	Site 2 – 11-16 storeys
Yield	Site 1 – 155-175 apartments
	Site 2 – 100-120 apartments
Commercial Floor Space	1,358sqm
Gross Floor Area (GFA)	Site 1 – 14,340sqm (approx.)
	Site 2 – 10,085sqm (approx.)
Floor Space Ratio (FSR)	Site 1 – 5:1
	Site 2 – 5:1
Parking	Site 1 – 351 spaces (approx.)
	Site 2 – 228 spaces (approx.)

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Under the current controls (SLEP 2012), the subject site can achieve a total floor space of 17,097.5sqm or an overall FSR of 3.5:1. This Planning Proposal proposes a FSR of 5:1 which generates a floor space of 24,425sqm.

The Planning Proposal is supported by the following documents as outlined in the following Table (Table 7 below)

Table 7: Documentation supporting the Planning Proposal

Report	Prepared by
Architectural Drawings and Urban Design Report	<i>Kennedy Associates Architects</i>
Site Survey Plan	<i>DJ Barrington & Associates</i>
Traffic Impact Assessment	<i>McLaren Traffic Engineers</i>
Acoustic Report	<i>Acoustic Noise and Vibration Solutions</i>
Air Quality Assessment	<i>Todoroski Air Sciences</i>
Powells Creek Flood Study	<i>WMA Water for Strathfield Council</i>
Preliminary Site Investigation	<i>EI Australia</i>

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Assessment of the Planning Proposal

Strategic Planning Context

Consideration of the Planning Proposal request in relation to the *Greater Sydney Region Plan (A Metropolis of Three Cities)*, the *Eastern District Plan*, the State and Regional Statutory Framework and Section 9.1 Directions is provided below:

Greater Sydney Region Plan (A Metropolis of Three Cities)

A Metropolis of Three Cities was finalised and released by the Greater Sydney Commission in March 2018 and established the aspirations for the region over the next 40 years. The Region Plan is framed around 10 directions relating to infrastructure and collaboration, liveability, productivity and sustainability.

The Planning Proposal is considered to be consistent with the following Directions and Objectives of the *Greater Sydney Region Plan*:

Direction 2: A City for People

Objective 6: Services and infrastructure meet communities' changing needs

Objective 7: Communities are healthy, resilient and socially connected

Objective 8: Greater Sydney's communities are culturally rich with diverse neighbourhoods

The Planning Proposal assists in encouraging healthy communities by creating a mixed-use development that provides a convenient focus for daily activities and benefits from its proximity to retail and services and employment opportunities within the Strathfield centre.

The Proposal provides active frontages with commercial and retail uses at ground level, supporting and complementing the adjacent Strathfield Town Centre. The provision of a public car park on-site will also support the local community in visiting both the town centre and act as a commuter car park for users of the nearby rail network.

Direction 3: Housing the City

Objective 10: Greater Housing Supply

Objective 11: Housing is more diverse and affordable

The Planning Proposal will increase housing opportunities within the Strathfield Centre (with an estimated 280 additional residential apartments) within walking distance from Strathfield station and bus interchange.

Housing choice to suit the varying needs of residents will be provided through the development of the 280 new dwellings on the site, with a range of sizes provided. The variety of housing available will provide housing choice to suit the different lifestyles, household sizes and affordability.

Direction 4: A City of Great Places

Objective 12: Great places that bring people together

The site is located on the western edge of the Strathfield town centre which contains a variety of commercial and retail uses. The Planning Proposal anticipates the creation of a quality mixed use

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development with commercial floor space on the ground floor and residential above. Other key considerations of the development include:

- Design Excellence – Promote design excellence through the creation of a sustainable and liveable environment for people through the use of expert panels and active engagement between Council and applicants; that aligns with the vision for Strathfield Centre.
- Transition / Interface – Provide adequate transition compliant with the Apartment design Guide between the new development and the adjoining residential development on Raw Square and Albert Road
- Active Street Frontages – Ground floor frontages along Albert Road, Pilgrim Avenue and Raw Square to be active and pedestrian orientated.

Direction 5: A well connected city

Objective 14: *A Metropolis of Three Cities* – integrated land use and transport creates walkable and 30 minute cities

The Planning Proposal anticipates providing approximately 280 new dwellings in the form of a mix of apartments within 200m of a key transport interchange, contributing to the shift towards public transport. The site is suitable for this increase in dwellings as it is located within the Strathfield Centre, close to job opportunities and public transportation services (T1 North Shore, Northern and Western Line and the T2 Airport, Inner West and South Line, Blue Mountains Line, Central Coast and Newcastle Line and bus interchange) with frequent services.

Strathfield Railway Station has strong connections to greater Sydney, and is strategically located to allow direct access to the Sydney and Parramatta CBDs via both rail and bus services.

Direction 6: Jobs and Skills for the City

Objective 22: Investment and business activity in centres

Objective 23: Industrial and Urban Services land is planned, retained and managed

The ground floor retail and commercial floor space contribution will provide employment floor space within the Strathfield Centre, on a site which benefits from its proximity to the commercial and retail services within Strathfield Centre. This will also assist in delivering more investment and business activity and increased productivity.

Eastern City District Plan

The Eastern City District Plan was finalised and released by the Greater Sydney Commission in March 2018. The District Plan is a guide for implementing a Metropolis of Three Cities at the district level and proposes a 20 year strategic target by setting out aspirations and proposals for the Eastern District. The District Plan provides 18 Planning Priorities under four Directions.

The Eastern City District Plan sets five year dwelling and employment targets which Councils in the Eastern district are expected to accommodate in their LEPs. The Plan specifies a five year housing supply target of 3,650 for Strathfield Council. Council's need to develop 6-10 year housing targets through the preparation of housing strategies.

The proposed height and FSR increases for the site will provide:

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- Employment opportunities within the proposed retail and commercial floor space (1,358sqm approx.)
- Improvements to housing choice and availability in close proximity to public transport and the Strathfield Centre which offers retail and essential services.

The Planning Proposal is considered to be consistent with the following Planning Priorities of the *Eastern City District Plan* as in Table 8 below:

Table 8: Summary of consistency with the priorities and actions of the Eastern City District Plan

Liveability	
Planning Priority E4: Fostering healthy, creative, culturally rich and socially connected communities	
Action 10	<p>The Planning Proposal will allow for the redevelopment of an underutilised site directly adjacent to the established Strathfield Town Centre. It will contribute to the provision of a walkable place with an active street life (supported by the proposed active frontages to Albert Road and Raw Square) at a human scale.</p> <p>The location of the site near to the Strathfield Railway Station allows for residents to access schools, social, health, sporting, cultural and shared facilities through the public transportation network.</p>
Planning Priority E5: Providing housing supply, choice and affordability with access to jobs and services	
Action 15	<p>Strathfield Council, as directed by the District Plan, is required to prepare a local or district housing strategy outlining the delivery of housing targets for 5 years, 6-10 years and longer term 20-year periods. The Planning Proposal will facilitate the delivery of approximately 280 new dwellings, contributing to these targets.</p>
Planning Priority E6: Creating and renewing great places and local centres and respecting the Districts heritage	
Action 17	<p>The Planning Proposal is located in an area adjacent to an established town centre, and offers potential for a highly walkable and accessible redevelopment of an inner centre site. It is considered to improve the amenity of the town centre and its surrounds, while introducing active uses to the area, creating a community focus.</p>
Planning Priority E11: Growing investment, business opportunities and jobs in strategic centres	
Action 37 & 38	<p>While the Planning Proposal will reinvigorate the existing town centre through a new, high-quality built form. This will assist with future leasing opportunities in and around the Strathfield town centre, and be recognised as a natural expansion of the town centre footprint.</p> <p>Furthermore, the provision of residential dwellings near to a transport</p>

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	interchange (the Strathfield Railway Station) will allow residents the option of living in Strathfield and working elsewhere, with the Harbour CBD, Parramatta and Sydney Olympic Park being within 30minutes – contributing to the creation of a 30-minute city.
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State and Regional Statutory Framework

The following is a list of the State Environmental Planning Policies (SEPPs) that apply to the subject site and consideration of the Planning Proposal's consistency with the objectives and provisions of the SEPPs as detailed in Table 9 below:

Table 7: SEPPs applicable to the subject Planning Proposal

State Environmental Planning Policy	Applicable	Consideration
SEPP No 32 Urban Consolidation (Redevelopment of Urban Land)	Applicable to Planning Proposal	The planning proposal is consistent with SEPP No.32 in providing for the opportunity for the development of additional housing in an area where there is existing public infrastructure, transport, and community facilities, and is close to employment, leisure and other opportunities.
SEPP No. 55 – Remediation of Land	Applicable to Planning Proposal	Proposal is generally consistent with this SEPP. The site is presently used for residential dwellings and a service station, adjacent to an established rail corridor. A Preliminary Site Investigation has been undertaken which indicates that the site can be made suitable for the future intended use.
SEPP No 65 Design Quality of Residential Apartment Development	Applicable to Planning Proposal	Compliance with SEPP No.65 will be demonstrated at the time of making a development application.
SEPP (BASIX) 2004	Applicable to Planning Proposal	Compliance with SEPP (BASIX) will be demonstrated at the time of making a development application
SEPP (Exempt and Complying Development Codes) 2008	Applicable to Planning Proposal	May apply to the development of the site
SEPP (Housing for Seniors or People with a Disability) 2004	Applicable to the Planning Proposal	Not inconsistent. The Planning Proposal does not hinder the application of this SEPP.

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State Environmental Planning Policy	Applicable	Consideration
SEPP (Infrastructure) 2007	Applicable to Planning Proposal	A preliminary traffic assessment has been undertaken. It is proposed that a further detailed traffic assessment be undertaken as part of the Development Application stage.

Section 9.1 Directions

The following is a list of the Directions issued by the Minister for Planning to relevant planning authorities under Section 9.1 of the *Environmental Planning and Assessment Act 1979* and detailed below in Table 10:

Table 10: Section 9.1 Directions applicable to the subject Planning Proposal

Section 9.1 Ministerial Directions	Applicable	Comment
1. Employment and Resources		
1.1 Business and Industrial zones	Applicable	The site is located in the B4 Mixed Use Zone. The proposal does not seek to change the zoning of the site, but rather the FSR and height controls to permit a mixed-use development.
1.2 Rural zones	N/A	The planning proposal does not seek to rezone from a rural land use to a residential, business, industrial, village or tourist land use
1.3 Mining, Petroleum Production and Extractive Industries	N/A	
1.4 Oyster Aquaculture	N/A	
1.5 Rural Lands	N/A	
2. Employment and Heritage		
2.1 Environmental Protection zones	N/A	
2.2 Coastal Protection	N/A	
2.3 Heritage Conservation	N/A	
2.4 Recreation Vehicle Areas	N/A	
3. Housing, Infrastructure and Urban Development		
3.1 Residential zones	Applicable	The Planning Proposal is consistent with Clauses 4 and 5 of this direction. The proposed amendment will allow for the provision of a variety of additional housing in a highly appropriate location, adjacent to a key public transport interchange and an emerging town centre.
3.2 Caravan Parks and Manufactured Home Estates	N/A	

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<i>Section 9.1 Ministerial Directions</i>	<i>Applicable</i>	<i>Comment</i>
3.3 Home Occupations	N/A	
3.4 Integrating Land Use and Transport	Applicable	The Planning Proposal is consistent with this direction and the relevant government policies that apply to the Direction. The site is well served by public transport and the proposal will maximise the use of these facilities.
3.5 Development near Licensed Aerodromes	N/A	
3.6 Shooting Ranges	N/A	
4. Hazard And Risk		
4.1 Acid Sulfate Soils	Applicable	The site is identified as being within a Class 5 Acid Sulfate Soils zone under the LEP. The Prospect-Parramatta River Acid Sulfate Soil Risk Map identifies the site as within the map class of 'No Known Occurrence'. This considers that acid sulfate soils are not known or expected to occur and 'land management activities are not likely to be affected by acid sulfate soil materials'.
4.2 Mine Subsidence an Unstable Lands	N/A	
4.3 Flood Prone Land	Applicable	The Powells Creek and Saleyards Creek Flood Study (WMA Water, November 2016) identifies the site within the Powells Creek catchment area, and during flood events is impacted by overland flow and the main channel in parts. However the use of appropriate freeboard levels will assist in reducing the flood risk impact to development on the site, noting that the site is already zoned for a residential purpose and this planning proposal seeks only height and FSR increases.
4.4 Planning for Bushfire Protection	N/A	
5. Regional Planning		
5.1 Implementation of Regional Strategies	N/A	
5.2 Sydney Drinking Water Catchments	N/A	

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<i>Section 9.1 Ministerial Directions</i>	<i>Applicable</i>	<i>Comment</i>
5.3 Farmland of State and Regional Significance on the NSW Far North Coast	N/A	
5.4 Commercial and Retail Development along the Pacific Highway, North Coast	N/A	
5.5 Development on the vicinity of Ellalong	N/A	
5.6 Sydney to Canberra Corridor	N/A	
5.7 Central Coast	N/A	
5.8 Second Sydney Airport: Badgerys Creek	N/A	
<i>6. Local Plan Making</i>		
6.1 Approval and Referral Requirements	N/A	
6.2 Reserving land for Public Purpose	N/A	
6.3 Site Specific Provisions	N/A	
<i>7. Metropolitan Planning</i>		
7.1 Implementation of the Metropolitan Plan for Sydney 2036	Applicable	The Planning Proposal will provide an outcome consistent with this Direction and the provisions of the Plan for Growing Sydney, the Greater Sydney Region Plan and the Eastern City District Plan.

Local Planning Context

Consideration of the Planning Proposal in relation to Council's local strategic plans is provided below:

Strathfield Community Strategic Plan 2030

The Community Strategic Plan is a primary document that represents the community's aspirations for the next twelve years. It is accompanied by an annual Operational Plan, three year Delivery Program and a Resourcing Strategy. The Plan identifies five key themes (called 'pillars') that underpin the future work. They include:

1. Connectivity
2. Community Wellbeing
3. Civic Pride and Place Management
4. Liveable Neighbourhoods
5. Responsible Leadership

The Planning Proposal is considered to satisfy a number of these themes.

Public Exhibition of the Planning Proposal

**PLANNING PROPOSAL - No 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield
To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)**

The Planning Proposal was placed on public exhibition from **1 May 2018 – 1 June 2018**, in accordance with the provisions of the Act and Regulations.

The notification for the public exhibition included the following:

- Statutory notice in the Inner West Courier (1 May 2018)
- Dedicated page on Council's website (under 'Planning Proposals')
- Update on Council's e-News
- Displays in Council's Customer Service Centre and libraries including the public exhibition information, the Planning Proposal and appendices, draft Strathfield Local Environmental Plan 2012 Maps, Gateway Determination, relevant Council reports and other legislative documents and information (e.g/ Section 9.1 Directions compliance table, State Environmental Planning Policy compliance, Strathfield LEP 2012 written instrument and Maps).
- Just under 2,000 letters were sent to the affected and adjoining land owners and the key transport agencies.
- Telephone and face to face contact with Strategic Planning and Council's Duty Planner.

During the exhibition, 20 submissions were received and the comments raised which specifically relate to the Planning Proposal are considered in this report. Acknowledgement letters / emails were sent to all submitters following the end of the public exhibition / notification period.

For the purpose of reviewing and summarising the key issues raised, the submissions received during the public exhibition are grouped in the following categories: Community submissions and Public Authority submissions. A summary table is provided below in **Table 11**:

Table 11: Submissions received on the subject Planning Proposal

Submission	Number Received
Community	16
Public Authority	4
Total	20

A summary of all submissions received during the public exhibition (1 May 2018 to 1 June 2018) is provided in **Attachment 3**. A copy of the submissions received have been distributed under separate cover

Community Submissions

A total of 16 Community Submissions were received during the exhibition period (as detailed in Table 11 above). Of the 16 objections received, nine were form letters which were identical in form and content. However, as per the requests of the objectors, these have been noted as individual submissions.

These submissions predominantly came from adjoining residents and included issues relating to overshadowing, excessive height, lack of affordable housing, traffic and parking, residential amenity and overcrowding / overdevelopment of the site. A detailed summary of the submissions received and a response to the submissions is provided in **Attachment 4**.

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To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)**

The following is a summary of issues raised in the Community submissions (note that an individual submission may raise more than one topic):

- Reduced ventilation for existing residents in surrounding buildings
- Overshadowing of Pilgrim Avenue and properties to south
- Overdevelopment of the site
- Height increase considered excessive
- Exacerbates overcrowding at Strathfield station
- Lack of affordable housing and housing mix
- Lack of direct sunlight and cross ventilation in proposed units
- Request to demonstrate compliance with Apartment Design Guidelines at Planning Proposal stage
- Excessive provision of car parking spaces
- Concerns over provision of ground floor commercial and retail floorspace and separation from the existing town centre
- Perceived negligible effect on employment in the local area
- The traffic study does not consider the cumulative impacts of traffic in the local area
- Traffic study does not model 'worst case' scenario and assumes majority of residents will utilise public transport

No changes have been recommended to the Planning Proposal as a result of the community submissions as the issues raised are anticipated to be considered in the future development application (DA) as part of the SEPP No.65 and Apartment Design Guide (ADG) compliance.

Independent urban design advice was sought from *David Lock & Associates* (DLA) to conduct a peer review of the proposal and to advise on what was considered to be an appropriate FSR and height for the site as part of undertaking a preliminary assessment of the proposal.

Public Authority Submissions

The following public authorities were consulted and submissions received during the public exhibition of the Planning Proposal:

- Transport for NSW – Sydney Trains
- Transport for NSW – Roads and Maritime Services
- NSW Department of Education and Communities
- NSW Fire and Rescue
- Ambulance NSW
- NSW Police Force
- NSW State Emergency Service
- NSW Ministry of Health
- Energy Australia; and
- Sydney Water

A detailed summary of the comments received and a response to the submissions is provided in **Attachment 4**.

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A response was received from four Public Authorities being, TfNSW – Sydney Trains, TfNSW – Roads and Maritime Services, NSW Department of Education and Communities and Energy Australia. No objection was raised in these submissions.

TfNSW – RMS advised that the proposal would not have significant traffic impacts on the state road network, and provided general comments on connectivity, safety and accessibility for pedestrians and bicycle riders to the existing networks and public transport links.

Draft DCP for the Subject Site

The Planning Proposal was supported by a draft DCP in accordance with the requirements of the Gateway Determination. In this regard, the condition required that the proponent

(II) prepare a site specific DCP reflecting the distribution of height across the entire site. This must ensure that the site which adjoins the residential area should have the lowest, while the part which is closest to the station should have the highest.

A draft DCP has been submitted and is included at **Attachment 5**. The draft DCP was exhibited as part of the Planning Proposal. Council considers that the draft DCP does not go far enough to address a number of issues such as building massing and active street frontages.

It is recommended that an amended DCP for the site be prepared that includes specific provisions including (but not limited to) detailed urban design considerations for any future development of the site including the provision of public access, built form, boundary setbacks, deep soil areas, tree retention and/or future greening and planting, vehicular access and any other relevant issues. This is to be prepared at the proponent's cost and must be submitted to Council for consideration and review

The amended DCP would also need to be re-exhibited in accordance with the requirements of the Act and the Regulations

Updated Traffic and Parking Assessment

As part of the Gateway Conditions, the applicant was required to provide an updated traffic and transport assessment to address the cumulative effect of traffic generation on the intersection at Raw Square and Everton Road;

An updated Traffic and Parking Assessment was prepared by McLaren Traffic Engineering and Road Safety Consultants (dated 6 February 2018)

The traffic assessment is based on the redevelopment of the two (2) sites, upon which a total of four buildings are proposed. The development of Site 1 will include the amalgamation of a total of five (5) existing residential lots. The details of the site relevant to this traffic and parking impact assessment are summarised below:

Site 1 includes the following:

- 51 one-bedroom units;
- 117 two-bedroom units;
- 888m² ground floor retail area;
- 5 levels of basement parking including 355 car parking spaces;
- Two-way driveway access to Pilgrim Avenue.

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Site 2 includes the following:

- 113 two-bedroom units;
- 471m² ground floor retail area;
- Two-way driveway access to Raw Square.

In addition to the above, a total of 30 parking spaces will be provided to the public for commuters.

A copy of the updated Traffic and Parking Assessment is included in **Attachment 6**. The updated Traffic and Parking Assessment makes the following conclusions:

The SIDRA intersection model indicates that there will be negligible impact on the surrounding road network as a result of the traffic generated by the proposed development of both Site 1 and Site 2. Nearly all relevant intersections maintain their existing Level of Service, with no significant increase in delay times or queue lengths predicted. The change in Level of Service occurs at the intersection of Raw Square / Everton Road / Leicester Ave which increases from LoS A to LoS B during the PM peak period. Level of Service B however, still has the characteristic of minimal delays and spare capacity and as such is not significantly detrimental to the intersections performance. It should be noted that a worst case has been modelled, with no consideration given to the existing traffic generation of the sites.

The planning proposal has the capacity to provide some 583 car parking spaces, including 30 spaces for public commuter parking, across 5 basement levels and ground floor, exceeding the requirements of Council's DCP. The concept plans include appropriate loading and servicing facilities and the site has the capacity to meet the requirements of the DCP with regards to both bicycle storage and disabled parking, which can be detailed at the Development Application stage.

The traffic generation associated with the two sites, estimated at a total of 160 vehicle trips in the AM and 149 vehicle trips in the PM (AM – 79 inbound and 81 outbound; PM – 72 inbound and 77 outbound) has been shown to have no noticeable impact on the surrounding road network in terms of intersection delays or queue lengths.

Provision of Affordable Housing

As part of the Planning Proposal the proponent has not offered any public benefit. The Eastern city District Plan makes provision for the development of affordable housing. A recent approval by the Sydney Eastern City Planning Panel for a Planning Proposal at No 11- 17 Columbia Lane required that the proponent provide a minimum 5% affordable housing as part of the Planning Proposal.

Strathfield Council is not currently listed in State Environmental Planning Policy No 70 (SEPP 70) and there isn't currently a mechanism available within Strathfield LEP 2012 to "lock in" affordable housing as part of an amending LEP.

The proponent of the subject Planning Proposal has not negotiated to enter into a Voluntary Planning Agreement (VPA) with Council to provide any public benefit, however Council considers that the need for affordable housing as part of any development is important.

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In seeking consistency with respect to the provision of affordable housing it is recommended that a minimum of 5% of the overall number of units be provided as affordable housing. It is recommended that this requirement be included in the Planning Proposal as part of the amending LEP.

Based on the details provided with the Planning Proposal with respect to the total number of units, a 5% minimum would yield approximately 8 affordable housing units for Site 1 and 5 affordable housing units for Site 2.

Conclusion and Next Steps

Council in relation to 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield has exhibited the Planning Proposal.

Council received 20 submissions from the community and public authorities in relation to the Planning Proposal. No changes have been recommended to the Planning Proposal as the issues raised in the community submissions were design and traffic related rather than to the Planning Proposal. These issues will be addressed in the assessment of any future development application

The public authorities did not raise any objections to the Planning Proposal and therefore, no changes are required to the Planning Proposal as a result.

RECOMMENDATION

- a. That the Strathfield Local Planning Panel recommends to the Council that the Planning Proposal to amend Strathfield LEP 2012 in relation to 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield to:
 - i. Amend the Height of Buildings Map (HOB) to increase the height of buildings for Nos.2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield to 54m
 - ii. Amend the Floor Space Ratio Map (FSR) to increase the maximum floor space ratio for Nos.2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield to 5:1

Subject to a minimum of 5% of the total number of units being provided as affordable housing and the requirement for affordable housing being included as a provision in Strathfield LEP 2012.

- b. That the Strathfield Local Planning Panel recommends to the Council that as part of any future redevelopment of the subject sites, a minimum of 30 parking spaces be provided for use as public/commuter parking. The details of the provision of the public/commuter parking are to be negotiated with Council as part of any future Development Application for the redevelopment of Site 1 and/or Site 2.
- c. That the Strathfield Local Planning Panel recommends to Council to prepare an amendment to the Strathfield Comprehensive Development Control Plan 2005 to reflect detailed urban design considerations for any future development of the site including the provision of public access, built form, boundary setbacks, deep soil areas, tree retention, vehicular access and any other relevant issues. The DCP is to be prepared at the proponent's cost

**PLANNING PROPOSAL - No 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield
To amend the Height of Buildings and FSR in Strathfield LEP 2012 (Cont'd)**

- d. That the Strathfield Local Planning Panel notes the submissions received during the public exhibition of the Planning Proposal for Nos.2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield and recommends to Council that all persons who made a submission to the Planning Proposal be advised of Council's decision.

ATTACHMENTS

1. [↓](#) Gateway Determination
2. [↓](#) Planning Proposal Document - Exhibition Version
3. [↓](#) Letter to DPE from Council - Issues Relating to Planning Proposal
4. [↓](#) Submission Summary
5. [↓](#) Draft Development Control Plan - Planning Proposal
6. [↓](#) Traffic and Parking Assessment



Gateway Determination

Planning Proposal (Department Ref: PP_2017_STRAT_004_00): to amend the floor space ratio and height development controls at 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield.

I, the Executive Director, Regions at the Department of Planning and Environment as delegate of the Greater Sydney Commission, have determined under section 56(2) of the *Environmental Planning and Assessment Act, 1979* (the Act) that an amendment to the Strathfield Local Environmental Plan (LEP) 2012 to amend the floor space ratio and height development controls at 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield should proceed subject to the following conditions:

1. Prior to community consultation, the Planning Proposal is to be updated and amended to:
 - (a) provide a project timeline, outlining the anticipated timeframes for the plan making process;
 - (b) reference and address all relevant priorities and actions in the draft Greater Sydney Region Plan;
 - (c) reference and address all relevant priorities and actions outlined in the draft Eastern City District Plan;
 - (d) provide updated overshadowing diagrams to state the correct date of the analysis (being the winter solstice at 21 June) and also show the overshadowing impacts of the existing built form;
 - (e) provide updated analysis of solar access and cross ventilation to the indicative design for the existing service station site at 9 Albert Road;
 - (f) address and justify the inconsistency with Section 117 Direction 4.1 Acid Sulphate Soils as the subject site is identified as being on Class 5 land in the Strathfield LEP 2012;
 - (g) address and justify the inconsistency with Section 117 Direction 4.3 Flood Prone Land. In this regard, a flood study is to be provided to demonstrate the suitability of the site for redevelopment;
 - (h) provide an updated traffic and transport assessment to address the cumulative effect of traffic generation on the intersection at Raw Square and Everton Road;
 - (i) provide a preliminary contamination assessment of the extended site to demonstrate the suitability of the site for development;
 - (j) provide an acoustic report to demonstrate the suitability of the site for redevelopment in light of its proximity to the adjacent railway line and road network. The report should also consider the noise impacts of the operation of the existing service station should this use remain whilst the properties at 2-6 Pilgrim Avenue and 11-13 Albert Road are redeveloped;

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- (k) provide an air quality report to demonstrate the impacts of the existing service station should it not be redeveloped; and
 - (l) prepare a site specific DCP reflecting the distribution of height across the entire site. This must ensure that the site which adjoins the residential area should have the lowest, while the part which is closest to the station should have the highest.
2. The planning proposal is to be amended to include any relevant findings of the required studies and the amended proposal is to be forwarded to the Department for review and endorsement prior to community consultation.
3. The planning proposal is to be updated to address the Strathfield Priority Precinct draft plan(s) should this be released either prior to community consultation of the planning proposal or the LEP being finalised.
4. Community consultation is required under sections 56(2)(c) and 57 of the Act as follows:
 - (a) the planning proposal must be made publicly available for a minimum of **28 days**;
 - (b) the relevant planning authority must comply with the notice requirements for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in Section 5.5.2 of *A guide to preparing local environmental plans* (Department of Planning and Environment 2016); and
 - (c) consultation must be undertaken with the landowner(s) of the service station at 9 Albert Road, Strathfield.
5. Consultation is required with the following public authorities and / organisations under section 56(2)(d) of the Act and/or to comply with the requirements of relevant Section 117 Directions:
 - Transport for NSW – Sydney Trains;
 - Transport for NSW - Roads and Maritime Services;
 - NSW Department of Education and Communities;
 - NSW Fire and Rescue;
 - Ambulance NSW;
 - NSW Police Force;
 - NSW State Emergency Service;
 - NSW Ministry of Health;
 - Energy Australia; and
 - Sydney Water.

Each public authority/organisation is to be provided with a copy of the planning proposal and any relevant supporting material, and given at least 21 days to comment on the proposal.

6. A public hearing is not required to be held into the matter by any person or body under section 56(2)(e) of the Act. This does not discharge Council from any

obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).

7. The timeframe for completing the LEP is to be **12 months** following the date of the Gateway determination.

Dated *6th* day of *November* 2017.

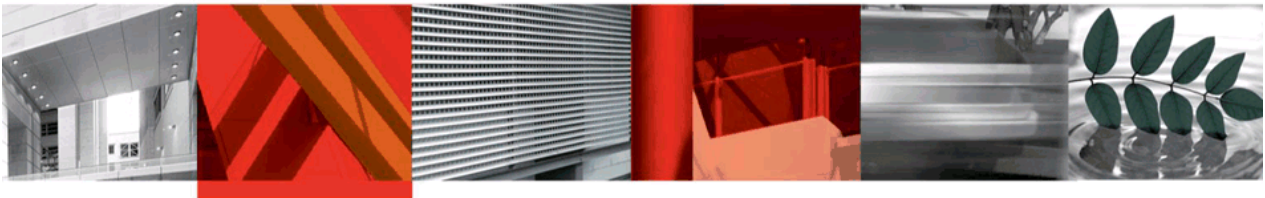

Stephen Murray
Executive Director, Regions
Planning Services
Department of Planning and Environment

Delegate of the Greater Sydney
Commission

Strathfield PP_2017_STRAT_004_00 (17/08831)



Planning Proposal For Public Exhibition



2-6 Pilgrim Avenue, 9 Albert Road & 11-13 Albert Road,
Strathfield

Amendments to Building Height and Floor Space Ratio

Submitted to Department of Planning and Environment via Strathfield Council
On Behalf of Convertia Pty Ltd

February 2018 ■ 14474

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JBA operates under a Quality Management System that operates in accordance with ISO 9001:2008. This report has been prepared and reviewed in accordance with that system. If the report is not signed below, it is a preliminary draft.

This report has been prepared by:



Christopher Curtis

12/02/2018

This report has been reviewed by:



Benjamin Craig

12/02/2018

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H	Powells Creek Flood Study <i>WMA Water for Strathfield Council</i>
I	Preliminary Site Investigation <i>EI Australia</i>
J	Draft Development Control Plan

1.0 Introduction

This report has been prepared by JBA on behalf of Convertia Pty Ltd in relation to land at 2-6 Pilgrim Avenue, 11-13 Albert Road and 9 Albert Road, Strathfield (the site).

The purpose of the Planning Report is to document and integrate the environmental analysis undertaken in support of the rezoning proposal and summarise the proposed approach to development. Specifically, the Planning Report demonstrates how the planning proposal responds to the requirements of section 55 of the *Environmental Planning and Assessment Act 1979* (EP&A Act). The Report has been drafted in accordance with the guidelines prepared by the Department of Planning dated October 2012 entitled "A guide to preparing Planning Proposals" and the subsequent publication titled "A guide to preparing local environmental plans" (April 2013).

This Planning Proposal addresses the following specific matters:

- Objectives and intended outcomes of the Planning Proposal;
- Explanation of the proposed provisions;
- Justification of the proposal, specifically;
 - Need for the Planning Proposal;
 - Relationship to strategic planning framework;
 - Environmental, social and economic impact;
 - State and Commonwealth interests; and
- Community consultation.

The site is located within the Strathfield Council Local Government Area (LGA) and is therefore subject to the Strathfield Local Environmental Plan 2012 (Strathfield LEP). The current height and floor space ratio (FSR) planning controls under the Strathfield LEP are considered to unnecessarily restrict the ability for the site to realise its full development potential, taking into consideration the site's size and dimensions, its local context, the planning controls for surrounding sites and the opportunity presented by the site to deliver new high-quality housing in a timely manner and in a strategic location close to transport services and amenities.

This planning proposal describes the site, outlines the proposed changes to Strathfield LEP and provides an environmental assessment of the proposal. The report should be read in conjunction with the indicative scheme prepared by Kennedy Associates Architects at **Appendix A** and supporting technical reports appended to this Planning Proposal.

1.1 Pre-Gateway Review

A Pre-Gateway Review was requested by the proponent in April 2016. The Department of Planning and Environment (DP&E) conducted an assessment and proceeded to forward the Planning Proposal on to the Joint Regional Planning Panel (JRPP) for determination. The JRPP determined on 18 October 2016 that the Proposal can proceed to Gateway, subject to the below changes and additional information being provided (**Appendix D**):

- The extension of the proposal across the whole street block, including the service station site;
- The permissible FSR over the whole street block being 5:1;
- The maximum permissible height being 54m;

- A Development Control Plan (DCP) being prepared showing the proposed distribution of height, with that part adjoining the residential area having the lowest height and the part closest to the railway station the highest;
- A traffic study be prepared that applies to the whole street block and includes any public commuter parking provided; and
- Land contamination and hydraulic studies are to be left to the development application stage.

1.2 Gateway Determination

A Gateway Determination for this Planning Proposal was received on 6 November 2017 from the DP&E. The Determination identified a number of updates required to the Planning Proposal, which have been addressed in this report.

Table 1 – Gateway Determination Conditions

Condition	Where in this report?
(a) provide a project timeline, outlining the anticipated timeframes for the plan making process;	Section 1.2.1
(b) reference and address all relevant priorities and actions in the draft Greater Sydney Region Plan;	Section 7.1.2
(c) reference and address all relevant priorities and actions outlined in the draft Eastern City District Plan;	Section 7.1.2
(d) provide updated overshadowing diagrams to state the correct date of the analysis (being the winter solstice at 21 June) and also show the overshadowing impacts of the existing built form;	Appendix A
(e) provide updated analysis of solar access and cross ventilation to the indicative design for the existing service station site at 9 Albert Road;	Appendix A
(f) address and justify the inconsistency with Section 117 Direction 4.1 Acid Sulphate Soils as the subject site is identified as being on Class 5 land in the Strathfield LEP 2012;	Section 7.2.3 and Appendix I
(g) address and justify the inconsistency with Section 117 Direction 4.3 Flood Prone Land. In this regard, a flood study is to be provided to demonstrate the suitability of the site for redevelopment;	Section 7.2.3, Section 8.3 and Appendix H
(h) provide an updated traffic and transport assessment to address the cumulative effect of traffic generation on the intersection at Raw Square and Everton Road;	Appendix C
(i) provide a preliminary contamination assessment of the extended site to demonstrate the suitability of the site for development;	Appendix I
j) provide an acoustic report to demonstrate the suitability of the site for redevelopment in light of its proximity to the adjacent railway line and road network. The report should also consider the noise impacts of the operation of the existing service station should this use remain whilst the properties at 2-6 Pilgrim Avenue and 11-13 Albert Road are redeveloped;	Appendix E
(k) provide an air quality report to demonstrate the impacts of the existing service station should it not be redeveloped; and	Appendix F
(l) prepare a site specific DCP reflecting the distribution of height across the entire site. This must ensure that the site which adjoins the residential area should have the lowest, while the part which is closest to the station should have the highest.	Appendix J

1.2.1 Timeline for Implementation

It is anticipated that this amended Planning Proposal will be resubmitted to the DP&E for endorsement for public exhibition in March 2018.

Public exhibition is to be for a period of 28 days, with public authorities and other organisations provided 21 days to comment on the proposal, during the public exhibition period. Public exhibition is anticipated to occur between April and May 2018.

The Planning Proposal will be amended as required subject to comments from the public exhibition phase and other agencies and organisations and resubmitted to DP&E for approval and gazettal. This is anticipated to occur in June/July 2018 with the Plan made by the DP&E in August 2018.

Table 2 – Timeline

Description	Date/Timeframe
Commencement Date (date of Gateway Determination)	6 November 2017
Completion of updated Planning Proposal	February 2018
Submission to DP&E for endorsement	March 2018
Public Exhibition	April - mid May 2018
Consideration of submissions	Mid May-June 2018
Timeframe for consideration of a proposal post-exhibition	June 2018
Date of submission to DP&E to finalise the LEP	June/July 2018
Anticipated date for the DP&E to make the Plan	August 2018

2.0 Background

The Sydney metropolitan area is projected to experience significant population growth in the next 10-15 years with an additional 1.6 million people forecast to be living in the region by 2031. Coupled with this anticipated growth will be increased demand on housing, employment, transport, services and facilities. To manage such significant growth, the State Government has released the Plan for Growing Sydney, which sets out a strategic framework to guide and shape Sydney's future development and growth and enable it to realise the NSW Government's vision for Sydney of becoming competitive global city. To achieve this overarching goal of particular importance is the forecast need to deliver some 664,000 new dwellings in the metropolitan area by 2031 to meet the projected population growth. Of this metropolitan wide total, some 130,700 are required in the Eastern City District which includes Strathfield.

The Eastern City District is a highly-urbanised location and therefore to accommodate such significant growth in this area the State Government has made a clear priority to deliver housing through urban renewal, with a particular focus on around key centres and transport hubs. This is reflected within the Plan through Goals 2 and 3 which seek to make Sydney the "city of housing choice, with homes that meet our needs and lifestyles" and "A great place to live with communities that are strong, healthy and well connected."

Underneath these goals the Plan sets out a series of clear 'Directions' and 'Actions' aimed at focusing urban renewal and maximising housing deliver within and around centres and public transport facilities, including:

Accelerate housing supply and local housing choices. (Action 2.1.1)

Undertake urban renewal in transport corridors which are being transformed by investment and around strategic centres. (Action 2.2.2)

Support urban renewal by directing local infrastructure to centres where there is growth. (Action 3.1.1)

These Actions are supported by the updated draft Eastern City District Plan, which identifies an increased minimum housing target of 157,500 by 2036. Importantly, the draft Plan recognises that Strathfield has a housing target of an additional 3,650 by 2021, with 75% growth in the single-person household group, the largest in the Eastern City District. The draft District Plan confirms that Strathfield Council will monitor and support the delivery of that housing target, while investigating local opportunities to address demand (and diversity) in and around local centres and infill areas. A key focus on transport corridors (such as the adjacent rail corridor passing through the Strathfield Railway Station to the east of the site) and other areas with high accessibility is also to be adhered to by Strathfield Council to successfully meet the necessary housing requirements. The draft Greater Sydney Region Plan also reflects this focus.

The emerging focus on delivering additional housing through transit-oriented development around key transport interchanges, notably railway stations, is clearly seen through the re-development of centres such as Epping, St Leonards, Chatswood, and Hornsby. Interestingly, all these centres (except for Chatswood) which have or are planned to accommodate significant growth and density, all maintained lower passenger movement figures compared to the Strathfield Railway Station (Table 3) in 2014.

Strathfield Station provides access to several major rail lines (the T1 North Shore, Northern and Western Line, and T2 Airport, Inner West and South Line) and is served by a nine different Sydney Bus routes that utilise the Railway Station as an

interchange. Furthermore, when compared to other similar town centres, Strathfield is second in 24-hour barrier counts to only Chatswood (Table 1). It therefore has more passenger movements in and out than St Leonards, Hornsby and Epping. It is worth considering that these barrier counts only measure movements in and out of the station, and do not take internal train line changes into account. Noting that Strathfield is a key terminal for three key train lines, and a link to a further four, internal passenger movements are likely to be significantly higher than the barrier counts suggest.

Table 3 – Typical 24-Hour Barrier Counts for 2014

Centre Railway Station	Typical 24-hour Barrier Counts (2014)
Chatswood	44,400
Strathfield	41,420
St Leonards	35,180
Hornsby	24,500
Epping	21,140

Source: Bureau of Transport Statistics 2014

St Leonards, while not being the juncture of a rail branch line, is a key redevelopment centre identified within the Plan with potential for a future Sydney Rapid Transit station. Accordingly, due to the existing public transport interchange and potential future capability, development is permitted of buildings up to 190m in height. These land use controls are designed to focus high density development in and around the existing train station to maximise public transport accessibility and to achieve the Plan's objective of a well-connected city with people able to live close to jobs, facilities and services.

Chatswood is a major interchange for public transport on the north shore with both the Epping to Chatswood Rail Links via Macquarie University branching off the main North Shore line to Hornsby. This ensures that the Chatswood is clearly identifiable as a major interchange for public transport. Accommodating higher density around the railway station and rail corridor, Chatswood exhibits permissible heights up to 90 metres to capitalise on the availability of transportation. With similar public transport movements to Strathfield, it is reasonable to expect that Strathfield could support densities around the transport interchange of similar scale to Chatswood.

Epping is a key public transport interchange with the Epping Railway Station providing access to the T1 North Shore, Northern and Western Line, and further north to the Central Coast and Newcastle Line. The future North-West Rail Link will also operate trains through the Epping interchange. Subsequently, in line with the State Government's focus on higher density development around key transport hubs, the Epping Urban Activation Precinct was announced in 2013 and permits buildings up to 22 storeys in height. This is assisting the establishment of Epping as a key development precinct with development primarily concentrated around the railway station and corridor.

Similarly, Hornsby exhibits heights of up to 25 storeys, with these heights decreasing as development moves away from the rail corridor. Again, density is centred on the established railway station which is a key public transport interchange that provides access south to the Sydney CBD through Epping and Strathfield, and north to Newcastle.

As is evident from the above, while heights and density slightly differ between the centres, the key similarity between these centres is the focus on higher density development within close proximity to the established railway station and along the rail corridor. Strathfield exhibits very similar characteristics to these centres, with a major transport interchange located adjacent to the town centre, and as

noted Strathfield presently services more commuters so is arguably a larger interchange worthy of greater density.

Overall, Strathfield town centre presents a clear opportunity to maximise housing delivery as it has all the underlying characteristics that support the delivery of high density transit-oriented development consistent with other centres within Metropolitan Sydney. Delivering such an outcome for Strathfield town centre would contribute significantly to realising the vision set out in A Plan for Growing Sydney as it would maximise the use of the existing public transport network and provide homes closer to jobs, services and facilities.

2.1 Strathfield Residential Land Use Study

The Strathfield Residential Land Use Study, prepared for Strathfield Council in 2011, identifies areas where residential growth is to occur, and where the character of existing neighbourhoods is to be maintained, while assisting Council to accommodate 8,300 new dwellings over the next 25 years.

The Study forecast in 2011 that by 2031 the population of Strathfield was expected to increase to 47,100. Subsequent to this in 2013 the Department of Planning and Infrastructure released updated population forecasts which identified the Strathfield Local Government Area (LGA) as having a 2011 population of 37,150, with this expected to grow by 52.2% to 56,550 persons by 2031.

Population projections for Strathfield between 2011 and 2013 have therefore risen by 9,450 persons, which represent a significant proportion of the total population of the LGA. The increased population forecast by the then Department of Planning and Infrastructure signifies an even greater need to deliver additional housing in the LGA that is above and beyond that which has already been planned for by Strathfield Council.

In seeking to develop a strategy for housing delivery, the Strathfield Residential Land Use Study recognised that the majority of existing residential capacity in the LGA is located around the Parramatta Road and rail corridors.

The Strathfield Town Centre precinct was identified by the study as having a maximum capacity (under the 2011 planning controls) for an additional 247 dwellings with the wider LGA identified as having capacity to provide a likely dwelling yield (again, under 2011 planning controls) of around 4,900 dwellings. Achieving the maximum growth available under the 2011 controls assumes that all the sites with spare capacity or potential for residential would be delivered prior to 2031, which is unlikely to occur and even if it were possible, would still result in significantly less housing than that required to meet forecast population growth under A Plan for Growing Sydney.

The Residential Land Use Study was therefore tasked with identifying further residential capacity in 2011. Following a review of the LGA additional capacity for the Strathfield Town Centre was calculated in the Study to be 410, an increase of 225 dwellings. Across the LGA as a whole, the Study identified a total of 6,294 dwellings for new residential development, still 2,006 dwellings less than the 8,300 required.

Amongst other things the Study concludes that increased residential capacity could be achieved through the intensification of the land bound by Elva Street to the west and Raw Square to the east, which includes the subject site. The use of this land for residential development would result in a theoretical likely yield of 595 dwellings assuming a 75% efficiency rate.

Whilst increased density in this location is clearly the appropriate conclusion, we note that when compared to similar transport interchange hubs, such as

Chatswood, Epping and St Leonards, the densities suggested in the Residential Land Use Study appear to represent a significant underutilisation of the lands potential for residential development, particularly due to its immediate proximity to the Strathfield Railway Station transport interchange.

In particular, we note that the study recommends a floor space ratio of 3.5:1 and building height of 35m for the subject site. Again, when likened to comparable centres serviced by major railway stations, the recommended controls appear to represent a significant underutilisation of the sites potential to accommodate residential development, particularly given recent population and housing projections.

Given the recent release of dwelling targets under A Plan for Growing Sydney, and the likely further increase of these targets following the release of the Sub-Regional Strategies, it is evident that the existing planning controls and dwelling projections for the Strathfield Town Centre will need be revised upwards. Particularly as the town centre and its surrounds is logically the most appropriate location to accommodate further growth, as this will maximise the use of existing and future public transport, services and facilities, and will minimise the need to deliver additional housing in less sustainable car dependent locations.

2.2 Key Redevelopment Precincts

A number of key redevelopment precincts in the Strathfield area are responding to the need for higher density development along the established rail corridor in accordance with A Plan for Growing Sydney, including the Columbia Precinct and the Strathfield Triangle (within the Canada Bay Council LGA). These are located to the north of the subject site as identified **Figure 1**. The Strathfield Town Centre that falls within the Strathfield LGA is situated to the south-east of the subject site in closer proximity to the railway station. Subsequently, the subject site forms a key connection between all three precincts (**Figure 2**) and will make a key contribution to the form and character of development along the rail corridor.

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- The Site
- Strathfield Town Centre Precinct
- Existing Sydney Train Lines (T1, T2)

Figure 1 – Key redevelopment precincts
 Source: NearMap

The Strathfield Town Centre, Strathfield Triangle and the Columbia Precinct all provide higher residential tower development than the surrounding area, focused around the established rail corridor, Homebush Railway Station and Strathfield Railway Station. This provides a consistent built form through the area around the intersection of the T1 North Shore, Northern and Western Line and the T2 Airport, Inner West and South Line rail lines, all of which merge at the key interchange provided by the Strathfield Railway Station. The subject site is positioned at the pivot point between these three precincts and transport connections (Figure 2), thus providing the ideal opportunity to deliver built form that reflects its central status.

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Figure 2 – The site is located at the pivot point of the establish development corridor

2.2.1 Strathfield Triangle Development Control Plan

As identified earlier, the LGA of Canada Bay Council (CBC) is located to the north of the rail corridor with development in that area clearly visible from the Strathfield Town Centre. This area is known as the Strathfield Triangle and is approximately 150m north of the subject site and 250m north of the Strathfield Town Centre (Figure 3). Subsequently, CBC have prepared an area specific Development Control Plan (DCP) for the area which guides development in the precinct.



Figure 3 – The Strathfield Triangle is situated directly north of the site
Source: NearMap

Notably, the DCP recognises the precincts’ convenient location to the Strathfield Railway Station and the Strathfield Town Centre and seeks to encourage development of a compatible mix of retail and residential development. It makes mention of the fact that the site is surrounded by major transport corridors which create barriers to its integration with the urban fabric of Strathfield, however that its proximity to the Strathfield Railway Station underpins the precincts potential for urban renewal and higher density built form.

To accommodate this urban renewal, the DCP provides building heights across the precinct varying from five storeys in the central-eastern part of the precinct (Figure 4), to 18 storeys in the southern-most point of the triangle (adjacent to the rail

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corridor). The DCP identifies the need for the tallest buildings to address the widest spaces along the rail corridor (particularly where the rail lines branch out), to create a strong urban edge and act as an acoustic and visual buffer for land behind.

The built form controls set out in the DCP; particularly the building heights confirm the clear vision of CBC to establish a clear town centre location through more intense forms of development to the south of the precinct nearest to the Strathfield Town Centre and the railway station. It is noted that subject site is better positioned to the town centre or the railway station than all the land within the Strathfield Triangle Precinct.



Figure 4 – Strathfield Triangle building heights from the CB Council DCP
Source: Canada Bay Council, Clouston Associates

2.2.2 Columbia Precinct

The Columbia Precinct is a Part 3A Major Project determined by the Department of Planning and Environment in 2013. The concept plan for the precinct proposed to redevelop the existing industrial area to a mixed-use development, involving a mix of commercial, residential and recreational spaces. The precinct sits to the west of the Strathfield Triangle, approximately 600m to the north-west of the subject site, and covers an area of just under 30,000m².

The concept plan was approved for a proposed mix of uses with over 60,000m² of residential, across several buildings. These buildings included seven residential towers above podiums, with heights ranging between six and 21 storeys. These

buildings transitioned from the lower heights in the north-west, to the taller buildings closest to the rail corridor in the south-west, which is consistent with the Strathfield Triangle DCP as described above, by presenting higher development to the rail network to act as an acoustic and visual buffer.

2.2.3 Strathfield Town Centre

The subject site is situated to the north-west of the Strathfield Town Centre, which is adjacent to a key interchange on the Sydney Trains rail network, the Strathfield Railway Station. Strathfield town centre is unique in that it is influenced by the planning visions of three Councils – Strathfield, Canada Bay and Burwood – due to the suburb falling across LGA boundaries. The subject site is located in the Strathfield LGA and is therefore falls under the planning controls imposed by the Strathfield LEP.

Strathfield Town Centre Master Plan Project

The Strathfield Town Centre Master Plan, prepared and exhibited in 2008 by Strathfield Council, affects land bound by the railway corridor to the north, Raw Square to the west, Redmyre Road to the south and The Boulevard to the east. While the subject site is not located within this area (**Figure 5**), the Master Plan identifies it as a 'key site' for future higher density development of a residential and mixed-use nature.

The primary purpose of the Master Plan was to apply planning controls designed to deliver and reinforce the town centre's status as the key focal point of the Strathfield LGA, which inevitably necessitate the centre to having the most intense forms of development.

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Figure 5 – The site is situated to the immediate west of the Strathfield Town Centre
Source: NearMap

The town centre is zoned B3 Commercial Core to reflect the precincts standing within the urban hierarchy, which focuses the centre around its public transport interchange. The Master Plan also identifies floor space ratios for the town centre of between 4:1 and 7.5:1, with building heights from 11m up to 54m, with parts of the site identified as potentially having 40 storeys. The Master Plan also sets out several key priorities and initiatives for the town centre including:

- redevelopment of the existing Council car park site to an underground Bus Interchange in the east of the precinct with major mixed-use development in two towers above (**Figure 6**);
- the creation of a new town square with active frontages, including restaurants and cafes; and
- Redevelopment of the Strathfield Plaza with an enlarged retail component and two mid-rise towers above (**Figure 7**).

The Master Plan also identifies the subject site as a 'key site' for future higher density residential and mixed-use development. Whilst this is the case it is noted that the current planning controls do not reflect the sites standing as a future higher density site in accordance with the dwelling targets under A Plan for Growing Sydney. Additionally, to maintain consistency with the adjacent Strathfield Triangle and Columbia Precinct, building heights along the rail corridor

should generally be higher than currently allowed for to maximise the use of the public transport network and to provide a visual and acoustic barrier to the rail corridor.



Figure 6 – Strathfield town centre with underground transport interchange
Source: Strathfield Council



Figure 7 – “New LOOK” Strathfield Town Centre
Source: Strathfield Council

2.3 Summary

In summary, it is noted that:

- Significant population growth within the Sydney Metropolitan Area is forecast to drive future housing demand above and beyond that previously anticipated prior to 2014.
- A Plan for Growing Sydney identifies a need for an additional 664,000 dwellings in Metropolitan Sydney by 2031, with some 130,700 of these to be provided in the Central Subregion including Strathfield.

- Given the significant population growth and limited greenfield opportunities, urban renewal and infill development is expected to be pivotal to meeting these dwelling targets.
- The Plan clearly prioritises urban renewal around key centres and transport hubs as this will help deliver a more accessible, connected and sustainable Sydney;
- The draft District Plan identifies Strathfield as needing to provide additional housing in areas near centres with high accessibility along transport corridors;
- Transit oriented development is a key feature of the plan as has already occurred or been planned for in centres such Epping, Chatswood, St Leonards and Hornsby. These key centres focus high density residential development around strategic transport interchanges;
- Strathfield Town Centre exhibits almost identical underlying characteristics to these other key centres given it's standing as a key transport interchange that provides links to the northern, western and eastern suburbs of Metropolitan Sydney.
- The Strathfield Residential Land Use Study, while recognising the need for additional residential growth within the Town Centre area, does not recognise the strategic nature of the Strathfield Railway Station and does not take into account the most recent dwelling and population forecasts. Subsequently, its dwelling targets are somewhat out of date and fall short of the numbers required to satisfy A Plan for growing Sydney. This planning proposal seeks to respond and address that shortfall and in doing so seeks to maximise the opportunity presented by the station and the town centre.
- It is noted that there are three key areas that make up the wider area surrounding Strathfield Station, these being the Columbia Precinct, the Strathfield Triangle and the Strathfield Town Centre. Despite not being as central as the Strathfield Town Centre or the subject site, both the Columbia Precinct and Strathfield Triangle focus high buildings towards the rail corridor to act as visual and acoustic buffers for future residents. The building heights in these areas range from between 18 storeys up to potentially 40 storeys.
- The subject site is situated in close proximity to Strathfield Town Centre, approximately 200m from the railway station and acts as a key link between the town centre, the Columbia Precinct and Strathfield Triangle. The principle of additional height and density in this location is therefore considered to be highly appropriate and development controls should be updated to reflect the clear opportunity provided by the site to deliver significant housing in a sustainable and highly strategic location.
- Delivering additional housing in and around the town centre and railway station will have the added benefit of reducing the need for Strathfield Council to identify other less suitable sites for additional housing to meet the updated dwelling targets.

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3.0 The Site

This chapter briefly describes the site. Further detail is provided in the sections describing the different elements of the existing environment.

3.1 Site Location

The proponent’s site is located at 2-6 Pilgrim Avenue and 11-13 Albert Road, with the Planning Proposal also including the adjacent site at 9 Albert Road, in Strathfield (Figure 8). It is situated close to the boundary of the Strathfield LGA with the LGAs of Canada Bay Council and Burwood Council.

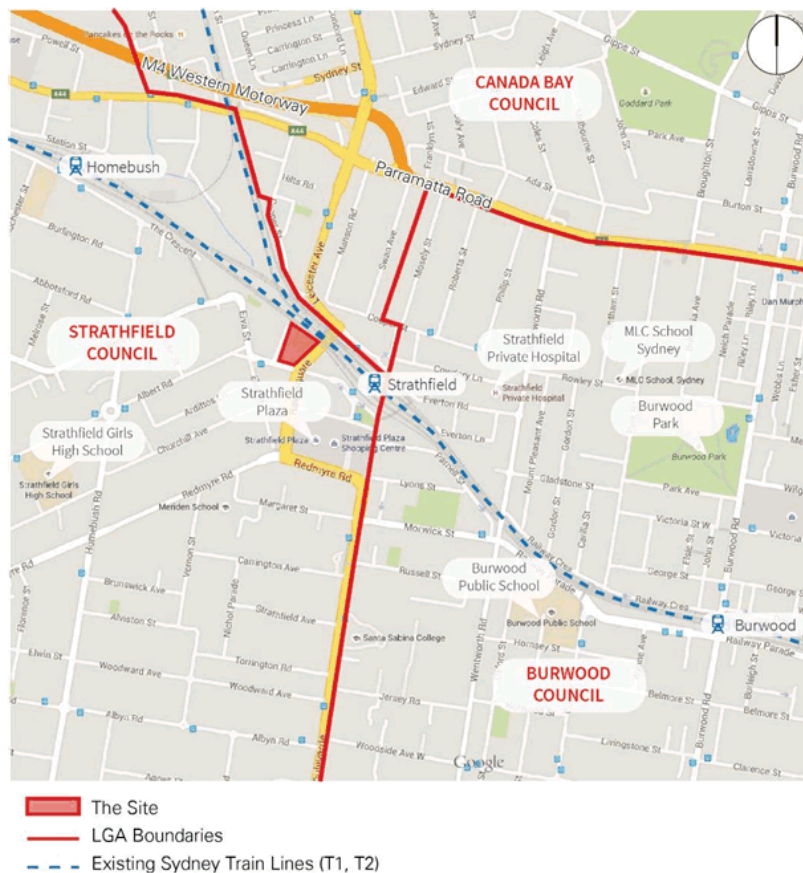


Figure 8 – The subject site is located at the intersection of three local Council areas
 Source: Google

The general Strathfield area varies in its style, with a mix of high-rise and low-rise residential development. The Strathfield Railway Station is located approximately 200m to the east of the site, providing fast, direct and reliable access to the north, east and west of the metropolitan Sydney area. The M4 Motorway also begins to the north of the site, heading west to Parramatta, Blacktown and Penrith.

The site itself is located approximately 10kms west of the Sydney Central Business District, and 10kms south-east of Parramatta.

3.2 Site Description

The site comprises six lots as outlined in **Table 4**, and is irregular in shape, with a total area of approximately 4,885m². Five of these lots are under the ownership of the proponent with the adjacent lot owned separately. The primary frontage is approximately 60m to Pilgrim Avenue, with secondary frontage to Albert Road provided for the adjacent lot. A site survey prepared by DJ Barrington & Associates is attached at **Appendix B** and provides full details of the lot boundaries, levels and easements.

Table 4 – Lots subject to this planning proposal

Reference	Street address	Legal description	Area
Site A (proponent owned)	2 Pilgrim Avenue	SP8785	500 m ²
Site B (proponent owned)	4 Pilgrim Avenue	Lot 9 DP15917	472 m ²
Site C (proponent owned)	6 Pilgrim Avenue	Lot 8 DP15917	433 m ²
Site D (proponent owned)	13 Albert Road	Lot A DP100558	748 m ²
Site E (proponent owned)	11 Albert Road	Lot B DP100558	715 m ²
Site F	9 Albert Road	Lot 100 DP807807	2,017 m ²

The majority of the site is covered by asphalt and concrete paved areas that are accessed via a single driveway off Albert Road and two driveways off Pilgrim Avenue. Sites A, B and C contain detached single storey brick residential dwellings, with associated landscaping and outbuildings. Sites D and E each contain two storey unit buildings, with a central driveway to a rear at-grade parking area. Site F is currently occupied by a Shell Service Station.

Vegetation is limited to a select number of trees, mainly along Pilgrim Avenue, on the western boundary of the site. Several trees are situated along the western boundary of Site F.

For the purposes of this Planning Proposal, Sites A through E are referred to as Site 1, and Site F (the service station) is referred to as Site 2.

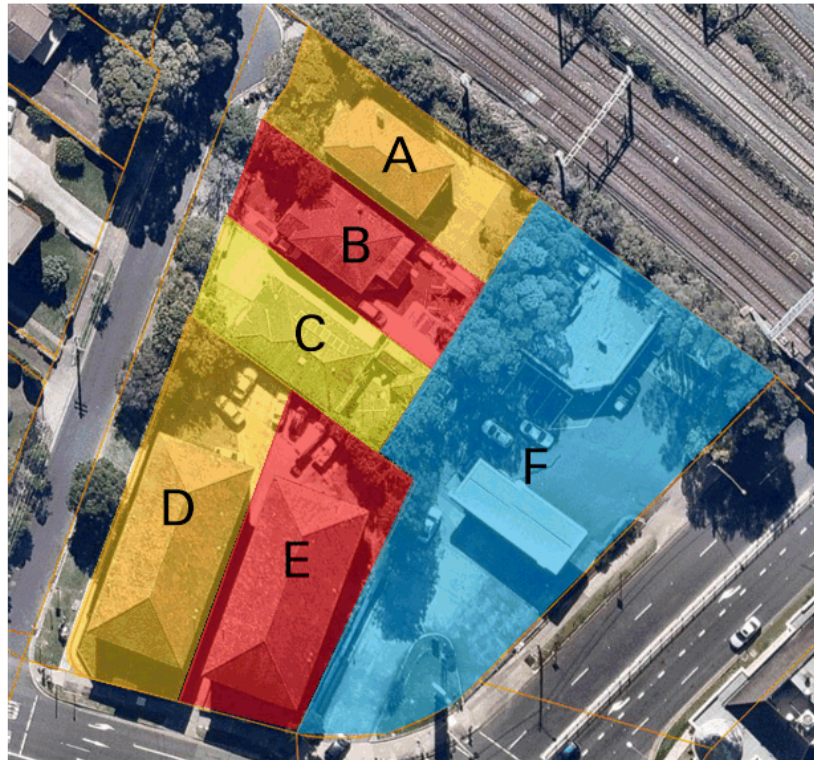


Figure 9 – The subject site contains five separate lots
Source: NearMap

3.3 Surrounding Development

The surrounding locality comprises a mix of land uses. Further to the east of the Shell Service Station is the Sandalwood Apartments residential tower (approximately 18 storeys high) and the Strathfield Railway Station approximately 200m from the site.

As highlighted earlier in this report, the Strathfield Railway Station is a key interchange on the Sydney Trains rail network with a number of key rail services stopping at the station including the T1 North Shore, Northern and Western Line and the T2 Airport, Inner West and South Line provide frequent services from the station into wider Sydney.

Across Albert Road to the immediate south of the site is a mix of single and double storey detached residential dwellings, with a new residential tower development on the corner currently under construction, to be approximately 11 storeys.

The Regal Court mixed use development (approximately 18 storeys high) is located to the site's south-east and comprises a number of smaller ground level retail shops with residential above. Further to the south-east is the Strathfield Plaza shopping complex and a mix of commercial and residential buildings.

West of the site across Pilgrim Avenue are a number of older style 1970's residential flat buildings that vary between two to four storeys in height. The higher residential unit blocks generally front Elva Street, and overlook the rail corridor. Further to the west the height of the residential flat buildings along Elva Street increases to approximately 10 storeys.

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The site overlooks the rail corridor to the north. Additional residential areas, within the Canada Bay LGA, are located to the north-east, and generally comprise single storey detached dwellings. A light industrial and warehouse precinct is located approximately 400m to the north-west of the site.



- The Site
- Existing Sydney Train Lines (T1, T2)

Figure 10 – Site and surrounding context
 Source: JBA

3.4 Land Use Capability

Our analysis of the capability of the physical attributes and context of the site to support redevelopment for the purposes of a mix of residential and non-residential uses is summarised in **Table 5**.

Table 5 – Land use capability

Site Attributes	Capability for a medium density mixed residential and commercial redevelopment
Urban and subregional context	The site's urban and subregional context is defined by its location and proximity to the Strathfield Town Centre and Railway Station, which underpin its clear potential for high density residential and mixed-use development. The current and proposed future redevelopment of this area will reinforce Strathfield town centres role as the primary centre in the LGA and will maximise its contributions to the delivery of future housing to meet the forecast population and dwelling growth outlined in A Plan for Growing Sydney.
Surrounding properties	Subject to an appropriate urban design response, the proposed use and density is compatible with the properties and built form that surrounds and adjoins the site, particularly those existing developments in the Strathfield Town Centre and current construction occurring to the south of Albert Road.
Land size and topography	The topography of the site lends itself to achieving a future development outcome as illustrated in the indicative concept plans. The site is of a sufficient size capable of supporting development of a high density and scale which provides a transition from the northern Strathfield Triangle to the Strathfield Town Centre in the east.
Existing vegetation	Minimal vegetation is located on the site at present, with existing vegetation generally being planted as part of residential landscaping. New landscaping and vegetation will be provided as part of any new mixed-use development and will be above and beyond that which presently exists.
Existing buildings and infrastructure	The site contains two double storey unit blocks and three single storey detached dwellings, fronting both Albert Road and Pilgrim Avenue. Existing infrastructure is available to service the future development and further investigations will be undertaken post 'Gateway Determination' to identify if any upgrades are required.
Access and transport	The site currently has direct road access from both Pilgrim Avenue and Albert Road, although the concept plan seeks to relocate road access into the site from Pilgrim Avenue only. Public transport is available in the form of frequent rail services from the nearby Strathfield Railway Station, and bus services (routes 407, 408, 480 and 483) along Albert Road. The surrounding road network is capable of supporting the type of development proposed.
Heritage significance	No items of heritage significance are located on the site. The site is located adjacent to the State Heritage Register listed Strathfield rail under bridges, to the north-east, however future redevelopment of the site is highly unlikely have any adverse impact on this.
Contamination	The site is presently being used for residential purposes, adjacent to a working rail corridor. This proximity to the rail corridor may introduce potential for relatively low level contaminants. Further investigations can be undertaken following the gateway determination if required.
Stormwater and flood risk	Currently there is no prepared flood study for the area however Strathfield Council has recently received funding for a flood study for the Powells Creek and Saleyards Creek. If necessary, flood analysis is able to be prepared following gateway determination or prior to any development application or consent being granted.

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3.5 Current Planning Controls

The Strathfield LEP is the primary environmental planning instrument applying to the site. The current planning controls that are applicable to the site are set out below.

Land Use Zoning

The site is zoned B4 Mixed Use under Strathfield LEP 2012. It is not proposed to change the zoning of the land. The current zoning is shown in **Figure 11**.

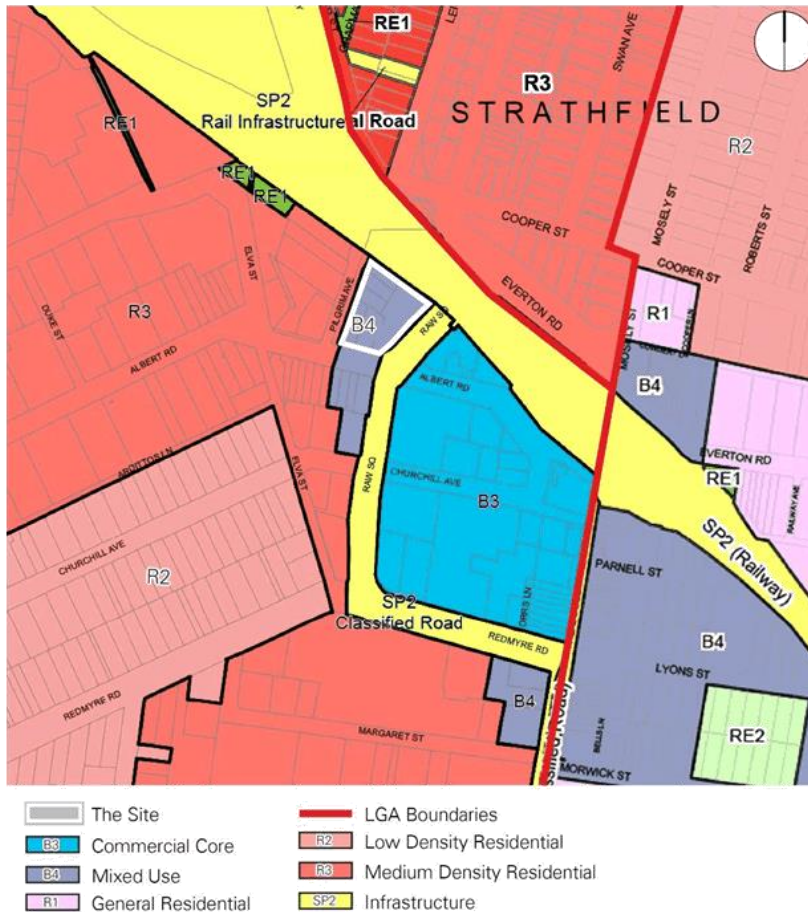


Figure 11 - Strathfield LEP Zoning Map

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Building Height

The site has a current height limit of 35m applied under the Strathfield LEP, as shown in Figure 12.

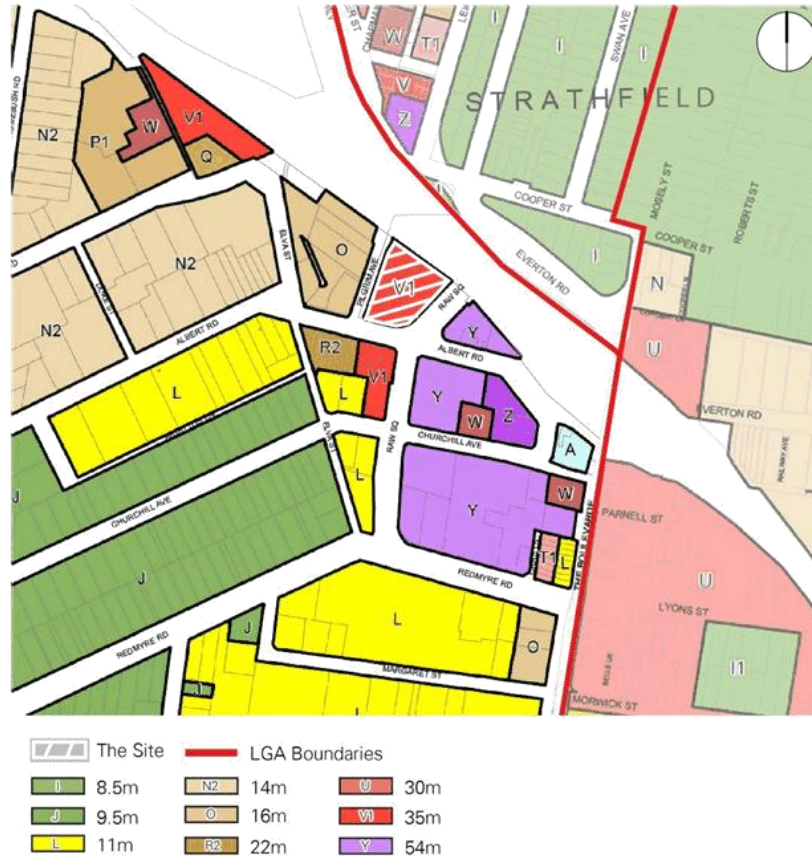


Figure 12 - Strathfield LEP Building Height Map

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Floor Space Ratio

The site has a maximum floor space ratio of 3.5:1 applied under the Strathfield LEP, as shown in **Figure 13**.

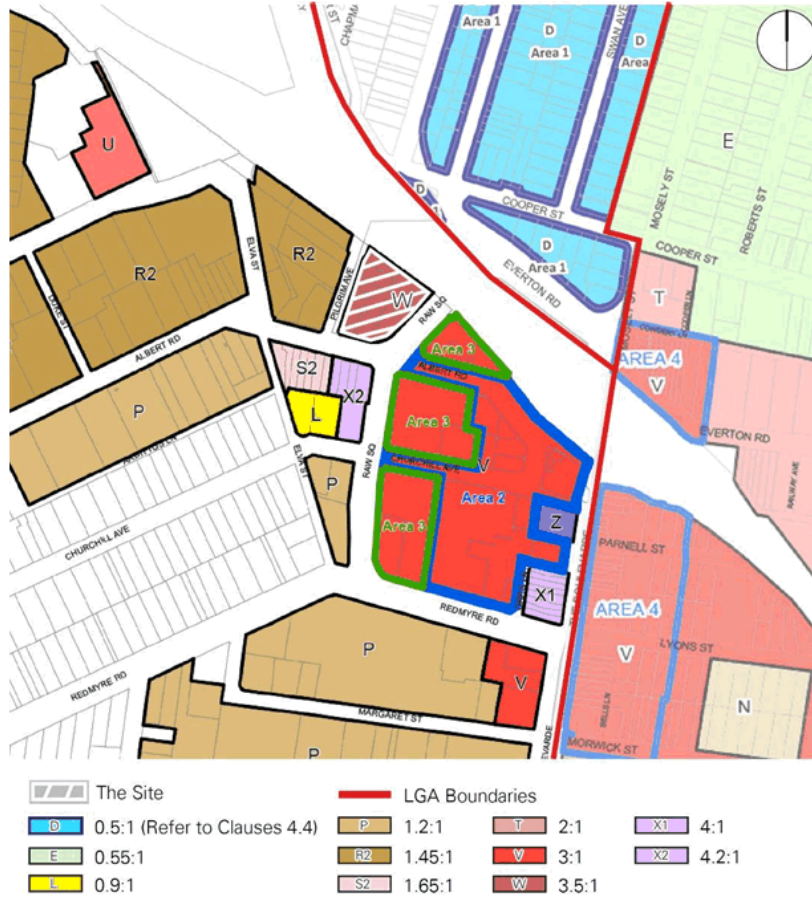


Figure 13 – Strathfield LEP FSR Map

4.0 Indicative Scheme

4.1 Overview

Taking into consideration the site-specific opportunities and constraints including but not limited to its locational attributes, strategic planning policy and the surrounding built form; a number of planning and design principles were established to guide and inform how the site may be redeveloped in the future under the proposed planning controls. Specifically, it was established that any future redevelopment of the site was to:

- Improve the site's frontage and contribution to the Strathfield Town Centre, recognising and responding to the form and character established by the existing tall buildings;
- Contribute to the local housing stock with the provision of a range of dwellings of different sizes;
- Incorporate buildings of varying form and heights that facilitate an appropriate built form outcome that responds to the opportunity presented by the site, whilst minimising overshadowing and view impacts to the west;
- Deliver a high-quality environment for residents in accordance with SEPP 65;
- Be designed to comply with the Apartment Design Guide, particularly with regard to building separation, landscape open space, deep soil planting, solar access, cross ventilation and apartments sizes;
- Deliver a high quality built form and design outcome that complements the desired future modern character of the centre;
- Ensure the future development rights of adjacent sites are not compromised by the proposed development;
- Continue to provide a number of permanent jobs on the site through some ground floor non-residential uses; and
- Ensure minimal adverse environmental and amenity impacts on the existing surrounding buildings.

4.2 Description of Indicative Scheme

Using the above principles, Kennedy Associates Architects have prepared an indicative scheme for the site (**Appendix A** and **Figure 14**) that sets out to achieve the aforementioned objectives and intended outcomes. The indicative scheme demonstrates how the site could be redeveloped in the future under the proposed height and floor space ratio controls, while preserving future development rights on the adjacent service station site. Full details of the indicative scheme are contained in the accompanying Urban Design Report at **Appendix A** however the key components of the scheme include:

- A predominant street wall height of approximately 10 storeys that wraps around the site along all street frontages;
- Building form across the subject site comprising a podium structure with two residential towers of 11 and 15 storeys primarily aligned to the Pilgrim Avenue and Raw Square frontages and act as northern and southern bookends for the site;
- A central lower rise podium level that houses the primary communal open space area characterised by landscaping and recreational pool facilities;
- Ground floor commercial/retail tenancies; and
- Basement level car parking accessed from Pilgrim Avenue and Raw Square.

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It is important to note that the indicative scheme represents a single preferred solution for how the site might be redeveloped under the proposed planning controls. It does however not represent the only possible solution to the site’s future design which would be subject to detailed analysis to ensure amenity for future residents.

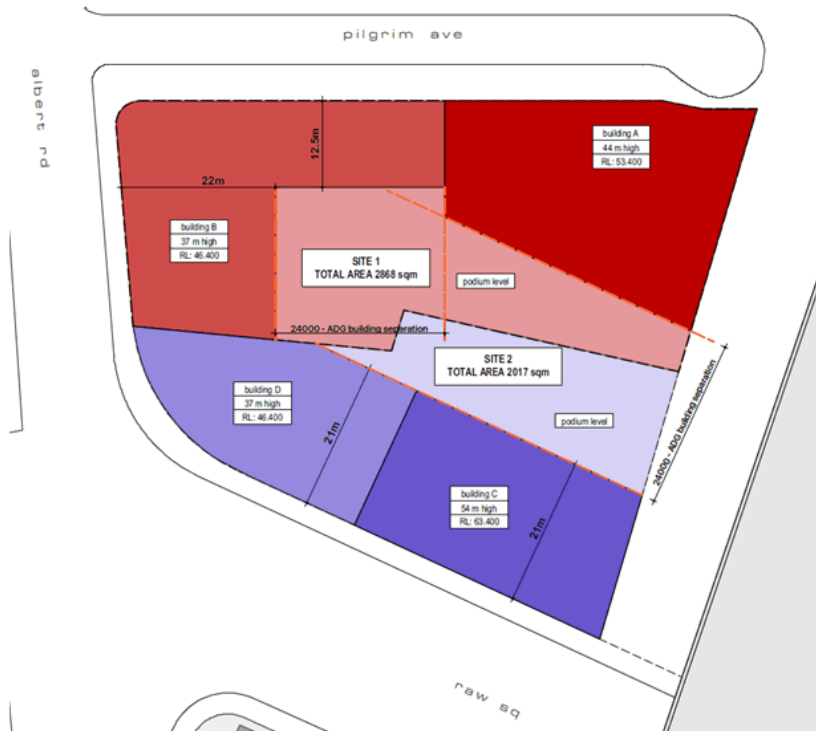


Figure 14 – Indicative layout scheme of the subject site and adjoining site

Indicative Scheme – Key Development Statistics

Key development information is summarised in Table 6.

Table 6 – Key development information for the indicative scheme

Component	Detail
Site area	4,885m ²
Gross floor area	24,077m ²
Floor space ratio	4.95:1
Heights	Building A – 12 storeys (44m) Building B – 10 storeys (37m) Building C – 15 storeys (54m) Building D – 10 storeys (37m)
Car spaces	351 spaces on Site 1 and 228 on Site 2.
Residential apartments	280 Site 1: 167 units Site 2: 113 units
Commercial floor space	1,358m ²

Built Form

A key priority for the indicative scheme was to ensure that the built form appropriately responded to the scale and height of the residential buildings to the east of the site, and the recently completed 11 storey development to the south, whilst also ensuring the development rights of the adjacent service station site were maintained.

As shown in **Figure 16** this has been achieved by locating the taller residential building adjacent to the established rail corridor along the Pilgrim Avenue frontage, with heights stepping down towards the Albert Road frontage, respecting the lower storeys to the west and south-west of the site. This proposed height distribution reflects the requirements of the JRPP determination, with a focus on increased height closest to the Strathfield Railway Station, stepping down towards the western residential area.

When developed in conjunction with potential built form on the neighbouring service station site, the development forms a 'U' shape, with the open end towards the rail corridor. This allows for overshadowing to be evenly distributed across dwellings to the south, however ensures that at least 2 hours of sunlight is still obtainable for those properties.



Figure 15 – The proposed height distribution across the site
Source: Kennedy Associates

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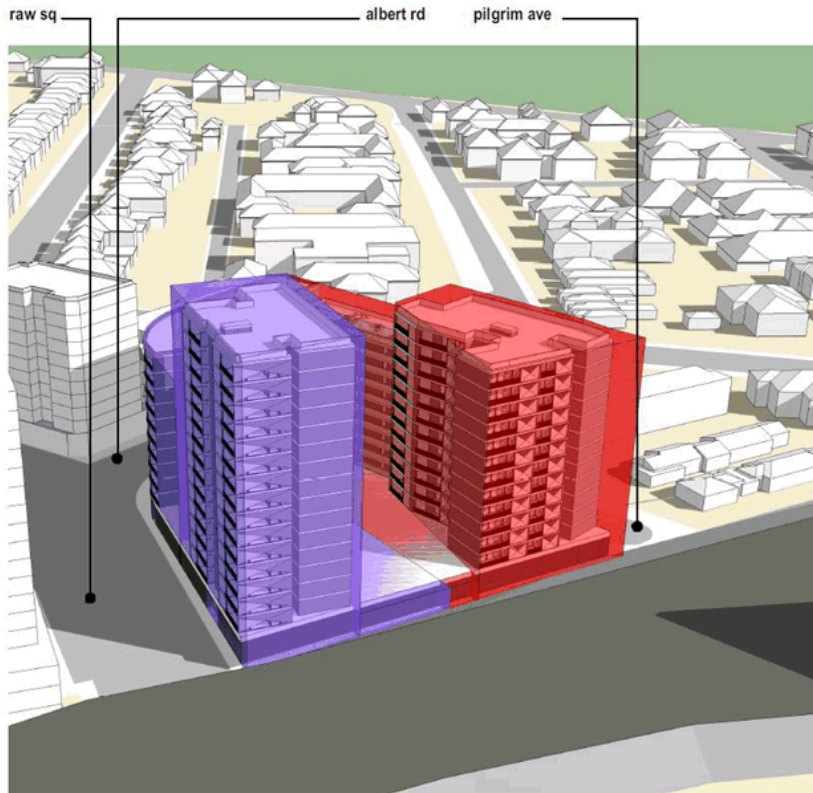


Figure 16 – The height distribution focuses the tallest buildings towards the railway corridor
 Source: Kennedy Associates

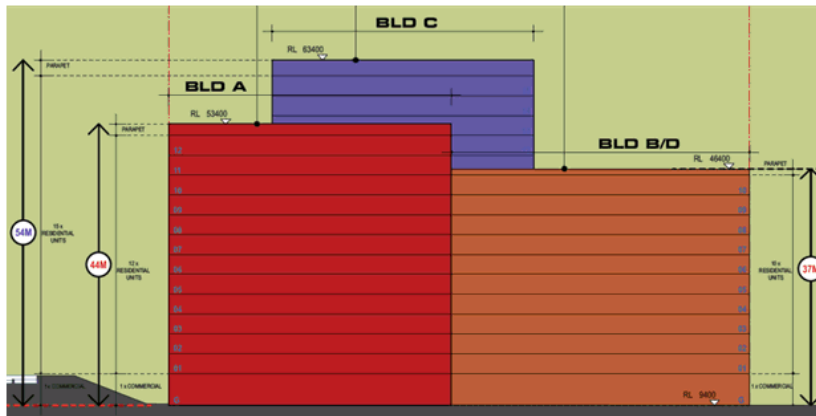


Figure 17 – Elevation showing the proposed relationship of the Indicative scheme
 Source: Kennedy Associates

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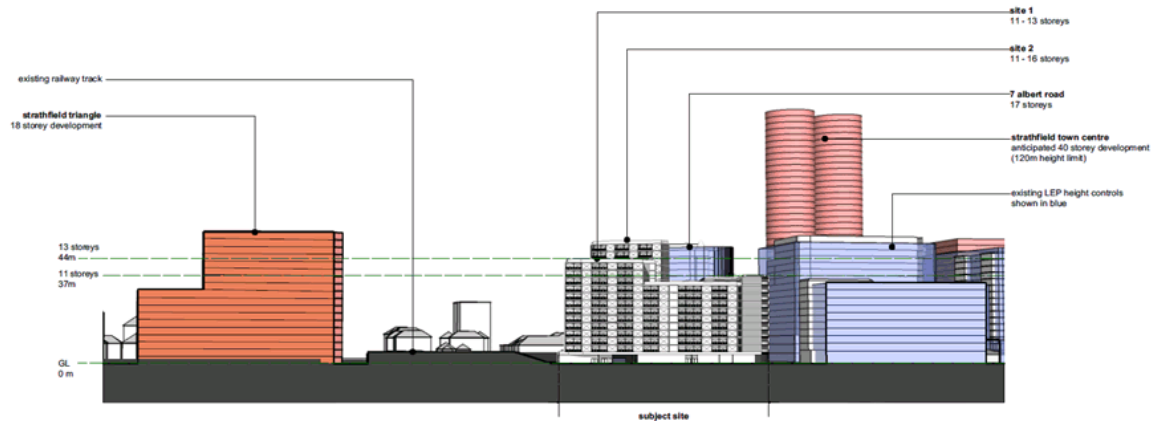


Figure 18 – Perspective showing the proposed relationship of the indicative scheme to the Strathfield Town Centre and Strathfield Triangle
 Source: Kennedy Associates

Apartment Design Guide

Table 7 lists the relevant ADG 'Rules of Thumb' and assesses the Indicative schemes consistency with those standards. The assessment demonstrates that the indicative scheme complies with the majority of the 'Rules of Thumb' and that the scheme is capable of providing a high standard of amenity for future residents. Where departures are proposed to the 'Rules of Thumb' they are discussed in further detail below the table.

Table 7 – Assessment against the relevant Design Criteria in the ADG

Design Criteria	Proposal
Part 3 Siting the Development	
3D Communal and Public Open Space	
<i>Objective</i> An adequate area of communal open space is provided to enhance residential amenity and to provide opportunities for landscaping	✓
<i>Design Criteria</i> Communal open space has a minimum area equal to 25% of the site	✓ Can comply – the area of communal open space will be confirmed during the DA stage.
Developments achieve a minimum of 50% direct sunlight to the principal usable part of the communal open space for a minimum of 2 hours between 9 am and 3 pm on 21 June (mid-winter)	Can comply (during detailed design at the DA stage)
3E Deep Soil Zones	
<i>Objective</i> Deep soil zones provide areas on the site that allow for and support healthy plant and tree growth. They improve residential amenity and promote management of water and air quality.	A proportion of the site will be confirmed as deep soil zone during the detailed design phase. Given the sites location in a highly-urbanised town centre environment, it is considered unrealistic for significant areas of deep soil zone.

Design Criteria	Proposal															
<p><i>Design Criteria</i> Deep soil zones are to meet the following minimum requirements:</p> <table border="1"> <thead> <tr> <th>Site Area</th> <th>Minimum Dimensions</th> <th>Deep Soil Zone (% of site area)</th> </tr> </thead> <tbody> <tr> <td>Less than 650m²</td> <td>-</td> <td>7%</td> </tr> <tr> <td>650m² – 1,500m²</td> <td>3m</td> <td></td> </tr> <tr> <td>Greater than 1,500m²</td> <td>6m</td> <td></td> </tr> <tr> <td>Greater than 1,500m² with significant existing tree cover</td> <td>6m</td> <td></td> </tr> </tbody> </table>	Site Area	Minimum Dimensions	Deep Soil Zone (% of site area)	Less than 650m ²	-	7%	650m ² – 1,500m ²	3m		Greater than 1,500m ²	6m		Greater than 1,500m ² with significant existing tree cover	6m		As above, details of deep soil zones will be confirmed during the detailed design stage.
Site Area	Minimum Dimensions	Deep Soil Zone (% of site area)														
Less than 650m ²	-	7%														
650m ² – 1,500m ²	3m															
Greater than 1,500m ²	6m															
Greater than 1,500m ² with significant existing tree cover	6m															
3F Visual Privacy																
<p><i>Objective</i> Adequate building separation distances are shared equitably between neighbouring sites, to achieve reasonable levels of external and internal visual privacy.</p>	✓ Appropriate separation between apartments and buildings has been provided.															
<p><i>Design Criteria</i> Separation between windows and balconies is provided to ensure visual privacy is achieved. Minimum required separation distances from buildings to the side and rear boundaries are as follows:</p> <table border="1"> <thead> <tr> <th>Building Height</th> <th>Habitable rooms and balconies</th> <th>Non-habitable rooms</th> </tr> </thead> <tbody> <tr> <td>Up to 12m (4 storeys)</td> <td>6m</td> <td>3m</td> </tr> <tr> <td>Up to 25m (5-8 storeys)</td> <td>9m</td> <td>4.5m</td> </tr> <tr> <td>Over 25m (9+ storeys)</td> <td>12m</td> <td>6m</td> </tr> </tbody> </table>	Building Height	Habitable rooms and balconies	Non-habitable rooms	Up to 12m (4 storeys)	6m	3m	Up to 25m (5-8 storeys)	9m	4.5m	Over 25m (9+ storeys)	12m	6m	✓			
Building Height	Habitable rooms and balconies	Non-habitable rooms														
Up to 12m (4 storeys)	6m	3m														
Up to 25m (5-8 storeys)	9m	4.5m														
Over 25m (9+ storeys)	12m	6m														
3K Bicycle and Car Parking																
<p><i>Objective</i> Car Parking is provided based on proximity to public transport in metropolitan Sydney and centres in regional areas</p>	✓															
<p><i>Design Criteria</i> For development in the following locations:</p> <ul style="list-style-type: none"> ▪ on sites that are within 800 metres of a railway station or light rail stop in the Sydney Metropolitan Area; or ▪ on land zoned, and sites within 400 metres of land zoned, B3 Commercial Core, B4 Mixed Use or equivalent in a nominated regional centre <p>The minimum car parking requirement for residents and visitors is set out in the Guide to Traffic Generating Developments, or the car parking requirement prescribed by the relevant council, whichever is less. The car parking needs for a development must be provided off street.</p>	✓ There are 351 car spaces provided on Site 1 and 228 on Site 2 under the indicative scheme.															
Part 4 Designing the Buildings																
4A Solar and Daylight access																
<p><i>Objective</i> To optimise the number of apartments receiving sunlight to habitable rooms, primary windows and private open space</p>	✓															
<p><i>Design Criteria</i> Living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 2 hours direct sunlight between 9 am and 3 pm at mid winter in the Sydney Metropolitan Area and in the Newcastle and Wollongong local government areas.</p>	✓ 74.2% for Site 1 and 77% for Site 2															
<p>In all other areas, living rooms and private open spaces of at least 70% of apartments in a building receive a minimum of 3 hours direct sunlight between 9 am and 3 pm at mid winter.</p>	✓ Can comply subject to detailed design															
<p>A maximum of 15% of apartments in a building receive no direct sunlight between 9 am and 3 pm at mid winter.</p>	✓ 10.2% for Site 1 and 15% for Site 2															
4B Natural Ventilation																
<p><i>Objective</i></p>	✓															

Design Criteria	Proposal										
The number of apartments with natural cross ventilation is maximised to create a comfortable indoor environment for residents	The indicative scheme generally complies, with units being less than 18m in depth. Natural ventilation through units will be achieved during detailed design.										
<i>Design Criteria</i> At least 60% of apartments are naturally cross ventilated in the first nine storeys of the building. Apartments at ten storeys or greater are deemed to be cross ventilated only if any enclosure of the balconies at these levels allows adequate natural ventilation and cannot be fully enclosed.	✓ 62.5% for Site 1 and 66.6% for Site 2.										
Overall depth of a cross-over or cross-through apartment does not exceed 18m, measured glass line to glass line.	Details will be provided at the DA stage, however the development will comply										
4C Ceiling Height											
<i>Objective</i> Ceiling height achieves sufficient natural ventilation and daylight access	✓										
<i>Design Criteria</i> Measured from finished floor level to finished ceiling level, minimum ceiling heights are: Minimum ceiling height Habitable rooms 2.7m Non-habitable 2.4m For 2 storey apartments 2.7m for main living area floor 2.4m for second floor, where its area does not exceed 50% of the apartment area Attic spaces 1.8m at edge of room with a 30 degree minimum ceiling slope If located in mixed use areas 3.3m for ground and first floor to promote future flexibility of use These minimums do not preclude higher ceilings if desired.	✓ The development will comply with detail provided at the DA stage.										
4D Apartment Size and Layout											
<i>Objective</i> The layout of rooms within an apartment is functional, well organised and provides a high standard of amenity	✓ Apartments have been designed to generally comply with this requirement. Further detail design of the indicative scheme will be carried out prior to a DA being lodged.										
<i>Design Criteria</i> Apartments are required to have the following minimum internal areas: <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">Apartment Type</th> <th style="text-align: left;">Minimum internal area</th> </tr> </thead> <tbody> <tr> <td>Studio</td> <td>35m²</td> </tr> <tr> <td>1 bedroom</td> <td>50m²</td> </tr> <tr> <td>2 bedroom</td> <td>70m²</td> </tr> <tr> <td>3 bedroom</td> <td>90m²</td> </tr> </tbody> </table> The minimum internal areas include only one bathroom. Additional bathrooms increase the minimum internal area by 5m ² each. A fourth bedroom and further additional bedrooms increase the minimum internal area by 12m ² each.	Apartment Type	Minimum internal area	Studio	35m ²	1 bedroom	50m ²	2 bedroom	70m ²	3 bedroom	90m ²	✓ Apartment sizes within the indicative scheme comply with the minimum ADG requirements
Apartment Type	Minimum internal area										
Studio	35m ²										
1 bedroom	50m ²										
2 bedroom	70m ²										
3 bedroom	90m ²										

Design Criteria	Proposal															
Every habitable room must have a window in an external wall with a total minimum glass area of not less than 10% of the floor area of the room. Daylight and air may not be borrowed from other rooms.	✓															
<i>Objective</i> Environmental performance of the apartment is maximised	✓															
<i>Design Criteria</i> Habitable room depths are limited to a maximum of 2.5 x the ceiling height. In open plan layouts (where the living, dining and kitchen are combined) the maximum habitable room depth is 8m from a window.	Apartments have been designed to generally comply with this requirement. Further detail design of the indicative scheme will be carried out prior to a DA being lodged.															
<i>Objective</i> Apartment layouts are designed to accommodate a variety of household activities and needs	✓															
<i>Design Criteria</i> Master bedrooms have a minimum area of 10m ² and other bedrooms 9m ² (excluding wardrobe space). Bedrooms have a minimum dimension of 3m (excluding wardrobe space). Living rooms or combined living/dining rooms have a minimum width of: <ul style="list-style-type: none"> ▪ 3.6m for studio and 1 bedroom apartments ▪ 4m for 2 and 3 bedroom apartments ▪ The width of cross-over or cross-through apartments are at least 4m internally to avoid deep narrow apartment layouts. 	Bedroom and living space dimensions and areas will be confirmed during detailed design, however the development will comply.															
4E Private Open Space and Balconies																
<i>Objectives</i> Apartments provide appropriately sized private open space and balconies to enhance residential amenity	✓ The indicative scheme provides private open space for apartments through the use of balconies.															
<i>Design Criteria</i> All apartments are required to have primary balconies as follows: <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Dwelling Type</th> <th>Minimum Area</th> <th>Minimum internal area</th> </tr> </thead> <tbody> <tr> <td>Studio apartment</td> <td>4m²</td> <td>-</td> </tr> <tr> <td>1 bedroom apartment</td> <td>8m²</td> <td>2m</td> </tr> <tr> <td>2 bedroom apartment</td> <td>10m²</td> <td>2m</td> </tr> <tr> <td>3+ bedroom apartment</td> <td>12m²</td> <td>2.4m</td> </tr> </tbody> </table> The minimum balcony depth to be counted as contributing to the balcony area is 1m. For apartments at ground level or on a podium or similar structure, a private open space is provided instead of a balcony. It must have a minimum area of 15m ² and a minimum depth of 3m.	Dwelling Type	Minimum Area	Minimum internal area	Studio apartment	4m ²	-	1 bedroom apartment	8m ²	2m	2 bedroom apartment	10m ²	2m	3+ bedroom apartment	12m ²	2.4m	The indicative scheme indicates general compliance. Details will be provided at the DA stage.
Dwelling Type	Minimum Area	Minimum internal area														
Studio apartment	4m ²	-														
1 bedroom apartment	8m ²	2m														
2 bedroom apartment	10m ²	2m														
3+ bedroom apartment	12m ²	2.4m														
<i>Objective</i> Common circulation spaces achieve good amenity and properly service the number of apartments	✓															
<i>Design Criteria</i> The maximum number of apartments off a circulation core on a single level is eight.	✓ There are multiple cores proposed with eight apartments off each core. Detail will be provided at the DA stage.															
For buildings of 10 storeys and over, the maximum number of apartments sharing a single lift is 40.	✓ There are multiple cores proposed.															
4G Storage																

Design Criteria	Proposal										
<p><i>Objective</i> Adequate, well designed storage is provided in each apartment</p>	<p>Details will be provided at the DA stage. The scheme is capable of complying.</p>										
<p><i>Design Criteria</i> In addition to storage in kitchens, bathrooms and bedrooms, the following storage is provided:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">Dwelling Type</th> <th style="text-align: left;">Minimum Area</th> </tr> </thead> <tbody> <tr> <td>Studio apartment</td> <td>4m²</td> </tr> <tr> <td>1 bedroom apartment</td> <td>6m²</td> </tr> <tr> <td>2 bedroom apartment</td> <td>8m²</td> </tr> <tr> <td>3+ bedroom apartment</td> <td>10m²</td> </tr> </tbody> </table> <p>At least 50% of the required storage is to be located within the apartment.</p>	Dwelling Type	Minimum Area	Studio apartment	4m ²	1 bedroom apartment	6m ²	2 bedroom apartment	8m ²	3+ bedroom apartment	10m ²	<p>The indicative scheme is capable of complying with this requirement. Further design development will be undertaken at DA stage to ensure this is achieved.</p>
Dwelling Type	Minimum Area										
Studio apartment	4m ²										
1 bedroom apartment	6m ²										
2 bedroom apartment	8m ²										
3+ bedroom apartment	10m ²										

Dwelling Mix

The indicative scheme illustrates that the entire site has the potential to accommodate approximately 280 new dwellings comprising a mix of types and sizes, these being:

- 51 x 1 bedroom units;
- 212 x 2 bedroom units; and
- 18 x 3 bedroom units.

The DCP does not identify a required dwelling mix. Notwithstanding this the proposal will still provide an acceptable mix that meets the objectives of the ADG as it will provide a diversity of apartments which cater to differing household needs both now and in the future.

Solar Access and Cross Ventilation

Indicative modelling of the proposal identifies that 74.4% of units (125 of 168) of Site 1 can achieve at least two hours of direct sunlight and 77% for Site 2. This has been modelled on an indicative design for the service station site as any design on that property would be the responsibility of the owner.

Cross ventilation is achievable to 62.5% of units on Site 1 and 66.6% on Site 2.

Landscape

The indicative scheme provides a general landscape concept design that illustrates how landscaping would be provided across the site. The landscape concept seeks to minimise the amount of hardscape within the site and provides private and communal open space areas that have been treated with plantings to soften the built form, maintain solar access and to offer high levels of amenity to future occupants.

Car parking

The indicative scheme indicates the capability for basement parking, with additional parking located on the ground floor behind the commercial premises fronting the street. In total, the scheme provides an indicative 351 car spaces on Site 1 and 228 on Site 2.

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Potential commercial component

The indicative scheme identifies commercial floorspace along the ground floor fronting Albert Road to present towards the Strathfield Town Centre. This is a deliberate design measure to facilitate street activation and provide ground floor uses that complement and support the town centre.

Summary

The indicative concept scheme illustrates that a high-quality design outcome is able to be achieved under the proposed planning controls. In particular, the indicative scheme demonstrates that a FSR of 5:1 and a maximum building height of circa 54m will sit comfortably within its surrounding context and will not result in an unacceptable adverse impact on the local area. Furthermore, it also illustrates that the apartments within the indicative scheme will support a high level of residential amenity in accordance with SEPP65, the ADG and the Strathfield DCP 2005.

5.0 Objectives and Intended Outcomes

The objectives and intended outcomes of the Planning Proposal are to:

- Facilitate the achievement and realisation of the Goals, Directions and Actions of A Plan for Growing Sydney, the draft Greater Sydney Region Plan and draft Eastern City District Plan, in particular maximising the delivery of new housing on strategically well located sites that are close to jobs and well serviced by infrastructure, transport, education, recreation and community facility facilities;
- Provide a variety of different housing typologies and products to meet the varying needs of the community, and improve housing choice and affordability in the local area;
- Provide planning controls that will enable the site to realise its full development potential whilst ensuring minimal impact on the surrounding area;
- Maximise the Strathfield Town Centre's potential for transit-oriented development, and minimise the need for land use intensification in less sustainable locations that rely more heavily on private motor vehicles and the sites that are less capable of accommodating increased density;
- Create an urbanised built form environment that reinforces Strathfield Town Centre's role as the primary focal point in the LGA, and which delivers a design outcome that better relates to the Strathfield Triangle and the Columbia Precinct;
- Provide the opportunity to retain a portion of non-residential land uses within the site;
- Facilitate the achievement of a high-quality design outcome that reflects the site's strategic location; and
- Generate investment in the construction sector.

6.0 Explanation of Provisions

This section provides an explanation of the provisions proposed to apply to the subject land under the Strathfield LEP 2012.

6.1 Strathfield LEP 2012

The site falls within the B4 Mixed Use land zoning under the Strathfield LEP 2012, which is proposed to remain unchanged. The following provisions are proposed to apply to the site in the Strathfield LEP 2012 under this Planning Proposal.

6.1.1 Land to which the Plan will apply

The Planning Proposal applies to the site known as 2-4 Pilgrim Avenue and 11-13 Albert Road, Strathfield, legally described as SP8785, Lot 9 DP15917, Lot 8 DP15917, Lot A DP100558, Lot B DP100558 and Lot 100 DP807807.

6.1.2 Maximum Building Height

The Planning Proposal is to amend the Strathfield LEP 2012 Building Height Map as follows:

- **Current** – A maximum building height of 35m currently applies to the site
- **Proposed** – A maximum building height of 54m is proposed to apply to the site

6.1.3 Maximum Floor Space Ratio

The Planning Proposal is to amend the Strathfield LEP 2012 Floor Space Ratio map as follows:

- **Current** – A floor space ratio of 3.5:1 currently applies to the site
- **Proposed** – A floor space ratio of 5:1 is proposed to apply to the site

It should be noted that these controls will apply to the whole site. Specific controls around setbacks could be imposed through a Stage 1 DA, but do not form part of this Planning Proposal.

7.0 Strategic and Statutory Framework

This chapter outlines the strategic and statutory planning framework within which the development outcomes for the land have been considered and provides commentary on how the proposal responds to each of these documents.

7.1 Strategic planning policies

7.1.1 A Plan for Growing Sydney

A Plan for Growing Sydney is the current strategic plan for the Sydney metropolitan area. Having been recently published in December 2014, it represents the most up to date strategic framework and sets out the Government’s vision for Sydney as a strong global city and a great place to live. To achieve this vision, it sets out four overarching goals for the region, these being:

- A competitive economy with world-class services and transport;
- A city of housing choice with homes that meet our needs and lifestyles;
- A great place to live with communities that are strong, healthy and well connected; and
- A sustainable and resilient city that protects the natural environment and has a balanced approach to the use of land and resources.

A series of more detailed directions and actions provide the framework for realising the goals and overall vision. These are discussed in further detail below.

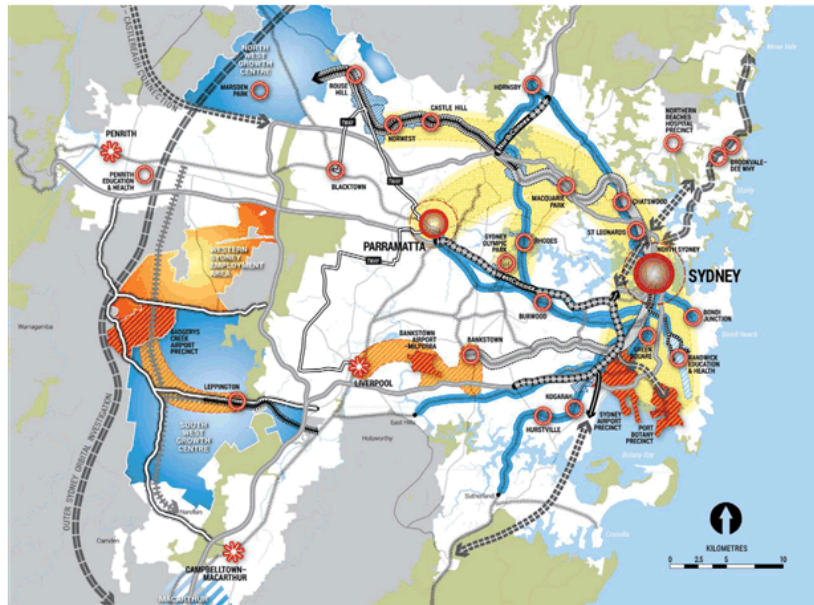


Figure 19 – A Plan for Growing Sydney, NSW Government 2014

Goal 1 – A competitive economy with world class services and transport

To ensure that Sydney has a competitive economy with world class services and transport the Plan sets out a number of key priorities including but not limited to:

- Creating new and innovative opportunities for growing and expanding the Sydney CBD office space;
- Diversifying the CBD by enhancing the cultural ribbon that surrounds the CBD including Barangaroo, Darling Harbour, Walsh Bay and the Bays Precinct;
- Growing greater Parramatta as Sydney's second CBD;
- Transforming the productivity of Western Sydney through growth and investment
- Enhancing the capacity of Sydney's gateways and freight networks;
- Expanding the global economic corridor; and
- Growing strategic centres and providing more jobs closer to home.

By carrying out the above, The Plan seeks to support and ensure that Sydney will continue to be a premier location for global commerce, business and investment with strong ties to its region and with world class infrastructure that supports growing, efficient and innovative industries. Of particular relevance to this Planning Proposal is Direction 1.7 which seeks to *'Grow strategic centres – providing more jobs closer to home.'*

The site is located 200m from the Strathfield Railway Station and forms part of the Strathfield Town Centre. In addition, it is situated in close proximity to the Burwood strategic centre, which has excellent access to jobs, education, and community facilities and services. Accordingly, the Plan notes that *"Focusing future growth in both strategic centres and transport gateways will provide the greatest benefits to Sydney in terms of land and infrastructure costs, social infrastructure and social and environmental outcomes."* Direction 1.7 also states that *"Delivering more housing through targeted renewal around centres on the transport network will provide more homes closer to jobs and boost the productivity of the city."* The Planning Proposal will therefore clearly provide an outcome that is consistent with the Plan in this regard.

Goal 2 – A city of housing choice with homes that meet our needs and lifestyles

The Plan identifies that some 664,000 additional homes need to be built over the next 20 years to meet forecast demand and highlights that *"The Government must accelerate the delivery of new housing in Sydney to meet the needs of a bigger population and to satisfy a growing demand for different types of housing"*

To achieve this, it sets out a number of strategic directions including:

- Accelerate housing supply across Sydney;
- Accelerate urban renewal across Sydney – providing homes closer to jobs;
- Improve housing choice to suit different needs and lifestyles; and
- Deliver timely and well planned greenfield precincts and housing.

Of particular relevance to this Planning Proposal is Action 2.2.2 which seeks to, *"Undertake urban renewal in transport corridors which are being transformed by investment and around strategic centres"*.

The proposal is situated adjacent to an established major public transport interchange and will provide new housing within an established urban area. The Planning Proposal will facilitate increased housing supply in the local area and in this regard, will make a significant contribution to enhancing the local economy and diversifying housing choice to meet the needs of the growing population.

Goal 3 – A great place to live with communities that are strong, healthy and connected

To create a city with strong, healthy and connected communities the plan highlights the importance of creating more vibrant places and revitalised suburbs where people want to live, and welcoming centres with character and vibrancy that offer a sense of community and belonging. The key Directions for achieving this goal include:

- Revitalise existing suburbs;
- Create a network of interlinked, multipurpose open and green spaces across Sydney;
- Create healthy built environments; and
- Promote Sydney’s heritage, arts and culture.

Of particular relevance to the Planning Proposal is Direction 3.1 which is to ‘revitalise existing suburbs’. Under this direction the Plan notes that *“research has found that new housing within Sydney’s established suburbs brings real benefits to communities and makes good social and economic sense.”*

According to the Plan directing new housing to existing urban areas will reduce the impact of development on the environment and protect productive rural land at the urban fringe. It also improves residents’ access to jobs, services and recreation which enhances the liveability of the city. The sites location in an established town centre (Strathfield) adjacent to a key public transport interchange provides the opportunity for an outcome that is directly consistent with this goal and direction.

Goal 4 – A sustainable and resilient city that protects the natural environment and has a balanced approach to the use of land and resources

The Plan notes that as the city grows, good urban design and planning will be more critical than ever to make the city’s built environment sustainable and energy efficient while also protecting the environment. To do this it sets out a number of key strategic directions, these being:

- To protect our natural environment and biodiversity;
- To build Sydney’s resilience to natural hazards; and
- To manage the impacts of development on the environment.

The above Directions are relevant to the Planning Proposal and will be supported by the sites future redevelopment as proposed, as it will:

- Promote increased density in a highly appropriate and sustainable location in close proximity to existing transport infrastructure, community facilities and jobs;
- Promote urban renewal of a well located urban block by enabling a built form outcome that respects the surrounding area, is consistent with the locality’s transitional status, and which will be capable of providing high residential amenity for future occupants;
- Will release pressure on the urban fringe and support a balanced approach to the use of land and resources by locating new housing and employment opportunities within an existing built up area; and
- Be appropriately designed in accordance with latest ESD initiatives thus minimising impacts on the environment.

Subregional Delivery

The site is included in the Central Subregion of the Plan (now the Eastern City District in the draft Greater Sydney Region Plan and draft Eastern City District Plan) which is expected to continue to play the dominant role in the economic, social and cultural life of Sydney. A Plan for Growing Sydney notes that the variety and diversity of activities and centres in the subregion will be key to creating liveable communities and ensuring that the subregion is a desirable place to live, work and visit. To achieve this, the plan sets a number of priorities for the subregion including the need to identify suitable locations for housing intensification and urban renewal particularly around Priority Precincts, established and new centres, and along key public transport corridors.

The Plan notes that it will continue to support Burwood as a strategic centre and major location for jobs and investment. The site is located in close proximity to the Burwood strategic centre thus ensuring existing and future residents will have excellent access to jobs, education, and community facilities and services.

The subregional plan under *A Plan for Growing Sydney* outlines the framework for growth, and provides future growth expectations and targets including:

- A population increase of 266,850 people to 2031; and
- The provision of 130,700 new residential dwellings by 2031.

The proposed development will support these growth targets through the provision of quality housing and renewed commercial opportunities in close proximity to existing infrastructure and services such as the Strathfield Town Centre, the Strathfield Triangle and the Strathfield Railway Station.

7.1.2 Draft Greater Sydney Region Plan

On 22 October 2017, the Greater Sydney Commission (GSC) released the Draft Greater Sydney Region Plan (draft Metropolitan Plan) for public exhibition. This Plan is a revision of A Plan for Growing Sydney and, once adopted, will become the overarching strategy for growing and shaping the Greater Sydney region. It accommodates changes in policy, trends, directions, and actions that will inform development until 2056. The vision of the draft Metropolitan Plan is framed on the creation of three-city metropolis and enhancing Greater Sydney's liveability, productivity and sustainability. This will also be supported through greater infrastructure provision and collaboration throughout the region.

To support the vision of boosting Greater Sydney's liveability, productivity and sustainability, the GSC have established ten (10) directions which establish the aspirations for Greater Sydney over the next 40 years. These include:

- A city supported by infrastructure
- A collaborative city
- A city for people
- Housing the city
- A city of great places
- A well-connected city
- Jobs and skills for the city
- A city in its landscape
- An efficient city
- A resilient city

This Planning Proposal is generally consistent with the above directions as outlined below in Table 8. Each of these directions is supported by several objectives.

Table 8 – Summary of consistency with the directions of the draft Greater Sydney Plan

Connectivity	
A city supported by infrastructure	The Planning Proposal seeks to capitalise on the nearby Strathfield Railway Station key characteristics as a transport interchange. The location of the site within 200m of rail and bus networks will reduce the sites reliance on private motor vehicles while allowing future residents easy and efficient access into greater Sydney.
A collaborative city	The Proposal will assist in the continued recognition of Strathfield as a key strategic centre associated with a transportation hub. While not identified as a Collaboration Area in the draft Plan, the site seeks to support future growth of the existing town centre, allowing for natural growth.
Housing + Great Places	
A city for people	The Proposal looks to provide active frontages with commercial and retail uses at ground level, supporting and complementing the adjacent Strathfield Town Centre. The provision of a public car park on-site will also support the local community in visiting both the Town Centre and acting as a commuter car park for users of the nearby rail network.
Housing the city	The Planning Proposal seeks to increase the permissible height and FSR on the site, allowing the site to take full advantage of its location near to a major transport interchange and provide much needed housing. Strathfield is identified in the draft Eastern City District Plan as needing to provide 3,650 dwellings in the coming years, and the Planning Proposal will contribute approximately 280 of these.
A city of great places	The site is located on the western edge of the Strathfield Town Centre which contains a variety of commercial and retail uses. The provision of active uses on the ground floor of the site through a future DA will contribute to the existing town centre.
Jobs	
A well-connected city	The site can potentially accommodate 280 new dwellings within 200m of a key transport interchange, contributing to the shift towards public transport. Strathfield Railway Station has strong connections to greater Sydney, and is strategically located to allow direct access to the Sydney and Parramatta CBDs via both rail and bus services.
Jobs and skills for the city	The site does not propose significant levels of commercial or employment generating floor space, however the provision of ground floor retail and commercial floor space will contribute to the local economy of the Strathfield Town Centre.
Landscape	
A city in its landscape	The Planning Proposal, while only seeking height and FSR, indicates that a communal open space can be provided in the central part of the site, allowing for landscaping of vegetation including shade trees. This will provide future residents with readily accessible communal space to enjoy.
An efficient city	The site will consolidate urban form on the site and given its location near a key transport interchange will reduce the need for private motor vehicle use.
A resilient city	The Planning Proposal has considered the flooding hazard associated with the site and future DAs will manage this appropriately. Refer to Section 8.3 .

Draft Eastern City District Plan

In October 2017, the Greater Sydney Commission (GSC) released the revised draft District Plans for the Greater Sydney Metropolitan Region for public exhibition and review. The draft District Plans will fill the gap between the metropolitan plan and Council's Local Environmental Plans giving effect to the metropolitan goals and planning priorities from A Plan for Growing Sydney by setting out priorities and actions for each of the six Sydney District. An assessment of the Planning

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Proposals consistency with the relevant priorities and actions is provided in **Table 9**.

The draft District Plans are structured around the GSC’s three key themes of a Productive City, a Liveable City and a Sustainable City and aims to achieve the following for Greater Sydney by 2036:

- support the generation of over 817,000 additional jobs;
- accommodate 1.74 million additional people and more than 725,000 new homes;
- increase Greater Sydney’s economic growth rate; and
- increase total economic activity by 75% to approximately \$655 billion.

The Eastern City District includes Sydney City and economic corridors to its north through the Macquarie Park and south through Sydney Airport and Port Botany to Kogarah. The site is strategically located near the economic corridor and is identified within the pocket of urban renewal area located west of Sydney Airport.

Table 9 – Summary of consistency with the priorities and actions of the draft Eastern City District Plan

Liveability	
Planning Priority E4: Fostering healthy, creative, culturally rich and socially connected communities	
Action 10	The Planning Proposal will allow for the redevelopment of an underutilised site directly adjacent to the established Strathfield Town Centre. This will contribute to the provision of a walkable place with an active street life (supported by the proposed active frontages to Albert Road and Raw Square) at a human scale. The location of the site near to the Strathfield Railway Station also allows for residents to access schools, social, health, sporting, cultural and shared facilities through the public transport network.
Planning Priority E5: Providing housing supply, choice and affordability, with access to jobs and services	
Action 15	Strathfield Council, as directed by the draft District Plan, will be required to prepare a local or district housing strategy outlining the delivery of housing targets for 5 years, 6-10 years and longer term 20-year periods. The Planning Proposal will facilitate the delivery of approximately 280 new dwellings, contributing to these targets.
Planning Priority E6: Creating and renewing great places and local centres, and respecting the District’s heritage	
Action 17	The Planning Proposal is located in an area adjacent to an established town centre, and offers potential for a highly walkable and accessible redevelopment of an inner centre site. This will improve the amenity of the town centre and its surrounds, while introducing active uses to the area, creating a community focus.
Planning Priority E11: Growing investment, business opportunities and jobs in strategic centres	
Action 37 and Action 38	While the Planning Proposal will not contribute significant jobs, it will reinvigorate the existing town centre through a new built form of high quality. This will assist with future leasing opportunities in and around the Strathfield Town Centre, and can potentially be recognised as a natural expansion of the town centre footprint. Furthermore, the provision of residential dwellings near to a transport interchange (the Strathfield Railway Station) will allow residents the option of living in Strathfield and working elsewhere, with the Harbour CBD, Parramatta and Sydney Olympic Park being within 30 minutes – contributing to the creation of a 30-minute city.

The revised draft Eastern City District Plan sets a 20-year strategic target for housing and employment growth within the Eastern City District, with a direction to “create housing capacity in the Eastern City District”, targeting 157,500 dwellings by 2036 and a short-term (5-year) housing target of 46,550 new dwellings. Approximately 3,650 of these dwellings are proposed to be delivered in the Strathfield LGA from 2016-2021.

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In relation to housing capacity, the draft Plan notes one of Strathfield Council's key actions to prepare housing strategies that address the delivery of five-year housing supply targets. The Planning Proposal will facilitate the delivery of approximately 280 new dwellings, contributing to the above targets set for the Eastern City District and Strathfield LGA.

Strathfield is in proximity to key areas of employment contained in surrounding Strategic and District centres, as shown in **Figure 21**. By enabling renewal, this can be the catalyst to creating a vibrant and rejuvenated centre that fosters a place to live, work and play and a more connected community. In line with the draft Plan, this would deliver upon the principles of the 30-minute city by locating new residents along the various rail lines from Strathfield Railway Station providing access to the following Strategic and District Centres within 30 minutes.

- Green Square / Mascot;
- Harbour CBD;
- Burwood;
- Rhodes;
- Parramatta; and
- Sydney Olympic Park.

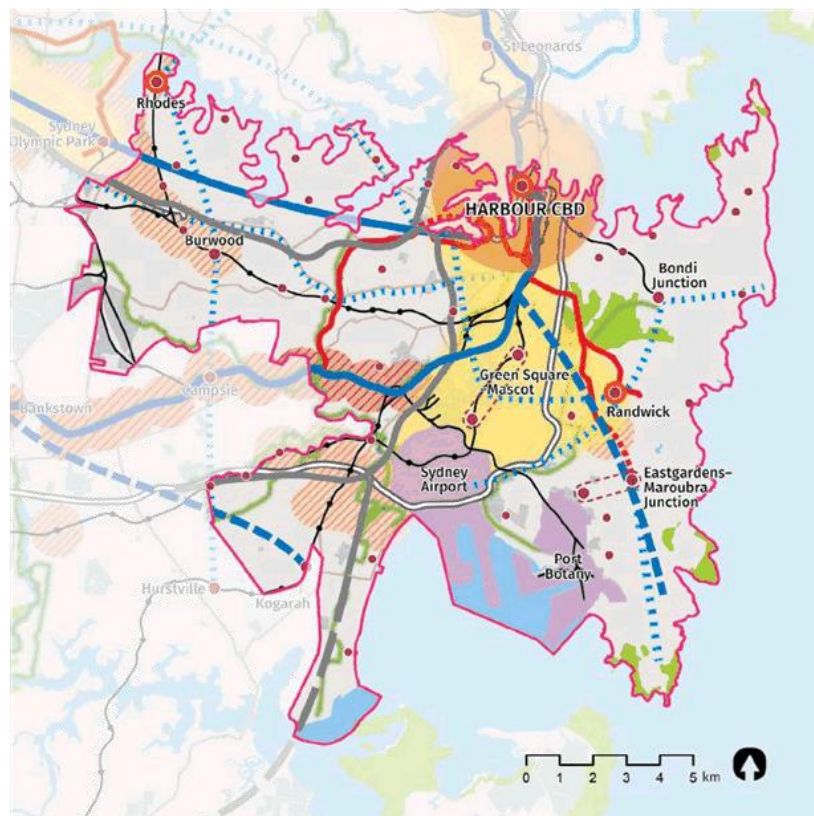


Figure 20 – Eastern City District Structure Plan
Source: Greater Sydney Commission 2017

Summary

The Planning Proposal is consistent with several key directions and actions in A Plan for Growing Sydney, the draft Greater Sydney Regional Plan and the Eastern City District Plan in that it:

- provides for development standards that reflect the site's proximity to a major public transport hub;
- facilitates the expansion of high density residential development to support the significant population growth envisaged for Eastern City District under the draft Greater Sydney Region Plan and the draft Eastern City District Plan;
- is consistent with the Plan's direction that the most suitable areas for urban renewal are those areas best connected to employment and which include centres that are close to jobs and serviced by high frequency public transport that can move large numbers of people;
- it is proposed on a site that has additional capacity for the development and which represents a logical location for increased density;
- provides for the opportunity to deliver more affordable housing, consistent with Action 2.3.3 of the Plan for Growing Sydney and Action 15 of the draft Eastern City District Plan;
- represents consistency with TOD principles by seeking to provide additional capacity around Strathfield Railway Station for additional high-density housing;
- is appropriately located in an area adjacent to a transport corridor, with high accessibility into wider Sydney;
- assists in meeting Strathfield's housing targets of 3,650 homes by 2021;
- represents consistency with the s priorities for the Eastern City District, particularly to work with Councils to identify suitable locations for housing intensification and urban renewal along key public transport corridors;
- ensures that the proposed additional levels on the site achieve a high standard of urban design and architectural excellence that will contribute to the amenity of future residents of and visitors to Strathfield; and
- proposes urban renewal of a site located within a key transport corridor, consistent with Action 2.2.2 of the Plan for Growing Sydney and the goals of the draft Greater Sydney Region Plan.

7.1.3 Strathfield Town Centre Master Plan Project

As discussed in Section 2.2.3, Strathfield Council are in the process of implementing the Strathfield Town Centre Master Plan, which will guide development to the east of the subject site. The primary purpose of the Master Plan is to apply planning controls designed to deliver the important status that the town centre requires, with Strathfield identified as the most important centre in the LGA. Refer to Section 2.2.3 for further detail.

7.2 State legislation

7.2.1 Environmental Planning and Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* (the EP&A Act) and the *Environmental Planning and Assessment Regulation 2000* (EP&A Reg) set out amongst other things:

- Requirements for rezoning land;

- Requirements regarding the preparation of a local environmental study as part of the rezoning process;
- Matters for consideration when determining a development application; and
- Approval permits and/or licences required from other authorities under other legislation.

7.2.2 State Environmental Planning Policies

The State Environmental Planning Policies directly applicable to the Planning Proposal are addressed in **Table 10** below.

Table 10 – Applicable State Environmental Planning Policies

State Environmental Planning Policies (SEPPs)	Consistent		N/A	Comment
	YES	NO		
SEPP No 1 Development Standards			✓	SEPP 1 does not apply to the Strathfield LEP 2012.
SEPP No 32 Urban Consolidation (Redevelopment of Urban Land)	✓			The planning proposal is consistent with SEPP 32 in providing for the opportunity for the development of additional housing in an area where there is existing public infrastructure, transport, and community facilities, and is close to employment, leisure and other opportunities.
SEPP No 55 Remediation of Land	✓			The site is presently used for residential dwellings and a service station, adjacent to an established rail corridor. A Preliminary Site Investigation has been undertaken which indicates that the site can be made suitable for the future intended use. Further investigations will be completed in conjunction with a future Development Application package.
SEPP No 64 Advertising and signage			✓	Not relevant to proposed amendment. May be relevant to future DAs
SEPP No 65 Design Quality of Residential Apartment Development	✓			Compliance with SEPP 65 will be demonstrated at the time of making a development application. Notwithstanding, a high-level assessment of the indicative scheme has been carried out at Section 6.0 .
SEPP No.70 Affordable Housing (Revised Schemes)			✓	Not relevant to proposed amendment.
SEPP (Affordable Rental Housing) 2009			✓	Not relevant to proposed amendment
SEPP (BASIX) 2004	✓			Compliance with SEPP (BASIX) will be demonstrated at the time of making a development application.
SEPP (Exempt and Complying Development Codes) 2008	✓			May apply to future development of the site.
SEPP (Infrastructure) 2007	✓			A preliminary traffic assessment has been undertaken (provided at Annexure C). It is proposed that a further detailed traffic assessment be undertaken as part of the Development Application stage.

7.2.3 Is the Planning Proposal consistent with applicable S.117 Ministerial Directions?

Consistency with the Ministerial Directions for LEPs under Section 117 of the Environmental Planning and Assessment Act 1979 is provided in Table 11.

Table 11 – Consistency with Ministerial Directions

No.	Title	Consistency with Planning Proposal
1. Employment and Resources		
1.1	Business and Industrial Zones	The site is located in the B4 Mixed Use Zone. The proposal does not seek to change the zoning of the site, but rather the FSR and height controls to permit a mixed-use development.
1.2	Rural Zones	Not applicable – this planning proposal does not seek to rezone from a rural land use to a residential, business, industrial, village or tourist land use
1.3	Mining, Petroleum Production and Extractive Industries	Not applicable
1.4	Oyster Aquaculture	Not applicable
1.5	Rural lands	Not applicable
2. Employment and Heritage		
2.1	Environmental Protection Zones	Not applicable
2.2	Coastal Protection	Not applicable
2.3	Heritage Conservation	Not applicable
2.4	Recreation Vehicle Areas	Not applicable
3. Housing, Infrastructure and Urban Development		
3.1	Residential Zones	The Planning Proposal is consistent with Clauses 4 and 5 of this direction. The proposed amendment will allow for the provision of a variety of additional housing in a highly appropriate location, adjacent to a key public transport interchange and an emerging town centre.
3.2	Caravan Parks and Manufactured Home Estates	Not applicable
3.3	Home Occupations	Not applicable
3.4	Integrating Land Use and Transport	The Planning Proposal is consistent with this direction and the relevant government policies that apply to the Direction. The site is well served by public transport and the proposal will maximise the use of these facilities.
3.5	Development near Licensed Aerodromes	Not applicable
3.6	Shooting Ranges	Not applicable
4. Hazard and Risk		
4.1	Acid Sulfate Soils	The site is identified as being within a Class 5 Acid Sulfate Soils zone under the LEP. The Prospect-Parramatta River Acid Sulfate Soil Risk Map identifies the site as within the map class of 'No Known Occurrence'. This considers that acid sulfate soils are not known or expected to occur and 'land management activities are not likely to be affected by acid sulfate soil materials'. As previously stated, the site is not proposing a change in land uses, only changes to permissible height and FSR controls. Refer to Appendix I.
4.2	Mine Subsidence and Unstable Lands	Not applicable
4.3	Flood Prone Land	Strathfield Council have recently received the Powells Creek and Saleyards Creek Flood Study from WMA Water (November 2016, at Appendix H). This study identifies that the site is subject to the Powells Creek catchment area, and during flood events is impacted by overland flow and the main channel in parts. However the use of appropriate freeboard levels will assist in reducing the flood risk impact to development on the site, noting that the site is already zoned for a residential purpose and this planning proposal seeks only height and FSR increases.
4.4	Planning for Bushfire Protection	Not applicable

No.	Title	Consistency with Planning Proposal
5. Regional Planning		
5.1	Implementation of Regional Strategies	Not applicable
5.2	Sydney Drinking Water Catchments	Not applicable
5.3	Farmland of State and Regional Significance on the NSW Far North Coast	Not applicable
5.4	Commercial and Retail Development along the Pacific Highway, North Coast	Not applicable
5.5	Development on the vicinity of Ellalong	Not applicable
5.6	Sydney to Canberra Corridor	Not applicable
5.7	Central Coast	Not applicable
5.8	Second Sydney Airport: Badgerys Creek	Not applicable
6. Local Plan Making		
6.1	Approval and Referral Requirements	Not applicable
6.2	Reserving land for Public Purposes	Not applicable
6.3	Site Specific Provisions	Not applicable
7. Metropolitan Planning		
7.1	Implementation of the Metropolitan Plan for Sydney 2036	The Planning Proposal will provide an outcome consistent with this Direction and the provisions of the Plan for Growing Sydney, the draft Greater Sydney Region Plan and the draft Eastern City District Plan.

7.3 Strathfield LEP 2012

7.3.1 Consistency with overall aims

The proposal’s consistency with the overall aims of the Strathfield LEP 2012 is demonstrated in **Table 12** below.

Table 12 – Consistency with overall aims of Strathfield LEP 2012

Aim	Proposal	Consistency
(a) to achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield	The proposal will facilitate delivery of housing in a highly appropriate location adjacent to public transport and the Strathfield Town Centre, consistent with the aims for Strathfield as defined in the draft Central District Plan.	✓
(b) to promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development	The proposal will provide new housing and employment opportunities that will support local business, educational, cultural and tourism activities in the local area	✓
(c) to promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community	The proposal will result in the redevelopment of a key landholding near the Strathfield Town Centre, and will ensure the centre is reinvigorated through the provision of a mix of residential and non-residential	✓
(d) to provide opportunities for economic growth that will enhance the local community	The proposal will provide opportunities for additional residential and non-residential floor space and in this regard, will support the future growth of Sydney.	✓

Aim	Proposal	Consistency
(e) to promote future development that integrates land use and transport planning, encourages public transport use, and reduces the traffic and environmental impacts of private vehicle use	The site is in a highly appropriate urban location close to an established key public transport interchange in accordance with the draft Central District Plan, and presents an opportunity to contribute to the areas future growth through provision of a diversity of housing to suit the market's needs. Detailed design will be explored at DA stage. The indicative scheme prepared by Kennedy Associates Architects demonstrates that the site can be redeveloped in a way that embodies the principles of achieving good amenity and design for future residents and the local community, particularly given the sites proximity to the Strathfield Town Centre.	✓
(f) to identify and protect environmental and cultural heritage	The proposal would not impact on environmental and cultural heritage as the site is well established in a disturbed urban area.	✓
(g) to promote opportunities for social, cultural and community activities	The proposal will facilitate the intensification of a highly accessible urban site and introduce active frontages to the site near to the existing Town Centre. This would direct residents to the Town Centre and its associated social, cultural and community activities.	✓
(h) to minimise risk to the community by identifying land subject to flooding and restricting incompatible development	The proposal would support the renewal of an established urban area with commercial uses on the ground floor and residential uses above, removing the potential risk to human life during a rainfall event. Further studies would likely be completed post-Gateway.	✓

7.3.2 Consistency with height objectives

The proposal's consistency with the relevant height objectives under the Strathfield LEP 2012 is demonstrated in Table 13 below.

Table 13 – Consistency with height objectives of Strathfield LEP 2012

Objective	Proposal	Consistency
(a) to ensure that development is of a height that is generally compatible with or which improves the appearance of the existing area	The proposal seeks to increase heights to create a suitable transition from the taller residential flat buildings to the east and to the existing dwelling housing to the west, noting the additional taller residential buildings further to the west. As demonstrated by the indicative scheme the proposed height limits will not have an adverse impact on the surrounding area, rather will focus development towards the rail corridor in the north.	✓
(b) to encourage a consolidation pattern that leads to the optimum sustainable capacity height for the area	As demonstrated in the indicative scheme, redevelopment of the site in accordance with the proposed height controls will	✓
(c) to achieve a diversity of small and large development options	facilitate an appropriate height from east to west and south to north, and proposed heights will not have an unacceptable adverse impact on amenity and views.	✓

7.3.3 Consistency with density objectives

The proposal’s consistency with the floor space ratio objectives under the Strathfield LEP 2012 is demonstrated in Table 14 below.

Table 14 – Consistency with FSR objectives of Strathfield LEP 2012

Objective	Proposal	Consistency
(a) to ensure that dwellings are in keeping with the built form character of the local area	The proposal will allow for a level of density which is appropriate for the site that reflects its strategic location near the railway station and which reinforces the higher order built form character of the town centre and its surrounds.	✓
(b) to provide consistency in the bulk and scale of new dwellings in residential areas	The proposed building bulk is commensurate with the site’s location and is integrated with the general height and scale of the immediate surrounding context.	✓
(c) to minimise the impact of new development on the amenity of adjoining properties	The proposal has been designed to reduce amenity impacts on neighbouring residential development through on-site building orientation.	✓
(d) to minimise the impact of development on heritage conservation areas and heritage items	The proposal does not impact on any heritage conservation areas or heritage items.	✓
(e) in relation to Strathfield Town Centre: (i) to encourage consolidation and a sustainable integrated land use and transport development around key public transport infrastructure, and (ii) to provide space for the strategic implementation of economic, social and cultural goals that create an active, lively and people-orientated development	The proposal consolidates development into a built form adjacent to the existing Strathfield Town Centre and Strathfield Railway Station to the east and provides a focus towards the rail corridor. The introduction of active frontages through commercial uses will enhance the town centres entrance point to the east.	✓
(f) in relation to Parramatta Road Corridor—to encourage a sustainable consolidation pattern that optimises floor space capacity in the corridor	Not applicable.	N/A

8.0 Environmental Analysis

This chapter of the report draws on the work undertaken by specialist contractors and summarises the existing environment of the site. The purpose is to provide an understanding of how the existing physical conditions and features of the land have shaped and informed the rezoning proposal and how, in turn, the rezoning proposal potentially impacts on these environmental features and conditions. The management response to any issues and impacts identified in the environmental analysis is also discussed.

8.1 Built Form and Overshadowing

The built form controls sought by this planning proposal result from a site-specific analysis involving the development and testing of several alternative design options. These were assessed in terms of their design outcomes and impacts on the surrounding area, with those less suited dismissed.

The built form illustrated in the indicative scheme and facilitated by this planning proposal therefore represents a deliberate design response to the site's surrounding context and its location adjacent to the existing rail corridor, the Strathfield Town Centre to the east and the Strathfield Triangle to the north, to establish the continuity of development through the area.

In addition to the above it should be noted that the Urban Design Report prepared by Kennedy Associates Architects also confirms that:

- The shadow footprint cast by the proposed buildings contained in the scheme will not result in any unacceptable overshadowing impacts on surrounding development;
- The additional height proposed will not result in any adverse visual impacts nor will it unreasonably restrict or block views from surrounding existing or future buildings due to the design of multiple towers of varying heights; and
- The proposed changes will support a built form outcome that recognises and responds to its current and future surroundings with its surroundings along the sites boundaries and both street frontages.

8.1.1 Overshadowing

The shadow diagrams prepared by Kennedy Associates Architects indicate that there will be no adverse impacts on solar access to neighbouring properties to the south or east of the subject site. While there is overshadowing present, the indicative scheme building orientation ensures that there is more than three hours of sunlight available to neighbouring properties. In particular, the new construction to the south of the subject site will still retain at least two hours (between 2.3 and 3.3 hours per unit between 8am and 4pm) of sunlight per day, subject to full development of the subject site (Site 1) and adjacent Shell Service Station (Site 2) at the proposed planning controls. Specifically, the solar access to that neighbouring property occurs mainly towards the middle of the day between 9am and 3pm.

In light of the above the proposed LEP height and FSR amendments are considered to be acceptable as they will allow the proposed to maximise the sites true development potential while providing greater flexibility to achieve a superior built form and public amenity outcome for the site than that of the current LEP controls.

8.2 Visual Impact

A visual impact analysis has been completed for the proposal (refer to **Appendix A**), with nine viewpoints from the surrounding neighbourhood identified. These viewpoints show that the building is appropriate in its context. It is noted that:

- Viewpoints A and B along Albert Road indicate that the site is generally screened from view due to established vegetation;
- Viewpoint C at the Elva Street and Albert Road intersection identifies the building is visible however does not overpower the existing vista provided along that corridor, rather acts as an identifiable edge of the town centre;
- Viewpoints D, E and F show the building in relation to the existing Town Centre, where it is clear that it supports the established built form by providing an end point; and
- Viewpoint H, along Leicester Avenue in the Strathfield Triangle to the north is introduced to the built form through its acting as a termination point of that view corridor.

The proposed planning control changes will result in a built form that has a minimal visual impact when seen from several key view corridors included those from lower density areas. The existence of established vegetation along the main view corridors to the site, allows for screening of the site and any future development from most view corridors, even when deciduous tree species drop their leaves. The proposed height limit for the site will not introduce an overbearing physical built form that will impact on the aesthetics of the town centre and surrounding area.

The potential built form, where seen from view corridors, acts as a termination point of that vista, and establishes a boundary for the existing town centre.

8.3 Flooding & Stormwater

The JRPP identified in the Pre-Gateway Review process that as there is no change in the permissible land uses for the site, hydraulic studies can be left to the future DA stage. However, the Gateway Determination issued by the DPE identifies that a flood study is to be provided indicating the suitability of the site for redevelopment.

Council have recently received the Powells Creek and Saleyards Creek Revised Flood Study (November 2016) prepared by WMA Water. The Flood Study is appended at **Appendix H**. The study looks at the catchment areas of Powells Creek and Saleyards Creek, with modelling completed using the TUFLOW model.

The model recognises obstructions as 'hydraulic structures' and which include buildings, fencing, bridges and assumptions of blockage during flood events. Results from this study are available in this report at **Appendix H**.

There are several key terms used in flood modelling including:

- Annual Exceedance Probability (AEP): the chance of a flood of a given or larger size occurring in any one year, usually expressed as a percentage. For example, if a peak flood discharge of 500 m³/s has an AEP of 5%, it means that there is a 5% chance (that is one-in-20 chance) of a 500 m³/s or larger event occurring in any one year (see ARI).
- Average Recurrence Interval (ARI): the long-term average number of years between the occurrence of a flood as big as, or larger than, the selected event. For example, floods with a discharge as great as, or greater than, the 20-year

ARI flood event will occur on average once every 20 years. ARI is another way of expressing the likelihood of occurrence of a flood event.

- Probable Maximum Flood (PMF): this is the largest flood that could occur at a particular location, estimated by probably maximum precipitation, combined with the worst flood producing catchment conditions. It is generally not physically or economically possible to provide complete protection against a PMF. PMF defines the extent of the flood prone land (the flood plain).

The flood study defines the ‘flood planning area’ (FPA) as:

The area of land below the flood planning level and thus subject to flood related planning controls.

The ‘flood planning level’ (FPL) is defined as:

The combinations of flood levels (derived from significant historical flood events or floods of specific AEPs) and freeboards selected for floodplain risk management purposes, as determined in management studies and incorporated in management plans.

Council determined that this study would identify the FPA by the following criteria:

- Mainstream flooding (with a significantly sized open channel
 - land inundated in the 1% AEP flood extent; and
- Overland flooding
 - The lot is subject to 100mm or greater depth of inundation in the 1% AEP event as defined in the study; and
 - At least 10% of the lot is inundated by floodwaters (i.e. the depth is greater than 0m).

Hydraulic categories define the type of floodwaters present:

- Floodway is defined as areas where:
 - the peak value of velocity multiplied by depth ($V \times D$) > 0.25 m²/s AND peak velocity > 0.25 m/s, OR
 - peak velocity > 1.0 m/s AND peak depth > 0.15 m.
- Flood Storage comprises areas outside the floodway where peak depth > 1 m; and
- Flood Fringe comprises areas outside the Floodway where peak depth < 1 m.

These categories are further refined into a High Risk (classified as Floodway and Flood Storage) and Low Risk (classified as Flood Fringe) categories.

Based on these definitions, the following results were modelled for the subject site.

Table 15 – Flood model results for the site

Address	1%AEP Main Channel Flood Depth (min-max)	1%AEP Main Channel Flood Level (min-max)	1% AEP Overland Flood Depth (min-max)	1%AEP Overland Flood Level (min-max)	Main Channel Flood Planning Level Max	1% AEP Overland Flow Hazard
9 Albert Road	0.00	0.00	0.15-1.69	9.89-9.95	N/A	High
11 Albert Road	0.00	0.00	0.10-0.53	9.57-9.95	N/A	High

Address	1%AEP Main Channel Flood Depth (min-max)	1%AEP Main Channel Flood Level (min-max)	1% AEP Overland Flood Depth (min-max)	1%AEP Overland Flood Level (min-max)	Main Channel Flood Planning Level Max	1% AEP Overland Flow Hazard
13 Albert Road	0.00	0.00	0.08-0.32	9.44-9.74	N/A	Low
2 Pilgrim Avenue	0.00	0.00	0.00	0.00	N/A	Low
4 Pilgrim Avenue	0.00	0.00	0.01-0.58	9.43-9.95	N/A	Low
6 Pilgrim Avenue	0.17-0.17	9.44-9.44	0.07-0.57	9.43-9.95	9.94	Low

Source: WMA Water

Specifically, Albert Road (near to the intersection with Raw Square, ID20, 1% AEP depth of 0.48m and PMF depth of 2.89m) and Pilgrim Avenue (near the end of the road adjacent to the rail corridor, ID38, 1% AEP depth of 0.55m and PMF depth of 3.16m) are identified as road hotspots. A road hotspot is where there is a known flood issue, identified by considering previous flood event accounts and examination of the flood behaviour.

Mapping of the site (Figure 22) indicates that only a small portion of 6 Pilgrim Avenue is mapped within the main channel (dark blue), with overland flow occurring on the remainder of the site (light blue).

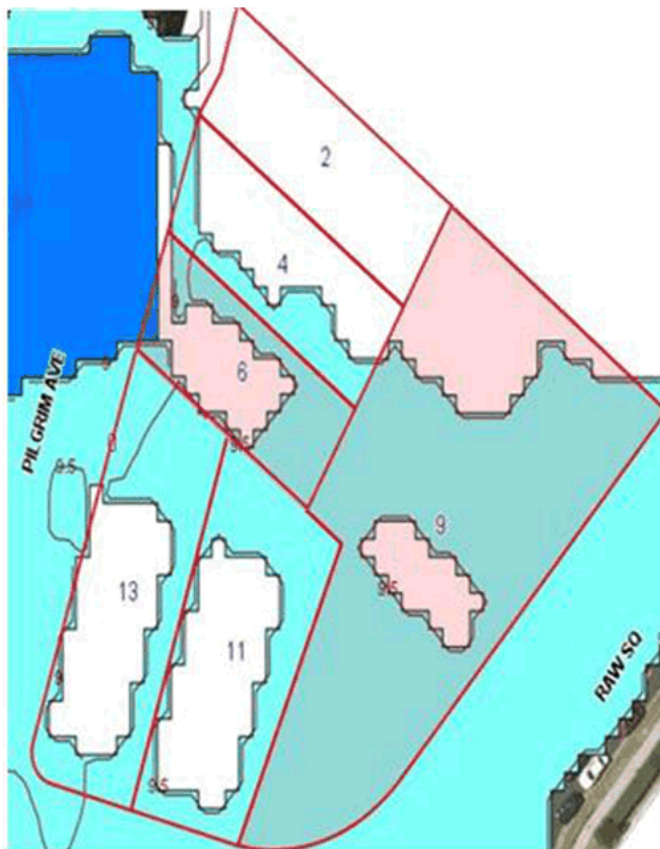


Figure 21 – Flood Planning Level mapping of the site
Source: Strathfield Council

From the above, the site has a flood planning level of between 9m and 9.5m AHD. The site has an existing ground level that ranges from 8.07m AHD to 9.87m AHD and is therefore subject to a small amount of potential flooding in the order of 0.9m. Given the relatively low flooding level it is considered that future development will be able to be easily designed to achieve the necessary freeboard above this level.

8.4 Contamination

Clause 6 of State Environmental Planning Policy No 55 – Remediation of Land (SEPP 55) requires that prior to the rezoning of land for residential, recreational, educational or child care purposes, the consent authority unless it has considered whether the land is contaminated and whether it is suitable or can be made suitable for the proposed uses.

This Planning Proposal seeks to amend the LEP building height and density controls and does not seek to change the zoning or permissible land uses on the site, noting that residential development is already a permissible use. However, the sites location adjacent to an established rail corridor introduces the potential for contamination from hydrocarbons and other heavy metals used in historical rail transportation.

A Preliminary Site Investigation (PSI) has been prepared by EI Australia (**Appendix I**) to confirm the presence of any contamination on the site. The PSI included on-site monitoring, conducted in January 2018.

Five test bores, resulting in five fill samples were extracted from the site (BH101M-BH105M). General observations during the investigation works include no odours noted, no visual signs of contamination were noted in fill samples and no fragments of asbestos were visually identified. The boreholes were sampled to a depth of:

- BH101M – 5.20m (drill bit refusal, converted to monitoring well);
- BH102 – 2.80m (target depth reached);
- BH103 – 2.00m (target depth reached);
- BH104M – 4.30m (drill bit refusal, converted to monitoring well); and
- BH105M – 4.90m (drill bit refusal, converted to monitoring well).

A further three groundwater monitoring wells were installed and left in place for a week. Water level gauging, well purging, field testing and groundwater sampling were unable to be completed due to the wells remaining dry the entire sampling time. These wells were drilled to the following depths:

- BH101M – 5.0m deep;
- BH104M – 4.3m deep; and
- BH105M – 4.9m deep.

Based on the previous investigations EI Australia understand that groundwater in surrounding areas is generally encountered at approximately 7m below ground level.



Figure 22 – Borehole and Groundwater Monitoring Locations
 Source: *El Australia*

The boreholes have identified the general site geology and subsurface profile, with bedrock being between 2.5m and 4.9m below ground level. Between these depths the subsurface of the site is made up of fill (silty clay and clay), natural (clay of high and medium plasticity) and residual soils (weathered claystone and silty clay of high plasticity).

Laboratory analysis of the collected samples was conducted. Specific Soil Investigation Levels (SILs), and Ecological Investigation Levels (EILs) were adopted for the analysis, assessed against the National Environmental Protection Measure 2013 (NEPM). The analysis identified that no hydrocarbons, pesticides, polychlorinated biphenyls and asbestos were above the adopted SILs. Copper, lead and zinc were identified as exceeding the adopted EILs. The lead concentration was found to be below the adopted levels at deeper sampling levels below the ground surface, suggesting that the lead impact was limited to the fill layer only. Generally, this exceedance was in the extent of future basement excavation, with the lead contaminated fill likely to be removed during bulk earthworks related to a future development application.

Investigation was not able to be conducted on the service station site due to the ongoing operation of the premises. Due to the presence of underground storage tanks on the site, the tanks and associated infrastructure would require decommissioning in accordance with the *Protection of the Environment Operations (Underground Petroleum Storage Systems) Regulation 2008*, during any future development application on the site.

Based on the PSI findings, it is considered that the site can be made suitable for the proposed land use.

8.5 Access, Traffic and Transport

Off street parking will be provided on site in accordance with Council's requirements. Under the current indicative concept, parking will be provided for both the residential and commercial components in a five-level basement and ground floor lower ground/partial basement level with access from Pilgrim Avenue. There will be 351 car spaces provided in total on Site 1, with capacity for a further 228 on Site 2. A total of 30 spaces will be dedicated for public commuter parking within Site 1. These parking requirements have been calculated using Council's DCP rates to identify the worst-case scenario. Further detailed assessment will be provided during the DA stage. All car parking spaces and manoeuvring zones will be designed in accordance with relevant Australian Standards.

The vehicular access is located from the secondary Pilgrim Avenue and as such, reduces the pressure on Albert Road for access. Detailed analysis of the proposed parking arrangements and potential traffic impacts of the proposal are contained in the accompanying Traffic Impact Assessment prepared by McLaren Traffic Engineers at **Appendix C**. Following their analysis, it is concluded that the proposed parking provisions are acceptable for the proposed use of the site and that any additional traffic generation will be able to be accommodated into the surrounding road network with negligible impacts.

The site fronts to Albert Road (Classified Regional Road) in the south, Pilgrim Avenue (Unclassified Local Road) in the west and Raw Square (Classified State Road) in the east, which acts as the main north-south connection in the immediate area. The Planning Proposal indicative scheme identifies that vehicular access to the site would be off the dead-end Pilgrim Avenue, removing the need for slow turning vehicles on Albert Road near to the intersection with Raw Square.

Traffic modelling for the indicative scheme has been undertaken by McLaren Traffic Engineers (**Appendix C**) using trip generation rates as specified within the RMS Guide to Traffic Generating Development. The RMS rates identify reduced trip rates for high-density residential developments, including those near public transport hubs. This modelling has resulted in the proposed development potential of the site resulting in a maximum traffic generation of 160 vehicle trips in the AM peak (79 inbound and 81 outbound) and 149 vehicle trips in the PM peak (72 inbound and 77 outbound). No reduction for the existing use of the site was applied during this modelling, resulting in a worst-case scenario being calculated.

Due to the sites location and proposed driveway entry, traffic generation of the site is assumed to travel via Raw Square with 67% of exiting vehicles using Raw Square northbound and 33% southbound. Furthermore, 40% of arriving vehicles will use Raw Square northbound and 60% southbound.

Existing intersection performance for the Pilgrim Avenue and Albert Road intersection identifies a Level of Service (LoS) of 'A' in the AM and PM peak periods, and Albert Road and Raw Square having a 'B' LoS for the same. A LoS is a qualitative measure of performance describing operational conditions, with 'A' representing the best operational condition.

The modelled traffic impacts of the indicative scheme, using the Sidra Intersection 7.0 system, results in no change in LoS for the Pilgrim Avenue and Albert Road intersection. The Albert Road and Raw Square intersection retains a 'B' LoS for AM and PM peak periods. This equates to an additional delay of 1.4 and 0.8 seconds during both the AM and PM peak periods, which is considered to be low and without adverse impacts on the intersection. The Raw Square and Everton

Road roundabout changes from an 'A' LoS in the PM peak period to a 'B' LoS with a 2.1 second delay increase. It must be noted that this is modelled on the worst-case scenario. Additionally, the provision of car parking for the site has been calculated from Council's DCP, which has considerably higher rates than those of the RMS Guide to Traffic Generating Development, which provides smaller parking requirements due to a sites location near a transport hub and within a regional metropolitan area.

Additionally, the site is located within 200m of the Strathfield Railway Station and Bus Interchange, which provide numerous routes and services. Sydney Buses operate numerous bus routes (407, 408, 415, 466, 480 and 483) along Albert Road and into the Strathfield Town Centre which provide access into greater Strathfield. No proposed changes to bus stops or routes form part of the indicative scheme.

8.6 Noise and Air Quality

The site sits adjacent to an established, highly trafficked rail corridor. Any noise impacts on residential units within the subject site from this rail corridor will be dealt with through appropriate management measures identified within a future development application.

An Acoustic Assessment completed by Acoustic Noise and Vibration Solutions included attended and unattended noise monitoring which was carried out on-site between 22 and 28 November 2017. Results from this monitoring provide that noise reduction will be required for any future built form on the site. This monitoring was conducted to review the noise impacts of the adjacent service station, rail corridor and road network near to the site. Two locations were selected – Location A was located on the boundary of the service station Site 2, and Location B was conducted on the boundary of Site 1 and the rail corridor.

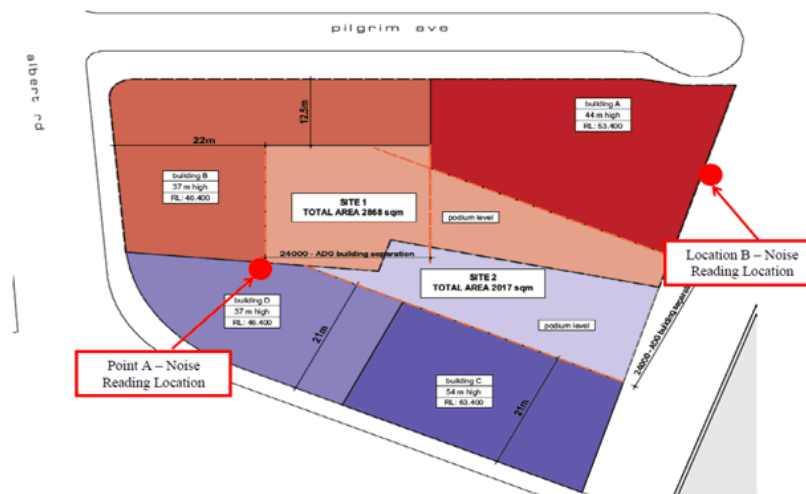


Figure 23 – Noise monitoring locations
Source: Acoustic Noise and Vibration Solutions

Results modelled for the site are based on the applicable noise criteria for residential buildings from the Development Near Rail Corridors and Busy Roads Interim Guideline which provides for an applicable noise criterion of 35dB(A) in any bedroom between 10pm and 7am, and elsewhere of 40dB(A) at any time.

2-6 Pilgrim Avenue, 9 Albert Road & 11-13 Albert Road, Strathfield ■ For Public Exhibition | February 2018

Table 16 – Service Station monitoring location results

LAeq dB(A), 7am-10pm (conservative sample)			LAeq dB(A), 10pm-7am (conservative sample)		
Outdoor Rail Noise	Noise Criteria	Noise Reduction	Outdoor Rail Noise	Noise Criteria	Noise Reduction
63	40	23	55	35	20

Table 17 – Rail Noise Survey Results, Noise Criteria and Noise Reduction Required

LAeq dB(A), 7am-10pm (conservative sample)			LAeq dB(A), 10pm-7am (conservative sample)		
Outdoor Rail Noise	Noise Criteria	Noise Reduction	Outdoor Rail Noise	Noise Criteria	Noise Reduction
63	40	23	55	35	20

To ensure the development achieves the required noise criteria for residential purposes in bedrooms and other areas, recommendations based on the above noise modelling results include:

- Sliding windows, doors and skylights in the living/kitchen/dining and bedroom areas facing the railway line are to be 10mm laminated with full acoustic seals;
- Sliding windows, doors and skylights in the living/kitchen/dining and bedroom areas facing the Albert Road and the service station are to be 10/12mm laminated with full acoustic seals;
- External doors are to be 42mm thick solid core; and
- External walls are to be double skin cavity bricks in a double brick/brick veneer construction.

These above construction details and noise mitigation measures will form part of any future development application for the site. For full details refer to the Acoustic Report at Appendix E.

An Air Quality Assessment (AQA) prepared by Todoroski Air Sciences (**Appendix F**) conducts an assessment of the potential redevelopment of the site, assuming the service station on Site 2 does not get developed at the same time as the remainder of the site.

The AQA considers the emissions associated with the existing service station, the applicability of fuelling bays and bowsers and the provision of vapour recovery on the fuel pumps. Conservative estimates consider the service station has a throughput of approximately 10 million litres of fuel per annum.

Key pollutants from fuel vapours that have the scope for any impact are benzene and toluene, with emissions calculated at 0.78% and 1.9% of the total volatile organic compound emissions respectively. Volatile organic compound emissions from diesel are significantly lower than emissions from petrol.

Dispersion modelling completed indicates that the vapour emissions from the service station would not lead to unacceptable impacts across the remainder of the site, based on methodology from the *Air Emissions Inventory for the Greater Metropolitan Region in New South Wales 2008 Calendar Year Commercial Emissions* by the Environmental Protection Authority.

The assessment results show the predicted levels of benzene and toluene would be below the respective air quality criteria across the remainder of the site. The most limiting pollutant is benzene, which is predicted to be at approximately one fifth of the acceptable level. This indicates that up to approximately five times more fuel throughput would be acceptable.

9.0 Assessment of Planning Proposal against NSW Department of Planning & Infrastructure Guidelines

The following section includes an assessment against the requirements in *A guide to preparing planning proposals* published by the Department of Planning and Infrastructure in October 2012.

9.1 Parts 1 and 2

Parts 1 and 2 of the guide have been covered in **Section 4** and **Section 5**, which outlines the objectives and intended outcomes of the proposal, as well as how the intended outcomes can be achieved through amendments to the LEP.

9.2 Part 3 - Justification

9.2.1 Need for a Planning Proposal

Q1 – Is the planning proposal a result of any strategic study or report?

This planning proposal is not the result of a strategic study or report but rather a site specific urban design analysis undertaken by Kennedy Associates Architects to determine the site's development potential taking into considering site specific constraints and opportunities and the immediately surrounding context. An Urban Design Report outlining the proposal's underlying design rationale is included at **Appendix A**. Built form is also discussed within section 4.2 of this report.

Q2 – Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?

This planning proposal represents the best means of achieving the intended outcome and objectives as it allows the site to realise its full development potential in a building form that is most likely to achieve a high-quality design. The changes to the applicable height and density controls will support the creation of a built form that is consistent with the more intense forms of development in the Strathfield Town Centre, Strathfield Triangle and the Columbia Precinct. The additional height and density will not have any adverse or noticeable environmental impacts. If density is not provided on this site, it would need to be provided elsewhere in a location less suitable for new housing.

9.2.2 Relationship to strategic planning framework

Q3 – Is the planning proposal consistent with the objectives and actions of the applicable regional or sub-regional strategy (including the Sydney Metropolitan Strategy and exhibited draft strategies)?

The Plan for Growing Sydney seeks to maximise housing delivery and development in locations with excellent access to public transport, services and facilities. The site is situated within immediate walking distance of the Strathfield Railway Station and adjacent to the Strathfield Town Centre which is earmarked for future redevelopment under the current master plan for the centre. The planning proposal supports this policy by delivering additional housing to meet future housing needs and is provided in a form that supports a better design and development outcome for the locality.

Consistency with the Plan for Growing Sydney is discussed further in section 7.1.1.

Q4 - Is the planning proposal consistent with a council's local strategy or other local strategic plan?

The planning proposal is situated to the west of the Strathfield Town Centre Master Plan, which outlines a vision for the area and sets out several underlying development guidelines including the desire for the Town Centre to improve its status as the centre of Strathfield through increased heights and densities. The planning proposal will facilitate the transition and continuity of these development guidelines through expanding the town centre area and providing a visual link with the Strathfield Triangle to the north of the site across the rail corridor.

Q5 - Is the planning proposal consistent with applicable State Environmental Planning Policies?

An assessment of the Planning Proposal against applicable State Environmental Planning Policies (SEPPs) is provided in **Table 10** above. As set out in that table the planning proposal supports an outcome that is consistent with the SEPPs.

Q6 - Is the planning proposal consistent with applicable Ministerial Directions (s. 117 directions)?

An assessment of the Planning Proposal against the Section 117 Directions is provided in **Table 11** above. As shown in this table the planning proposal is consistent with the relevant directions for planning proposals issued by the Minister for Planning under Section 117(2) of the EP&A Act.

9.2.3 Environmental, social and economic impact

Q7 - Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

Given the sites highly urbanised location the planning proposal will not result in any impact on critical habitat or threatened species, populations or ecological communities or their habitats.

Q8 - Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?

A detailed assessment of the environmental impacts of the planning proposal is provided in **Section 8.0**. As demonstrated in this analysis, whilst the proposal will result in a change in the mass, height and appearance of future development of the site the proposed changes will not result in any unacceptable impacts on the surrounding area.

Q9 - Has the planning proposal adequately addressed any social and economic effects?

The proposed development will result in positive economic and social flow-on effects for the local area. The residential component will deliver valuable housing in a location close to public transport, community facilities and jobs. The commercial ground floor areas will contribute to integrating the site with the surrounding town centre locality. In addition, the development will result in the creation of jobs throughout the construction process and once operational. The development would also provide additional Section 94 contributions for services and infrastructure in the area.

Overall, the proposal will support the implementation of the Strathfield Town Centre Master Plan and the successful integration of new housing and employment opportunities within a socially diverse region.

9.2.4 State and Commonwealth Interests

Q10 – Is there adequate public infrastructure for the planning proposal?

The site is in an established urban area and has access to a range of existing services and public infrastructure, including public transport. Further investigations will be undertaken as part of the preparation of the DA material to determine whether any upgrade of existing facilities is required.

Q11 – What are the views of State and Commonwealth public authorities consulted in accordance with the Gateway determination?

State and Commonwealth authorities will have the opportunity to provide comment on the Planning Proposal as part of its formal exhibition. Any future DA will be referred to the relevant authorities as required.

9.3 Part 4 – Mapping

The proposed amendments to the Strathfield LEP 2012 will require changes to the LEP Maps. The current Strathfield LEP 2012 controls and proposed amendments are shown in **Table 18** and **Figure 24** to **Figure 27**.

Table 18 – Strathfield LEP 2012 Map Amendments

Sydney LEP 2012 Map	Current	Proposed
▪ Floor Space Ratio	▪ 3.5:1	▪ 5:1
▪ Building Height	▪ 35m	▪ 54m

2-6 Pilgrim Avenue, 9 Albert Road & 11-13 Albert Road, Strathfield - For Public Exhibition | February 2018

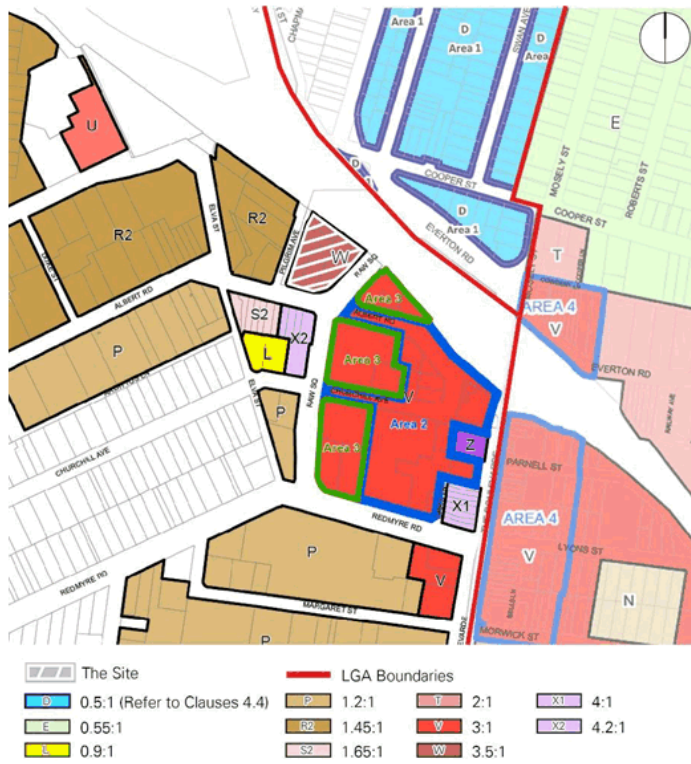


Figure 24 - Current Floor Space Ratio LEP Map

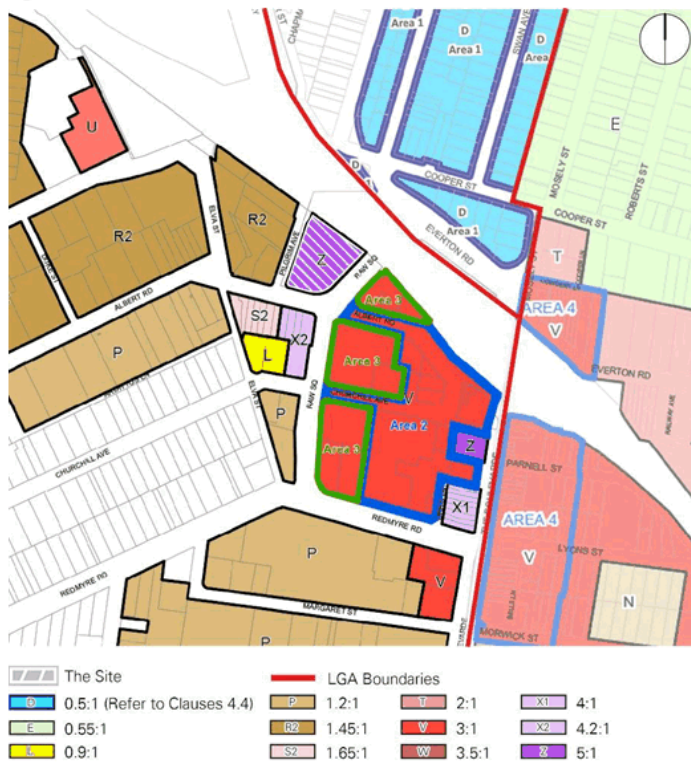


Figure 25 - Proposed Floor Space Ratio LEP Map

2-6 Pilgrim Avenue, 9 Albert Road & 11-13 Albert Road, Strathfield - For Public Exhibition | February 2018

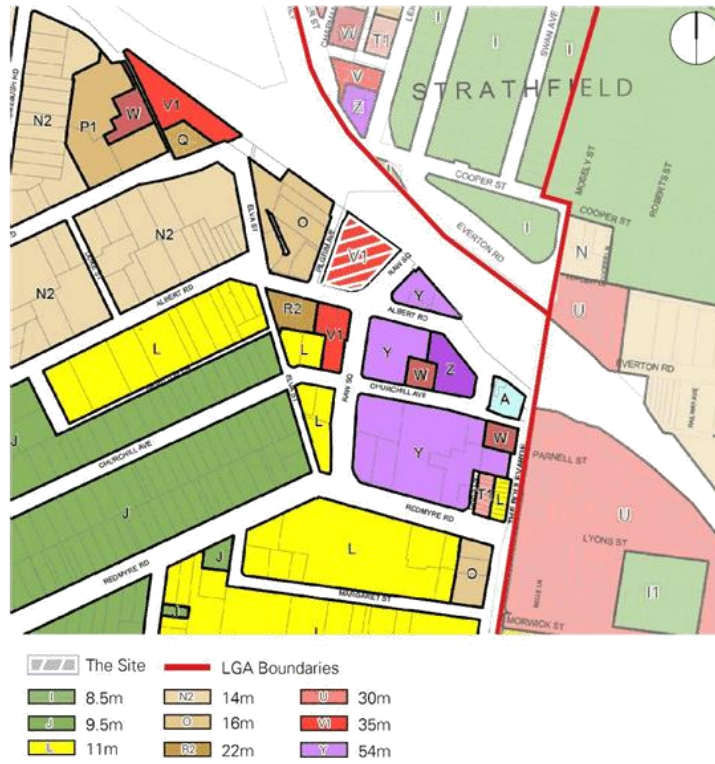


Figure 26 – Current Building Heights LEP Map

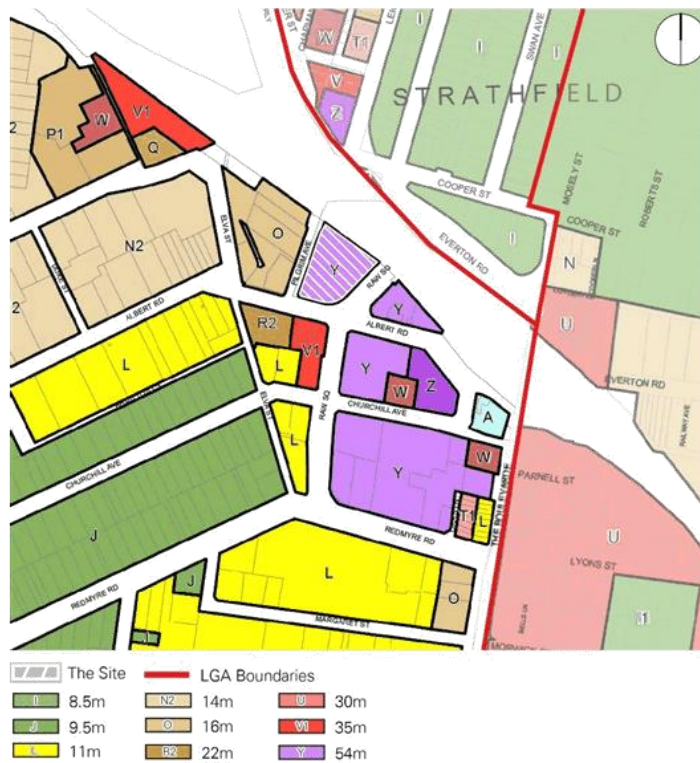


Figure 27 – Proposed Building Heights LEP Map

2-6 Pilgrim Avenue, 9 Albert Road & 11-13 Albert Road, Strathfield ■ For Public Exhibition | February 2018

9.4 Part 5 – Community Consultation

This Planning Proposal will be publicly exhibited as per Sections 56(2)(c) and 57 of the EP&A Act for a period of 28 days. Consultation will also be undertaken with a number of public authorities as required.

Any future DA for the site would also be exhibited in accordance with Council requirements, at which point the public and any authorities would have a further opportunity to make comment on the proposal.

10.0 Conclusions and Recommendations

As highlighted in this report, due to Sydney's ever increasing population and the significant and unprecedented need for housing delivery to 2031, the State Government has unequivocally outlined its priority to achieve housing growth through urban renewal of strategically well located sites in close proximity to public transport, amenities, services and facilities.

Transit oriented development, whilst not a new concept, is becoming more and more prominent in Sydney with centres such as Chatswood, St Leonards, Epping, and Hornsby, and is now a demonstrated successful model for achieving high density living and creating vibrant suburban and strategic centres, with the draft Central District Plan supporting the development of transit-oriented development sites. As pointed out in this report Strathfield town centre has the benefit of a major railway station which services several lines and provides various connections to other part of Sydney including Parramatta and the CBD. Maximising the use of land surrounding the train station therefore should be a high order strategic priority for Strathfield Council as this will significantly help to contribute to the achievement of housing targets as outlined in the draft District Plan whilst also assisting with the regeneration of the town centre.

The general area around the subject site is undergoing significant levels of revitalisation, with planning controls reflecting the areas important standing as a key centre. The three key areas identified for future development – the Columbia Precinct, the Strathfield Triangle and the Strathfield Town Centre – generally seek to focus higher buildings towards the rail corridor to act as visual and acoustic buffers for future residents. The subject site acts as a key link between these areas, and height and floor space ratio of development on the site should reflect this accordingly. Additionally, the location of the Strathfield Railway Station provides a key focal point and transport interchange which would support the development of higher density residential form.

Taking the above into consideration the current LEP controls for the site and wider town centre are conservative and prohibit the site and the wider town centre's from realising its ability to accommodate growth and development.

This Planning Proposal seeks changes to the height and floor space ratio controls affecting the subject site at 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield. Specifically, it seeks to:

- a) increase the maximum height limit for the site from 35m to 54m; and
- b) increase the maximum floor space ratio for the site from 3.5:1 to 5:1

As illustrated in the indicative scheme prepared by Kennedy Associates Architects these changes will enable the site's future regeneration as a new high quality residential led mixed-use development.

The proposed planning control amendments will expedite and facilitate the achievement a number of strategic planning, urban design and amenity benefits including, but not limited to:

- Contributing to achieving important goals, directions and actions of A Plan for Growing Sydney including provision of housing growth and in particular its delivery in a highly strategic location that is well serviced by infrastructure, transport, education, recreation and centres of employment;
- Provide the opportunity to retain a portion of non-residential land uses on the site;
- Delivering new temporary and permanent job opportunities;

- Improve the site's contribution towards the Strathfield Town Centre and the overall quality of built form in the local area;
- Reinforce Strathfield Town Centre's role as a key business, social, cultural and residential focal point in the LGA;
- Activation of the street;
- Deliver a high-quality environment for residents in accordance with SEPP 65 and the Apartment Design Guide;
- Continuity of the development corridor from the Strathfield Town Centre, to the Strathfield Triangle and Columbia Precinct while focussing development on the existing rail corridor; and
- Maximising the re-use of a well-located brownfield site for urban infill development whilst minimising adverse environmental and amenity impacts on the existing surrounding buildings.

Considering the above the planning proposal is consistent with relevant strategic and statutory planning documents and will deliver a number of demonstrable public benefits. An environmental assessment of the impacts of the proposed built form facilitated by the planning proposal has also been undertaken and it demonstrates that the planning proposal will not result in any unacceptable environmental impact.

Given the strategic planning merit of the proposed amendments, the applicant requests that Strathfield Council forward this Planning Proposal to the Minister for Planning for public exhibition, prior to the making of the Plan for gazettal.



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E council@strathfield.nsw.gov.au | www.strathfield.nsw.gov.au | ABN 52 719 940 263

31 August 2017

Stephen Murray
Executive Director, Regions
Planning Services
GPO Box 39
SYDNEY NSW 2001

Dear Stephen,

Re: Gateway Determination – Planning Proposal for 2-6 Pilgrim Avenue and 9-13 Albert Road, Strathfield (PGR_2016_STRAT_001_00)

As you are aware, on 18 October 2016, the Sydney East Joint Regional Planning Panel considered the Planning Proposal for 2-6 Pilgrim Avenue and 11-13 Albert Road, Strathfield at the pre-Gateway review. The Panel recommended that the Planning Proposal proceed to Gateway subject to the following changes and additional information:

- The proposal be extended over the whole street block to include the adjoining service station;
- The whole street block have a permissible FSR of 5:1 and a height limit of 54m;
- A development control plan be prepared and exhibited with the draft LEP;
- A new traffic study be prepared which applies to the whole street block; and
- Land contamination and hydraulic studies be left to the development application stage.

Following the Panel's recommendation, the applicant (JBA Urban Planning Consultants), submitted a revised Planning Proposal to Council in May 2017 accompanied by:

- Revised architectural drawings and urban design report prepared by Kennedy Associates Architects;
- Site survey plan prepared by DJ Barrington & Associates;
- Revised traffic impact assessment prepared by McLaren Traffic Engineers; and
- JRPP recommendation report and proceed to Gateway letter.

As you are aware, in Council's letter dated 20 July 2017, Council accepted the role of responsible planning authority (RPA) for the Planning Proposal. Subsequently, the Department of Planning & Environment (DP&E) requested that Council forward the Planning Proposal for Gateway Determination with a cover letter advising of any further comments and Gateway conditions.

Accordingly, Council officers are forwarding the attached Planning Proposal for Gateway Determination. However, Council officers have reviewed the Planning Proposal and raise a number of concerns which are detailed below. Based on the concerns, Council does not support the Planning Proposal in its current form. If the DP&E issues a Gateway Determination, it is recommended that the Planning Proposal be deferred until the matters of concern are addressed.

Defer Planning Proposal – Strathfield Priority Precinct

As you would be aware, on 1 June 2017, the DP&E announced Strathfield as a Priority Precinct which would encompass the subject site (i.e. within 800m of Strathfield Station). The DP&E will be working with Council, the community, and other stakeholders to plan for growth in this precinct.

Council is not in favour of site-specific planning proposals within the Strathfield Priority Precinct whilst a precinct wide planning proposal is being prepared for the following reasons:

- a) The opportunities and cumulative impacts of revised planning controls intensifying development are better considered and managed on a precinct wide basis, particularly given the site's location in proximity to the Canada Bay and Burwood LGAs. The Priority Precinct process will enable consideration of the subject Planning Proposal in context of the wider precinct, ensuring a consistent planning approach across the three (3) councils;
- b) Site-specific amendments to planning controls may pre-empt or be contrary to the desired outcomes of a precinct wide plan; and
- c) Site-specific planning proposals divert resources away from precinct planning, slowing the process and resulting in inequitable outcomes within the precinct.

Accordingly, it is recommended that the subject Planning Proposal and other site-specific planning proposals within the Strathfield Priority Precinct be deferred until precinct wide planning has been undertaken.

New Planning Proposal

The Panel's recommendation to extend the Planning Proposal over the whole street block to include the adjoining service station at 9 Albert Road, Strathfield is noted. However, the inclusion of the service station almost doubles the size of the precinct. Accordingly, it is Council's position that the changes recommended by the Panel are of such significance that they warrant the submission of a new Planning Proposal. Council is also of the view that consent should be obtained from all landowners within a Planning Proposal that is not initiated by Council. Landowner consent would also facilitate access to properties to undertake any necessary studies whose recommendations will affect the development outcomes, such as contamination investigations for the existing service station forming part of the subject Planning Proposal.

There is no evidence provided to demonstrate that the landowner of 9 Albert Road has been consulted or is aware that another landowner is lodging a Planning Proposal for their land.

Moreover, it is likely that a masterplan for the precinct would be compromised by the uncertainties of rezoning land owned by individuals who are not seeking a rezoning.

If the service station owner does not wish to develop their site, land use conflicts may arise with the adjoining development so an assessment of the impact of the service station, such as fumes and the hours of operation, on a development at 2-6 Pilgrim Avenue and 11-13 Albert Road (i.e. Site 1) should be submitted with the Planning Proposal. Applying the same logic, at the development application stage, a single application should be lodged for the precinct to ensure a cohesive development where services, basements and driveway access can be shared.

As previously indicated, it is Council's position that consent should be obtained from all landowners within a rezoning proposal that is not Council initiated. Council has not identified the subject precinct as a priority rezoning area and therefore, has no intention of initiating a planning proposal for this precinct at this stage.

Excessive Densities

It is recognised that the site has the potential to accommodate greater density and height. However, the Planning Proposal fails to justify why this particular site is suitable for such a significant uplift in height and floor area which is in direct contravention of the established hierarchy of the *Strathfield Local Environmental Plan 2012 (SLEP 2012)*.

The Planning Proposal attempts to justify the additional height and density by relying upon the proximity of the site to the Strathfield Town Centre and a draft concept plan for the future of the Town Centre which was prepared by Council a number of years ago.

The site is **not** located within the area that has been identified as the Strathfield Town Centre. Furthermore, the Planning Proposal places too much emphasis on the approaches taken in centres such as Chatswood, Burwood and Hornsby in their response to the interface between higher density town centres and their surrounding lower density residential areas. This approach is irrelevant to the specific characteristics of the subject site and the strategic context within which Strathfield is located.

Last year, Council engaged *David Lock & Associates (DLA)* to conduct a peer review of the originally submitted Planning Proposal and to provide advice regarding an appropriate FSR and height for the site. The original Planning Proposal recommended a maximum building height of 70 metres and FSR of 8.3:1.

DLA determined that contextually the proposed heights and densities were too high. DLA recommended a maximum building height of 14 storeys with shorter buildings of 8 and 11 storeys and an overall FSR of 4.5:1 based on the indicative layout scheme submitted by the proponent at the time. DLA also recommended:

- Higher densities be located adjacent to the railway line with heights stepping down towards the lower density residential areas;
- Development to a maximum height of 11 storeys should continue along Albert Road and wrap around the corner fronting Raw Square; and

- Development should provide a lower street wall to the west along Pilgrim Avenue with a 3 storey podium and a maximum of 8 storeys to respond to the lower density residential area, provide more articulation, soften the street wall edge, create a pedestrian scale at ground level and reduce overshadowing.

Council officers also recommend in accordance with the Panel's recommendation that a distribution of heights be provided across the site with the highest buildings closest to the station and lower heights adjoining the residential areas.

It is evident that the proposed controls are contrary to Council's testing and recommendations for the site and have had no merit assessment undertaken to support them. When recommending any increase in density and building height it is imperative that the resulting built form and scale is consistent with the context of the site and provides an appropriate scale proportionate with surrounding streets and buildings.

The applicant has not lodged a range of feasible options for the site for Council to consider. This is inadequate and does not allow Council to determine whether there are any other feasible options with a more desirable outcome.

The applicant should reconsider the heights and densities in light of DLA's review and Council's recommendations. In doing so, the applicant, through the Planning Proposal, must assess the urban design aspects and submit a range of urban design options (at least three) of varying heights, FSRs, and building envelopes that are tested, exhibit design excellence and are compliant with the Apartment Design Guide (ADG). The applicant should thoroughly assess each option and justify the preferred option.

It should be noted that none of these factors were taken into account at the pre-Gateway review where it was decided to essentially double the size of the land, the subject of the Planning Proposal.

Poor Amenity

The high density built form and configuration of building envelopes proposed adversely impacts on the amenity of future occupants. The excessive density and configuration of buildings, many of which face each other internally, compromises the privacy and solar access of many of the apartments. Furthermore, the proposal reduces the amenity of existing surrounding developments with no consideration of building transitions and interfaces, particularly the developments to the west and southwest.

The proposal's compliance with the building separation, solar access and ventilation requirements of the ADG are questionable. The indicative development plans only show solar access and ventilation for the site at 2-6 Pilgrim Avenue and 11-13 Albert Road (i.e. Site 1) with no indication that the service station site (i.e. 9 Albert Road – Site 2) has been considered in the assessment or whether a development on Site 2 could comply. Furthermore, the shadow analysis plans are unclear and amended shadow diagrams should be provided.

As discussed above, a number of options should be tested to ensure an optimal outcome for the applicant's site and the service station site. The proposal should not unfairly restrict or compromise development of the service station site and a cohesive development should be masterplanned across the precinct.

Inadequate Commercial Development and Active Street Frontage

The site's frontage to three (3) streets (i.e. Raw Square, Albert Road and Pilgrim Avenue) offers an opportunity to enhance the public domain by providing an active and continuous street frontage that promotes social activity and passive surveillance adjacent to the Strathfield Town Centre. The application proposes minimal commercial uses, occupying only the ground floor, in addition to building services which are adverse to the provision of an active street frontage. Since the site is zoned B4 Mixed Use and is on the periphery of the Town Centre, the most important commercial location in the Strathfield LGA, a more significant commercial component should be provided in accordance with Section 117 Direction – 1.1 Business and Industrial Zones.

Required Studies

The Panel's recommendation that land contamination and hydraulic studies may be left to the development application stage is noted. However, the precinct, in particular the service station, is affected by flooding and is likely contaminated. Furthermore, the precinct is affected by noise from the adjacent rail corridor. Given these constraints on development, relevant studies should be submitted with the Planning Proposal as their recommendations will determine the site's potential to be redeveloped and the appropriate development outcomes, including FSR, heights and building envelopes. For example, the application proposes five (5) levels of basement parking which may not be feasible depending on the extent of flooding and contamination. Justification for each of the required studies is provided below.

- Detailed flood study – The Planning Proposal is inconsistent with Section 117 Direction – 4.3 Flood Prone Land. The subject site is significantly affected by the '1-in-100 flood event'. A flood study is required to determine the flood planning level, the impact of development on flooding, the site's developable area, and therefore, the Planning Proposal's consistency with the *Floodplain Development Manual 2005*.
- Detailed contamination assessment – The Planning Proposal acknowledges that the site is potentially contaminated from the adjoining service station with potential fuel leakage, corrosion of underground tanks, emission of particulate matter and migration of contaminants. Contamination from hydrocarbons and other heavy metals from its proximity to the railway station is also identified.
- Acid Sulfate Soils Study – The site is identified in the *SLEP 2012* as containing Class 5 acid sulfate soils. Given the excavation proposed for the basement parking, an Acid Sulfate Soils Study should be required in accordance with Clause 6.1 (Acid sulfate soils) of the *SLEP 2012*.
- Acoustic report – Given the site's proximity to a rail corridor, an acoustic report is necessary to ensure that adjacent development achieves appropriate acoustic

amenity that complies with the internal noise criteria specified in *SEPP (Infrastructure) 2007* and the *Development near Rail Corridors and Busy Roads – Interim Guideline* (Department of Planning, 2008). The acoustic report should determine the required setback and acoustic attenuation to ensure the amenity of future occupants.

- Detailed traffic impact assessment – In accordance with the Panel's recommendation for a new traffic study to be prepared which applies to the whole street block, the applicant has submitted a revised traffic impact assessment. However, the traffic assessment fails to consider the cumulative potential traffic from surrounding sites if all the sites in and around the town centre were to be developed to the maximum height and floor area controls permissible under the *SLEP 2012*.

However, as discussed above, Strathfield has been identified as a Priority Precinct and it is likely that the planning controls for the surrounding area will be amended. Accordingly, it is recommended that the Planning Proposal be deferred until the completion of the Priority Precinct work to adequately determine the impact of the proposal in the context of the wider precinct.

Site Specific DCP

In accordance with the Panel's recommendations, the applicant should be required to submit site specific DCP controls and a revised masterplan for Council's review prior to exhibiting it with the Planning Proposal. The site specific DCP should provide detailed planning and design principles that ensure a cohesive development that considers the context of the site, in particular its proximity to the Strathfield Town Centre, lower density residential development, and adjacent rail corridor.

Public Benefit – Voluntary Planning Agreement

The Planning Proposal does not provide sufficient public benefit to support the significant uplift in density. The Planning Proposal states that:

“The residential component will deliver valuable housing in a location close to transport, community facilities and jobs. The commercial ground floor areas will contribute to integrating the site with the surrounding town centre locality. In addition the development will result in the creation of jobs throughout the construction process and once operational. The development would also provide additional Section 94 contributions for services and infrastructure in the area.”

At its meeting held on 1 August 2017, Council resolved to commence the preparation of a value sharing policy that will apply to any proposal which increases the development potential of a site above that which can be attained under *SLEP 2012*. As such, the applicant should discuss the preparation of a Voluntary Planning Agreement with Council officers which should be exhibited concurrently with the Planning Proposal. It is Council's preference that the public benefit be provided on the applicant's site (i.e. 2-6 Pilgrim Avenue or 11-13 Albert Road).

Gateway Conditions

As discussed above, Council has a number of concerns with the Planning Proposal and does not support the Planning Proposal in its current form. However, if the DP&E issues a Gateway Determination, it is recommended that the following Gateway conditions be imposed:

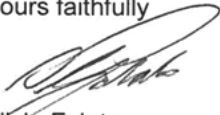
1. The Planning Proposal is to be deferred until the Strathfield Priority Precinct work has been undertaken and planning controls have been established for the Precinct.
2. Prior to community consultation, the Planning Proposal is to be updated and amended to the satisfaction of Strathfield Council to include the following for the entire precinct (i.e. 2-6 Pilgrim Avenue and 9-13 Albert Road, Strathfield):
 - a) A detailed flood study to demonstrate the suitability of the site for redevelopment;
 - b) A Stage 1 and 2 contamination assessment for the whole site including the adjoining service station;
 - c) An acid sulfate soils study to demonstrate the suitability of the site for redevelopment;
 - d) An acoustic report assessing the suitability of the site for redevelopment including the impacts of the adjacent railway corridor;
 - e) A detailed traffic impact assessment that considers the cumulative potential traffic from surrounding sites if all the sites in and around the Strathfield Town Centre were to be developed to the maximum height and floor area controls permissible under the *Strathfield Local Environmental Plan 2012*;
 - f) An assessment of the impact of the service station, such as fumes and the hours of operation, on a development at 2-6 Pilgrim Avenue and 11-13 Albert Road;
 - g) A range of urban design options (at least three) of varying heights, FSRs, and building envelopes that are compliant with the Apartment Design Guide and justify the preferred option;
 - h) Site specific DCP controls and a masterplan to guide the future development of the site; and
 - i) A voluntary planning agreement providing an appropriate public benefit (such as publically accessible open space) on 2-6 Pilgrim Avenue and 11-13 Albert Road, Strathfield.
3. Community consultation is required under sections 56(2)(c) and 57 of the Act as follows:
 - a) the planning proposal must be made publicly available for a minimum of **28 days**; and
 - b) the relevant planning authority must comply with the notice requirements for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in Section 5.5.2 of *A guide to preparing local environmental plans* (Department of Planning and Environment 2016).

4. Consultation is required with public authorities under section 56(2)(d) of the Act, including:
- Roads and Maritime Services; and
 - Sydney Trains.

As discussed above, Council officers have a number of concerns with the Planning Proposal and cannot support it in its current form. However, should the DP&E issue a Gateway Determination, it is recommended that the above Gateway conditions be imposed on the Planning Proposal to ensure the matters of concern are addressed prior to community consultation.

Should you have any queries regarding any of the above matters, please contact Joanne Chan, Council's Strategic Planning Officer, on 9748 9615.

Yours faithfully



Silvio Falato
Manager Planning & Development

Submission Summary – No 2-6 Pilgrim Avenue, 9 Albert Road and 11-13 Albert Road, Strathfield

Name	Address	Summary of Submission	Council Officer's Comment
Chris Wong	PO Box 142 StrathfieldNSW	Building Form may create poor outcome for residents of development Height across site/ commercial areas being used as residential Traffic impacts not considered in enough detail	The Planning Proposal is not a DA for development on the site. Any future development will need to comply with the Apartment Design Guide The planning proposal includes commercial areas. RMS had no concerns regarding the traffic modelling undertaken. Additional traffic studies will be required for any development application on the site.
Carlos Monteiro & Christine D'Mello	Sandalwood Apartments 3-7 Albert Road Strathfield.	Height of buildings blocking sunlight and impacts on privacy Over congestion of the area	This will be a matter for consideration during the assessment of any development application on the site. The area is located in close proximity to Strathfield Station and is well suited to an increase in density
Corel Law	56 Albion Road Strathfield	Form Letter 1. See Note below	Form Letter 1. See note below
Celia Tai	37 South Street Strathfield	Form Letter 1. See Note below	Form Letter 1. See Note below
Shiu Chung Lee	37 South St Strathfield	Form Letter 1. See Note below	Form Letter 1. See Note below
Ronald Lai	1 Mount St Strathfield	Form Letter 1. See Note below	Form Letter 1. See Note below
Mandy Zheng	6/45 Albert Road Strathfield	Form Letter 1. See Note below	Form Letter 1. See Note below
Dorothy Chu	107 Albert Road	Form Letter 1. See Note below	Form Letter 1. See Note below

<p>John and Helen Horsborough</p>	<p>Strathfield 44 Hastings Road Warrawee</p>	<p>View loss to the west/No consideration of visual impacts</p> <p>Loss of privacy</p> <p>Does not support any changes to existing controls</p> <p>Land is not suitable for further density</p>	<p>The increase in height proposed may change the extent of views from some properties. However, this can only be considered during the assessment of any Development Application when the built form of the building is known.</p> <p>This will be a matter for consideration during the assessment of any development application on the site.</p> <p>Noted</p> <p>The land is in close proximity to a major transport hub and is considered to be suitable for an increase in density.</p>
<p>Vincent Wong</p>	<p>117 Albert Road Strathfield</p>	<p>Form Letter 1. See Note below</p>	<p>Form Letter 1. See Note below</p>
<p>Matt Chan</p>	<p>No Address Provided</p>	<p>Concerns over validity of traffic assessment and impacts on local road network</p> <p>Planning Proposal references the Strathfield Town Centre proposal which is no longer relevant.</p> <p>Creation of a wind tunnel along Raw Square as a result of two Residential Flat Buildings (one existing, one proposed)</p>	<p>The Planning Proposal has undertaken a traffic study that analyses the impacts of the proposed development.</p> <p>The Planning Proposal references a number of strategic planning documents that are intended to guide future development within the Strathfield LGA.</p> <p>It is difficult to substantiate this claim.</p>

Redacted	No address provided	<p>Impacts on existing local infrastructure including schools and traffic</p> <p>Validity of changing the current LEP</p> <p>Planning proposal represents an over development of the site.</p>	<p>The PP was referred to relevant agencies and no objections were received.</p> <p>It is understood that DOE are working on ways to address the increased demand on schools.</p> <p>The spot rezoning process is the correct way of implementing site specific changes to the LEP.</p> <p>The Planning Proposal is of a similar scale to surrounding development.</p>
Elvin Cheng	37 Albyn Road Strathfield	Form Letter 1. See Note below	Form Letter 1. See Note below
Redacted	Redacted	Form Letter 1. See Note below	Form Letter 1. See Note below
Anthony Bennet-Smith	1108/5-7 Pilgrim Avenue Strathfield	<p>Negative impacts on road and pedestrian traffic</p> <p>Lack of fire brigade vehicles that can reach buildings in excess of 30m in height</p> <p>Lack of consideration of sunlight access to existing units.</p>	<p>The Planning Proposal has undertaken a traffic study that analyses the impacts of the proposed development. The site is located in an area that has significant pedestrian facilities</p> <p>Not considered relevant to the Planning Proposal</p> <p>This is a matter for consideration under subsequent Development Applications.</p>
J M Bonner	1206/5 Pilgrim Avenue Strathfield	<p>Loss of views due to development in the surrounding area</p> <p>Loss of character in the area</p> <p>Traffic impacts</p>	<p>Impacts on views will be considered during the assessment of the future development on site.</p> <p>The predominate character of the area is of multi storey Residential Flat Buildings</p> <p>The Planning Proposal has undertaken a traffic</p>

			study that analyses the impacts of the proposed development.
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Note 1:

Of the 16 objections received, 9 were a form letter which were identical in form and content. However, as per the requests of the objectors, these have been noted as individual submissions. In order to simplify the above table, one response to this submission is provided below:

Summary of Submissions	Council Officer's Comment
Reduction in ventilation for existing residents in surrounding buildings.	This is not grounds for refusal of a Planning Proposal
Impacts on overshadowing on Pilgrim Avenue considered overdevelopment of the site.	Overshadowing will be assessed in any assessment of the site The Planning Proposal seeks to increase the height and fsr of a site that is located in close proximity to a major public transport interchange.
35% increase in height considered excessive	The Planning Proposal corresponds to the height of nearby development and reflects the proximity of the site to both a major commercial/retail area and a transport hub.
Existing overcrowding of trains departing Strathfield station and overcrowding at station	No comments from TfNSW Sydney Trains regarding overcrowding at the station or on trains were received.
Housing mix and lack of affordable housing is not supported.	The detailed housing mix is a matter for a subsequent Development Application for the site.
Lack of direct sunlight and cross ventilation for proposed units	Details of compliance with the Apartment Design Guidelines will be provided for at the Development Application Stage.
Request for demonstration of compliance with Apartment Design Guidelines now, not at DA stage.	Details of compliance with the Apartment Design Guidelines will be provided for at the Development Application Stage. A Planning Proposal does not include full details of the proposed development.

Too many car parking spaces provided to reduce car use.	The car parking spaces are indicative and will be detailed within any Development Application
Overshadowing of properties to the south – at least 2 hours of sunlight provided to the south.	Overshadowing will be a matter for assessment of the building design under the Development Application
Concerns over provision of ground floor commercial and retail floorspace and separation from the existing town centre.	The Planning Proposal provides for ground floor commercial space to activate the streetscape and provide for additional commercial space within the immediate area of the Strathfield town centre.
The proposal will have a negligible effect on employment in the local area.	Temporary employment will be generated during the construction phase of the project. Additional long-term employment options will be generated by the ground floor commercial space. However, it is also noted that the Planning Proposal is primarily focused on the provision of housing.
The traffic study does not consider the cumulative impacts of traffic in the local area.	The traffic study considers the impacts of the Planning Proposal on the area and provides for recommendations.
Traffic study does not model 'worst case' scenario and assumes majority of residents will utilise public transport.	The location of the site, within close proximity of a major public transport hub, provides justification for this assessment.

Public Authority Consultation:

In accordance with the Gateway Determination the following State Agencies were consulted

- Transport for NSW – Sydney Trains
- Transport for NSW – Roads and Maritime Services
- NSW Department of Education and Communities
- NSW Fire and Rescue
- Ambulance NSW
- NSW Police Force
- NSW State Emergency Service

- NSW Ministry of Health
- Energy Australia; and
- Sydney Water

A response was received from TfNSW – Sydney Trains, TfNSW – Roads and Maritime Services, NSW Department of Education and Communities and Energy Australia.

No objection was raised in these submissions.

TfNSW – RMS advised that the proposal would not have a significant traffic impacts on the state road network, and provided general comments on connectivity, safety and accessibility for pedestrians and bicycle riders to the existing networks and public transport links.

PREPARATION NOTE: This DCP has been prepared to be generally consistent in format as DCP No. 14 Part Lot 1 and Lot 2 DP 711168 Davidson St, Greenacre, as adopted by Strathfield Council on September 2005 and in force from May 2006.

STRATHFIELD DEVELOPMENT CONTROL PLAN NO XX

SP8785, Lots 8 and 9 DP15917, Lots A and B
DP100558 and Lot 100 DP807807

2, 4 and 6 Pilgrim Avenue and 9, 11 and 13
Albert Road, Strathfield

1.0 INTRODUCTION

1.1 Name of this Plan

This Development Control Plan Shall be cited as “STRATHFIELD DEVELOPMENT CONTROL PLAN NO XX, SP8785, Lots 8 and 9 DP15917, Lots A and B DP100558 and Lot 100 DP807807, 2, 4 and 6 Pilgrim Avenue and 9, 11 and 13 Albert Road, Strathfield.”

1.2 Commencement of Development Control Plan

1.3 Aims of the Development Control Plan

The aims of this plan are:

- a) To provide appropriate development control principles for the development of the site for residential purposes;
- b) To provide for a distribution of height across the site;
- c) To ensure that the future development of the land is compatible with existing adjacent development;
- d) To specify landscape elements and concepts to be incorporated into any redevelopment of this land; and
- e) To ensure that the site is adequately provided with water, sewerage and stormwater drainage services; and
- f) To ensure environmental, flooding and contamination matters are addressed at the Development Application stage.

1.4 Land to which plan applies

This plan applies to the land in SP8785, Lots 8 and 9 DP15917, Lots A and B DP100558 and Lot 100 DP807807 known as 2, 4 and 6 Pilgrim Avenue and 9, 11 and 13 Albert Road, Strathfield.

1.5 Local Environmental Plan applying to the land

The Strathfield Local Environmental Plan 2012 applies to the land.

1.6 Relationship to Other DCPs and Policies

This DCP should be read in conjunction with:

- i) Strathfield Local Environmental Plan 2012;
- ii) XXXX
- iii) XXXX
- iv) XXXX

2.0 SITING AND DESIGN

2.1 Site Layout

Development on the land is to provide built form around the boundary of the site and common open space in the centre. Built form is to be open towards the rail corridor.

2.2 Building Form and Height

The development of the land is to substantially conform with the principles embodied in **Figure 1**.

Notwithstanding any other provision of this plan, the height of the tallest building on the site is not to exceed the levels outlined in the LEP Height Map at the time of a development application, unless a variation through Clause 4.6 is provided.

The tallest building on the site is to be located nearest to the Strathfield Railway Station, with the height across the site distributed in a way as to focus attention towards the Town Centre to the east.

Rooftop uses are encouraged. The maximum building height does not prevent the use of rooftops for communal spaces, facilities and associated structures (including lift overruns and the like), including access to them.

2.3 Active Frontages

Active frontages are to be provided along Albert Road and Raw Square as shown in **Figure 1**.

2.4 Access

Vehicle access is to be provided to the site from Pilgrim Avenue and Raw Square as shown in **Figure 1**. No vehicle access is permitted from Albert Road.

2.5 Parking

Basement car parking is to be designed and provided in accordance with Part I of Strathfield Consolidated Development Control Plan 2005 – Provision of Off-Street Parking Facilities, or the applicable planning controls at the time of submission of a development application for the site.

The development is to provide adequate off-street parking relative to the demand for parking likely to be generated.

2.6 Retail and Commercial Uses

Commercial premises or shops are to be provided along the street frontages to Albert Road and Raw Square to support the adjacent Town Centre.

2.7 Contamination

A Site Contamination Assessment of the land is to be submitted with any development application for the use of the land and associated construction works. The assessment is to indicate that the soil conditions on the site are suitable for the use to be conducted and/or identify any remedial measures that may be required to make the land suitable for the use.

2.8 Noise

An acoustic assessment is to be submitted with development applications proposing residential development on the site and any noise mitigation measures that are recommended are to be implemented. Any development of the site is to consider and address various matters raised within the Department of Planning's publication titled "Development Near Rail Corridors and Busy Roads."

2.9 Flooding

Driveways providing access to the site are to be designed to achieve the necessary flood free planning level for the site.

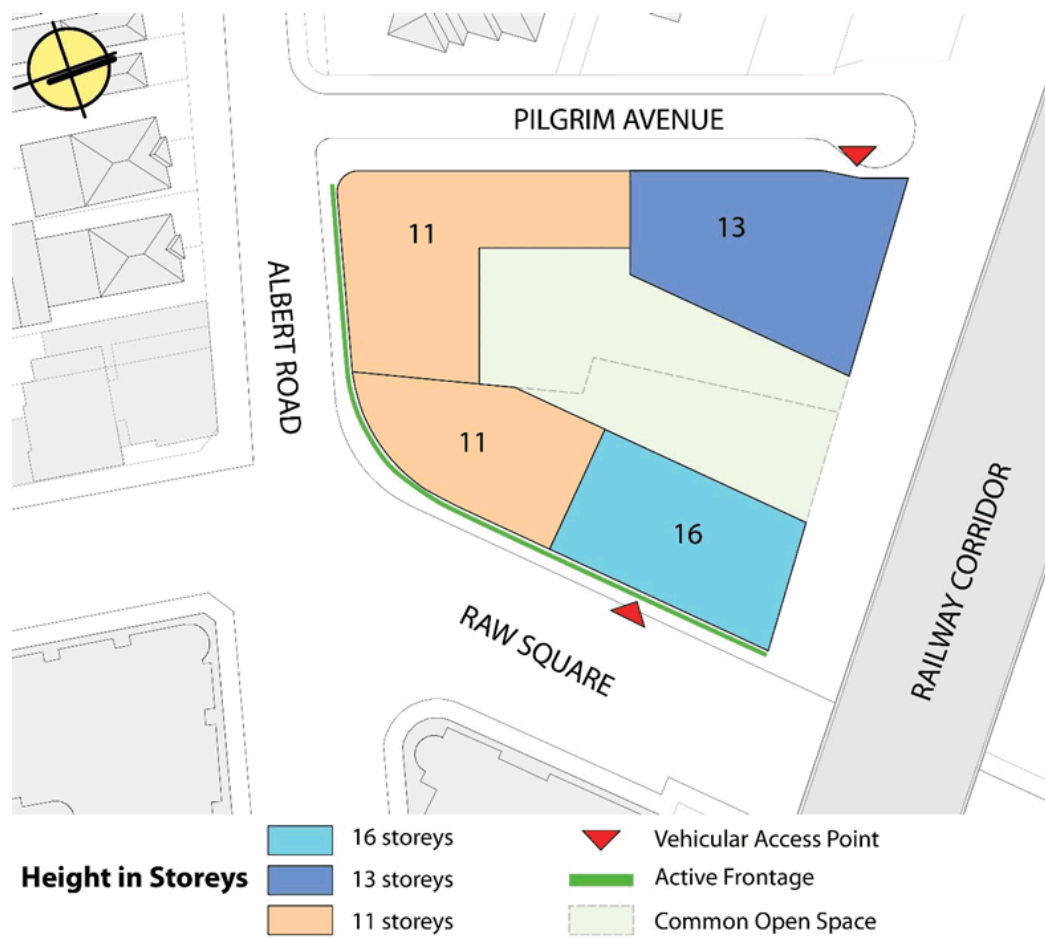


Figure 1: Development Controls



**TRAFFIC AND PARKING IMPACT ASSESSMENT OF
MIXED-USE DEVELOPMENT
AT 2-6 PILGRIM AVENUE & 11-13 ALBERT ROAD, STRATHFIELD**



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Transport Planning, Traffic Impact Assessments, Road Safety Audits, Expert Witness

17156.01FC - 6 February 2018



Development Type: Mixed-Use Development
Site Address: 2-6 Pilgrim Avenue & 11-13 Albert Road, Strathfield
Prepared for: Ethos Urban
Document reference: 17156.01FC

Status	Issue	Prepared By	Checked By	Date
Draft	A	BP	TH	21 April 2017
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Final	C	DW	CHM	6 February 2017

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1 INTRODUCTION

McLaren Traffic Engineering (MTE) was commissioned by *Ethos Urban* to provide a Traffic and Parking Impact Assessment of the Mixed-Use Development at 2-6 Pilgrim Avenue & 11-13 Albert Road, Strathfield as depicted in reduced plans reproduced in **Annexure A**.

1.1 Description and Scale of Development

The proposal includes two sites, upon which a total of four buildings are proposed. The development of Site 1 will include the amalgamation of a total of five (5) existing residential lots. The details of the site relevant to this traffic and parking impact assessment are summarised below:

- Site 1 includes the following:
 - 51 one-bedroom units;
 - 117 two-bedroom units;
 - 888m² ground floor retail area;
 - 5 levels of basement parking including 355 car parking spaces;
 - Two-way driveway access to Pilgrim Avenue.
- Site 2 includes the following:
 - 113 two-bedroom units;
 - 471m² ground floor retail area;
 - Two-way driveway access to Raw Square.

In addition to the above, a total of 30 parking spaces will be provided to the public for commuters.

1.2 State Environmental Planning Policy (Infrastructure) 2007

The proposal qualifies as a traffic generating development with relevant size and/or capacity and shall be assessed in accordance with Clause 104 of State Environmental Planning Policy (Infrastructure) 2007. Accordingly, formal referral to the Roads and Maritime Services (RMS) is necessary.

The site is located within the jurisdiction of Strathfield Council and is subject to their planning controls with the Joint Regional Planning Panel as the consent authority.



1.3 Site Description

Site 1 is located at 2-6 Pilgrim Avenue & 11-13 Albert Road, Strathfield, bounded by Pilgrim Avenue to the west, the state rail corridor to the north and Albert Road to the south and involves the amalgamation of the following five (5) existing residential lots:

- SP8785
- Lot 8 DP15917
- Lot 9 DP15917
- Lot A DP100558
- Lot B DP100558

Site 2 is located at 9 Albert Road, Strathfield and is currently occupied by a Shell service station. The site has road frontages to Albert Street to the south and Raw Square to the east and is bounded by a state rail corridor to the north.

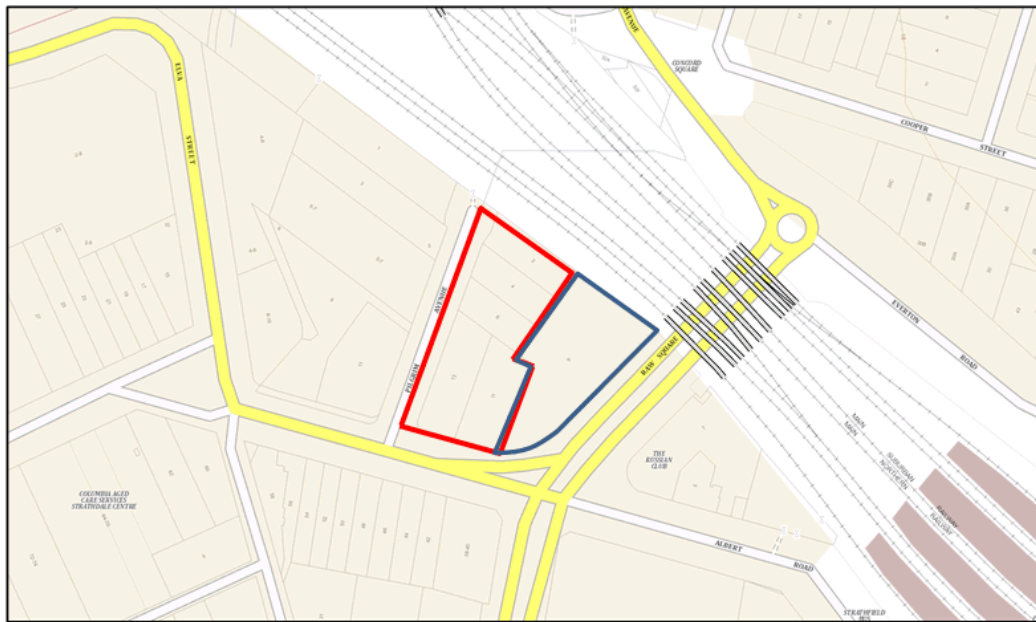
1.4 Site Context

The location of the two sites are shown on aerial imagery and a map in **Figure 1 & Figure 2** respectively.



— Site 1 Location — Site 2 Location

FIGURE 1: SITE CONTEXT – AERIAL PHOTO



— Site 1 Location — Site 2 Location

FIGURE 2: SITE CONTEXT – STREET MAP



2 EXISTING TRAFFIC AND PARKING CONDITIONS

2.1 Road Hierarchy

The road network surrounding the site has the following characteristics:

2.1.1 Pilgrim Avenue

- Unclassified LOCAL Road;
- Approximately 6m wide two-way carriageway with a cul-de-sac arrangement at the end of the street;
- Signposted 50km/h speed limit;
- “No Stopping” restrictions apply along the western side of the road.

2.1.2 Albert Road

- RMS Classified REGIONAL Road (Road No. 2057);
- Approximately 15m wide two-way carriageway with three eastbound lanes on the approach to Raw Square and two westbound exit lanes;
- One-way eastbound to the east of Raw Square;
- Signposted 50km/h speed limit;
- Generally “No Stopping” along the northern side of the road;
- Generally “No Parking” on the southern side of the road.

2.1.3 Raw Square

- RMS Classified STATE Road (Road No. 668);
- Approximately 17m wide two-way carriageway, generally two lanes in each direction;
- Signposted 60km/h speed limit;
- Multiple kerbside parking restrictions along both sides of the road;
- Clearways exist within the kerbside lanes between the hours of 6-10am and 3-7pm Monday to Friday.



2.2 Existing Traffic Management

The surrounding traffic and pedestrian management controls include the following:

- Give Way T-junction at the intersection of Pilgrim Avenue / Albert Road;
- Cul-de-sac arrangement at end of Pilgrim Avenue (No Through Road);
- Signalised intersection at the junction of Albert Road / Raw Square. Pedestrian phases on all legs excluding Albert Road west approach;
- 60km/h speed limit on Raw Square, 50km/h speed limit on Albert Road and within Pilgrim Avenue;
- Clearways exist within kerbside lanes on either side of Raw Square enforced during the hours of 6-10am and 3-7pm Monday to Friday;
- Albert Road is one-way eastbound to the east of Raw Square;
- Pedestrian footpaths on either side of Pilgrim Avenue, Albert Road and Raw Square.

2.3 Existing Traffic and Parking Environment

Intersection surveys were carried out between 7-9am and 4-6pm on Thursday the 28th May 2015 at the intersections of Pilgrim Avenue / Albert Road and Albert Road / Raw Square. Supplementary intersection surveys were carried out between 7-9am and 4-7pm on Friday 1st December 2017 at the intersections of Raw Square / Albert Road and Raw Square / Leicester Avenue / Everton Rd. Survey data is reproduced in **Annexure B** for reference and a summary of results is presented below.

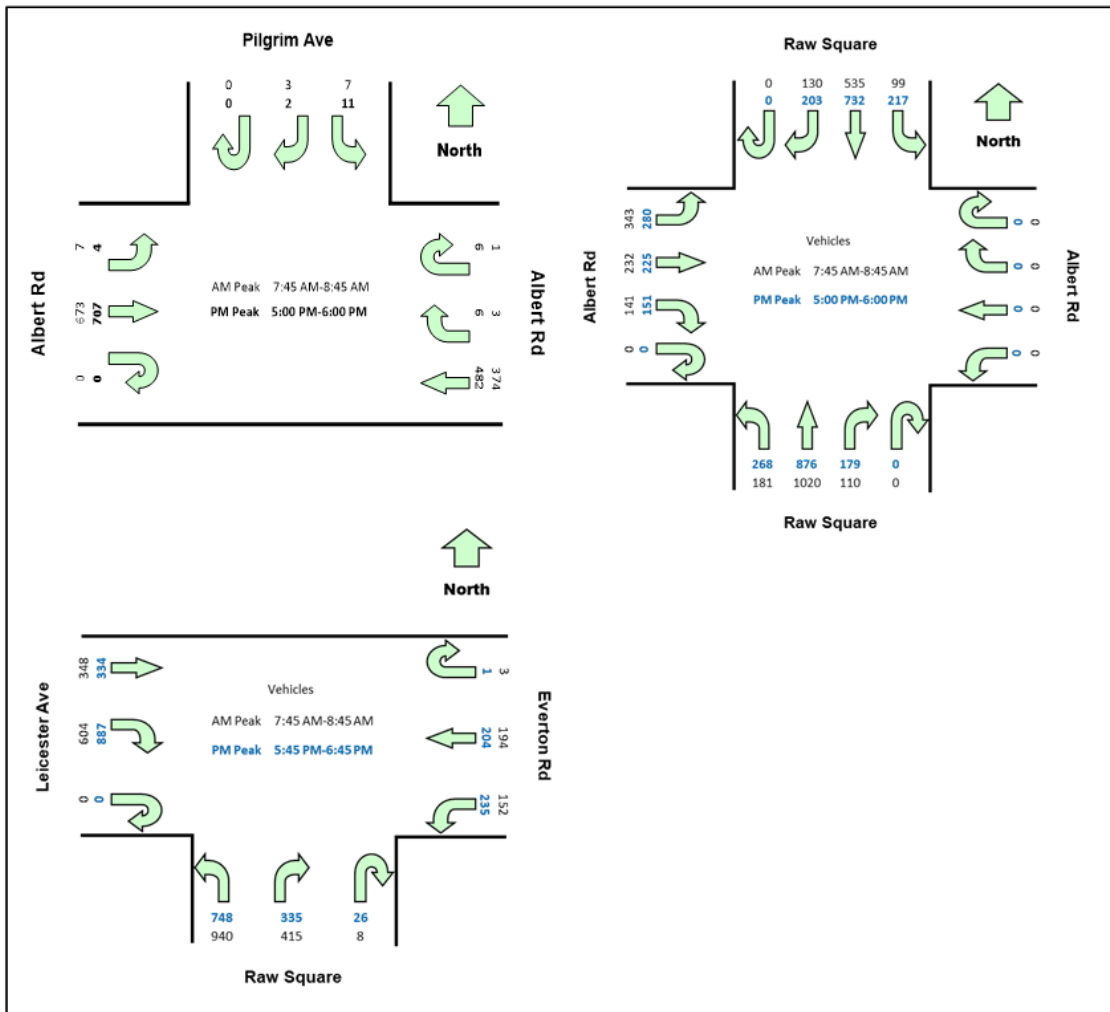


FIGURE 3: TRAFFIC COUNT RESULTS



2.3.1 Intersection Performances

Existing intersection performances have been assessed using SIDRA INTERSECTION 7. The results of this analysis are summarised in **Table 1** with detailed output reproduced in **Annexure C**.

**TABLE 1: INTERSECTION PERFORMANCE – EXISTING
SIDRA INTERSECTION 7.0**

Intersection	Peak Hour	Degree of Saturation ⁽¹⁾	Average Delay ⁽²⁾ (sec/veh)	Level of Service ⁽³⁾	Control Type	Worst Movement	95th Percentile Queue
EXISTING PERFORMANCE							
Albert Road / Pilgrim Avenue	AM	0.17	0.2 (Worst: 27)	A (Worst: B)	Give Way	RT from Pilgrim Avenue	0.1 veh (0.6m) Albert Road (E)
	PM	0.18	0.4 (Worst: 35.6)	A (Worst: C)		RT from Pilgrim Avenue	0.3 veh (2.1m) Albert Road (E)
Raw Square / Albert Road	AM	0.84	19.3	B	Signals	RT from Albert Road (W)	15.9 veh (111.6m) Raw Square (S)
	PM	0.92	27.2	B		RT from Raw Square (N)	20.8 veh (145.4m) Raw Square (N)
Raw Square / Everton Rd	AM	0.65	9.2 (Worst: 14.4)	A (Worst: A)	Roundabout	UT from Everton Rd (E)	5.6 veh (39.4m) Raw Square (S)
	PM	0.82	12.8 (Worst: 21.6)	A (Worst: B)		UT from Everton Rd (E)	13.3 veh (93m) Leicester (W)

NOTES:

- (1) The Degree of Saturation is the ratio of demand to capacity for the most disadvantaged movement.
 (2) The average delay is the delay experienced on average by all vehicles. The value in brackets represents the delay to the most disadvantaged movement.
 (3) The Level of Service is a qualitative measure of performance describing operational conditions. There are six levels of service, designated from A to F, with A representing the best operational condition and level of service F the worst. The LoS of the intersection is shown in bold, and the LoS of the most disadvantaged movement is shown in brackets.

As shown in **Table 1**, the intersections of Albert Road / Pilgrim Avenue, Raw Square / Albert Road and Raw Square / Leicester Ave currently experience generally good levels of service throughout the day (A and B LoS during AM and PM). Right turn volumes are not high though experience a slightly worse level of service due to through volumes on the major roads of each signalised intersection.



2.4 Public Transport

The subject site is located approximately 200m walking distance from Strathfield Railway Station which provides regular and frequent services along the T1 and T2 lines. Strathfield Bus Interchange is also located adjacent to Strathfield Railway Station, which provides numerous routes and services around Strathfield and to surrounding suburbs that are less accessible by trains. Therefore, it can be concluded that the site is well accessed via public transport and the residents, commuters and customers associated with the proposed development will not be disadvantaged in terms of public transport. The location of the site relative to the surrounding public transport infrastructure is illustrated in **Figure 4**.

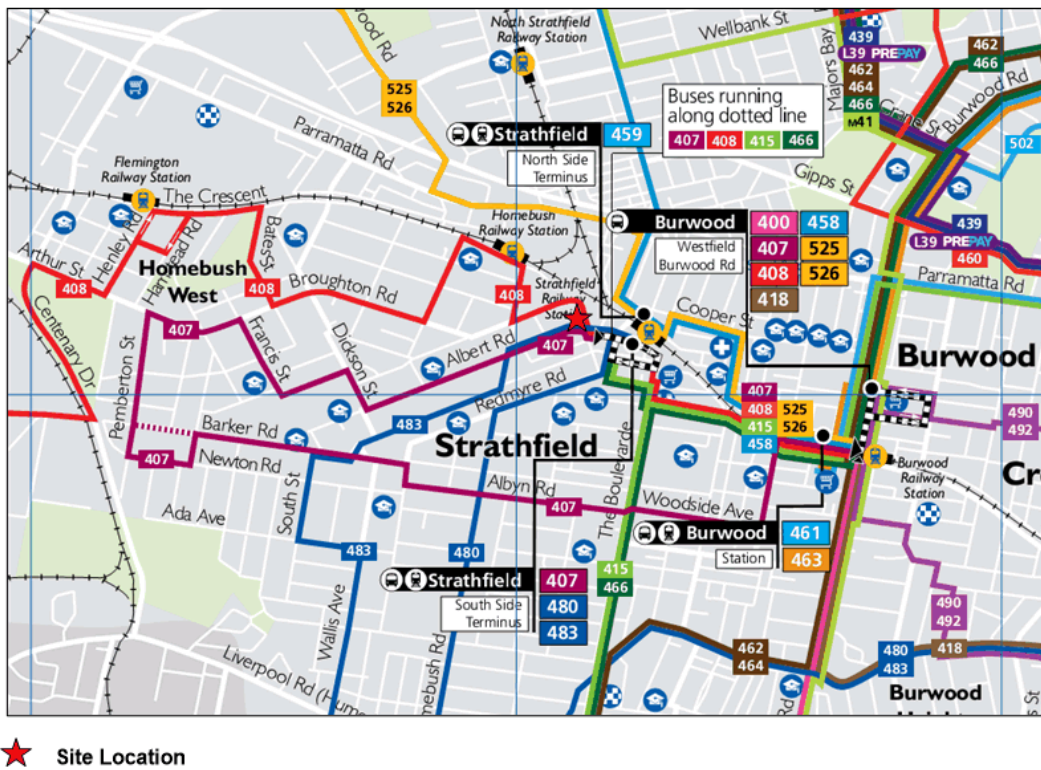


FIGURE 4: PUBLIC TRANSPORT CONTEXT

2.5 Future Road and Infrastructure Upgrades

According to Strathfield Council's website, there are no future planned road works in the vicinity of the site which will impact any proposed development on the site. Implementation of the Strathfield Town Centre Masterplan by Strathfield Council will occur at some stage in the future; improving the Town Centre Precinct public domain, urban design and pedestrianisation. Any street upgrades or the like are not considered to adversely impact the subject site.



3 PARKING ASSESSMENT

3.1 Council Parking Requirement

Reference is made to Strathfield Council's *Development Control Plan (DCP) 2005 – Part I: Provision of Off-Street Parking Facilities* and *Part C: Multiple Unit Housing* which designate the following minimum parking rates applicable to the subject development:

Residential Flat Buildings (DCP Part C) –

1 space per 1 bedroom dwelling

1.5 spaces per 2 bedroom dwelling

2 spaces per 3 bedroom dwelling

1 visitor space per 5 dwellings or part thereof

A designated car wash bay for a development of 10 or more dwellings

Commercial Premises (DCP Part I) –

1 space per 40m² of GFA

Retail (DCP Part I) –

6.2 spaces per 100m gross leaseable floor area.

The resulting car parking requirement is summarised in **Table 2**.

TABLE 2: CAR PARKING REQUIREMENTS

Land Use	Type	Scale	Rate	Spaces Required	Spaces Provided
SITE 1					
Residential	1 bedroom	51	1 space per unit	51	51
	2 bedroom	117	1.5 spaces per unit	175.5 (178)	178
	3 bedroom	0	2 spaces per unit	0	0
	Visitor	168	1 space per 5 dwellings	33.6 (34)	34
Non-Residential	Retail	888m ²	6.2 spaces per 100m ²	56	56
	Commuter			N/A	36
Site 1 Total				319	355
SITE 2					
Residential	2 bedroom	113	1.5 spaces per unit	169.5 (170)	Subject to Development by Others
	Visitor	113	1 space per 5 dwellings	22.6 (23)	
Non-Residential	Retail	471m ²	6.2 spaces per 100m ²	29.2 (30)	
Site 2 Total				223	
Total				542	578⁽¹⁾

Notes: (1) – Subject to the development of the adjacent site including exactly 223 spaces.



As shown, a total of 542 car parking spaces are required for the two sites, consisting of 319 for Site 1 and 223 for Site 2.

These parking requirements outlined in Council's DCP shall be met by the development within the DA and Construction Certificate stages. The concept plans provided demonstrate that sufficient parking can physically be provided on-site, with the detailed design to be completed and assessed at the Development Application stage.

3.2 Bicycle & Motorcycle Parking Requirements

Council's DCP only states that *suitable facilities for accommodating bicycle parking in all residential flat buildings must be provided* but does not outline any specific bicycle parking requirements.

Therefore, reference is made to Austroads Guide to Traffic Engineering Practice Part 14 – Bicycles which prescribes the following rates:

Residential

1 space per 4 units for residents

1 space per 16 units for visitors

Shop

1 per 300m² GFA for employees

1 per 500m² over 1000m² for shoppers

Application of these rates yields a total bicycle parking provision guideline of 95 bicycle spaces (70 for residents, 20 for visitors and 5 for staff).

All bicycle spaces shall be provided in accordance with the appropriate user class facilities as specified in AS2890.3 – *Bicycle Parking Facilities* and the *Austroads Guide to Traffic Engineering Practice – Part 14 – Bicycles*.

The DCP does not specify a parking rate for motorcycles and hence the development does not require a motorcycle parking provision.



3.3 Servicing & Loading

For commercial premises, Council's DCP states the following:

“Provision should be made for at least 1 courier parking space in a convenient and appropriately signposted location preferably with access off the principal street frontage. Additional parking for courier motorcycles would be desirable”.

No specific requirements are stated for residential developments. Detail of loading spaces will be defined in the Development Application Stage, however the scheme indicates that loading and servicing areas can be provided.

Given the scale of the development it is recommended to provide at least one loading/service bay on each site, however, there is also the potential for on-street waste collection in Pilgrim Avenue subject to Council's approval of a waste management plan. A service/loading bay has been shown in the concept layouts of both Site 1 and Site 2 on the ground level.

3.4 Disabled Parking

The required disabled parking should comply with those requirements set out in Council's DCP and the Building Code of Australia (BCA). For the proposed land-uses a rate of 1 disabled space per 100 parking spaces should be provided for visitors. The detailed design should take into consideration that 2.5m head clearance is required above disabled spaces and associated shared spaces.

3.5 Car Park Design & Compliance

Compliance assessments of vehicular access arrangements and internal parking layouts are subject to detailed design assessments at DA stages. The submitted concept plans appear to generally comply with AS2890.1, AS2890.2, AS2890.6 and AS4299 where applicable. Further, there is opportunity to comply with these standards and it is assumed that a compliant parking layout will be achieved at the DA stage.



4 TRAFFIC ASSESSMENT

The impact of the expected traffic generation levels associated with the subject proposal is discussed in the following sub-sections.

The assessment of traffic volumes generated by the development option has been conducted in accordance with the RMS *Guide to Traffic Generating Developments* (October 2002) and more recent supplements. The assessment takes the view that the overall Level of Service (LoS) of nearby critical intersections should be maintained whilst some increase in delay for individual movements could be tolerated, particularly for non-critical movements.

Additionally, along with the performance of the nearby critical intersections of Pilgrim Avenue / Albert Road and Albert Road / Raw Square, due consideration is to be given to the local area's road safety, traffic flow efficiency and local amenity.

4.1 Traffic Generation

As outlined above, the traffic generation rates have been based upon those specified in the RMS *Guide to Traffic Generating Developments* (October 2002). Updated data from the RMS (RMS Technical Direction TDT 2013/04) outlines reduced trip rates for high-density residential developments compared to those found in 2002. Further sensitivity is included by not discounting the existing traffic generation of the residential dwellings on Site 1 or the existing Service Station on Site 2. **Table 3** hence outlines a reasonable worst case traffic generation for the proposed development scale.

As shown, the maximum traffic generation associated with the proposed development is in the order of 160 vehicle trips in the AM and 149 vehicle trips in the PM (AM – 79 inbound and 81 outbound; PM – 72 inbound and 77 outbound) for the site. It should be noted that no reduction for the existing use of the site has been applied and that the above estimate of traffic generation is, therefore, a worst case.

4.2 Traffic Assignment

Given the location of the site, all traffic generation of the site is assumed to travel via Raw Square as a worst case scenario. As such, the traffic generated by the proposed development is assumed to follow that of the existing traffic assignment of Albert Rd, Raw Sq, Leicester and Everton Rd, such that, 33% will use Raw Square southbound and 67% of exiting vehicles will use Raw Square northbound. Of the 67% traveling northbound 70% will travel west along Leicester Road and 30% will travel east. Similarly, 60% of arriving vehicles will use Raw Square south and 40% will use Raw Square north comprising of 70% from Leicester (west) and 30% from Everton (east).

4.3 Traffic Impact

The traffic generation outlined in **Section 4.1 & 4.2** above has been added to the existing traffic volumes recorded. SIDRA INTERSECTION 7.0 was used to assess the performance of the surrounding intersections. The purpose of this assessment is to compare the existing intersection operations to the future scenario under the increased traffic load. The results of this assessment are shown in **Table 4**.



TABLE 3: TRAFFIC GENERATION OF SITE

Land-Use	Scale	Peak Hour Rate	Peak Hour Generation (AM/PM)	Peak Hour Split	
				AM	PM
SITE 1					
Residential	168 units	0.19/0.15 per unit ⁽¹⁾⁽²⁾	32/25	6 in 26 out	20 in 5 out
Retail	888m ² GFA	5.6 trips per 100m ²⁽³⁾	50	25 in 25 out	25 in 25 out
Public Parking	30 units	1 per space ⁽⁴⁾	30	30 in 0 out	0 in 30 out
Site 1 Total			112/105	61 in 51 out	45 in 60 out
SITE 2					
Residential	113 units	0.19/0.15 per unit ⁽¹⁾⁽²⁾	21/17	4 in 17 out	14 in 3 out
Retail	471m ² GFA	5.6 trips per 100m ²⁽³⁾	27	14 in 13 out	13 in 14 out
Site 2 Total			48/44	18 in 30 out	27 in 17 out
Total	-	-	160/149	79 in 81 out	72 in 77 out

- Notes: (1) 0.19 trips in the AM peak hour, 0.15 trips in the PM peak hour;
 (2) Assumes 20% inbound & 80% outbound during AM peak: Vice versa for PM.
 (3) Assumes 50% inbound & 50% outbound during both AM and PM peaks.
 (4) Assumed as a worst case, 100% inbound & 0% outbound during AM peak. Vice versa for PM



**TABLE 4: INTERSECTION PERFORMANCES - FUTURE
(SIDRA INTERSECTION 7.0)**

Intersection	Peak Hour	Degree of Saturation ⁽¹⁾	Average Delay ⁽²⁾ (sec/veh)	Level of Service ⁽³⁾	Control Type	Worst Movement	95th Percentile Queue
EXISTING PERFORMANCE							
Albert Road / Pilgrim Avenue	AM	0.17	0.2 (Worst: 27)	A (Worst: B)	Give Way	RT from Pilgrim Avenue	0.1 veh (0.6m) Albert Road (E)
	PM	0.18	0.4 (Worst: 35.6)	A (Worst: C)		RT from Pilgrim Avenue	0.3 veh (2.1m) Albert Road (E)
Raw Square / Albert Road	AM	0.84	19.3	B	Signals	RT from Albert Road (W)	15.9 veh (111.6m) Raw Square (S)
	PM	0.92	27.2	B		RT from Raw Square (N)	20.8 veh (145.4m) Raw Square (N)
Raw Square / Everton Rd	AM	0.65	9.2 (Worst: 14.4)	A (Worst: A)	Roundabout	UT from Everton Rd (E)	5.6 veh (39.4m) Raw Square (S)
	PM	0.82	12.8 (Worst: 21.6)	A (Worst: B)		UT from Everton Rd (E)	13.3 veh (93m) Leicester (W)
FUTURE PERFORMANCE							
Pilgrim Avenue / Albert Road	AM	0.17	1.2 (Worst: 31.3)	A (Worst: C)	Give Way	RT from Pilgrim Avenue	0.8 veh (5.9m) Albert Road (E)
	PM	0.18	1.2	A		RT from Pilgrim Avenue	1 veh (7.1m) Albert Road (E)
Raw Square / Albert Road	AM	0.87	20.7 (Worst: 32.9)	B (Worst: C)	Signals	RT from Raw Square (N)	17.9 veh (125.1m) Raw Square (S)
	PM	0.92	28	B		RT from Raw Square (N)	21 veh (147.2m) Raw Square (N)
Raw Square / Everton Rd	AM	0.67	9.6 (Worst: 14.8)	A (Worst: B)	Roundabout	UT from Everton Rd (E)	6.3 veh (44.1m) Raw Square (S)
	PM	0.86	14.9 (Worst: 24.1)	B (Worst: B)		UT from Everton Rd (E)	17.2 veh (120.2m)
Raw Square / Driveway	AM	0.41	0.2 (Worst: 10.9)	A (Worst: A)	Give Way	LT from Driveway	0.2 veh (1.6m) Driveway
	PM	0.35	0.2 (Worst: 9.2)	A (Worst: A)		LT from Driveway	0.1 veh (0.7m) Driveway

NOTES:

- (1) The Degree of Saturation is the ratio of demand to capacity for the most disadvantaged movement.
- (2) The average delay is the delay experienced on average by all vehicles. The value in brackets represents the delay to the most disadvantaged movement.
- (3) The Level of Service is a qualitative measure of performance describing operational conditions. There are six levels of service, designated from A to F, with A representing the best operational condition and level of service F the worst. The LoS of the intersection is shown in bold, and the LoS of the most disadvantaged movement is shown in brackets.



The SIDRA intersection model indicates that there will be negligible impact on the surrounding road network as a result of the traffic generated by the proposed development of both Site 1 and Site 2. Nearly all relevant intersections maintain their existing Level of Service, with no significant increase in delay times or queue lengths predicted. The change in Level of Service occurs at the intersection of Raw Square / Everton Road / Leicester Ave which increases from LoS A to LoS B during the PM peak period. Level of Service B however, still has the characteristic of minimal delays and spare capacity and as such is not significantly detrimental to the intersections performance. It should be noted that a worst case has been modelled, with no consideration given to the existing traffic generation of the sites.



5 CONCLUSION

In summary, the planning proposal for two sites at 2-6 Pilgrim Avenue & 11-13 Albert Road, Strathfield and 9 Albert Road, Strathfield for 280 residential units and 1403m² GFA of retail space has been assessed on its traffic and parking impacts.

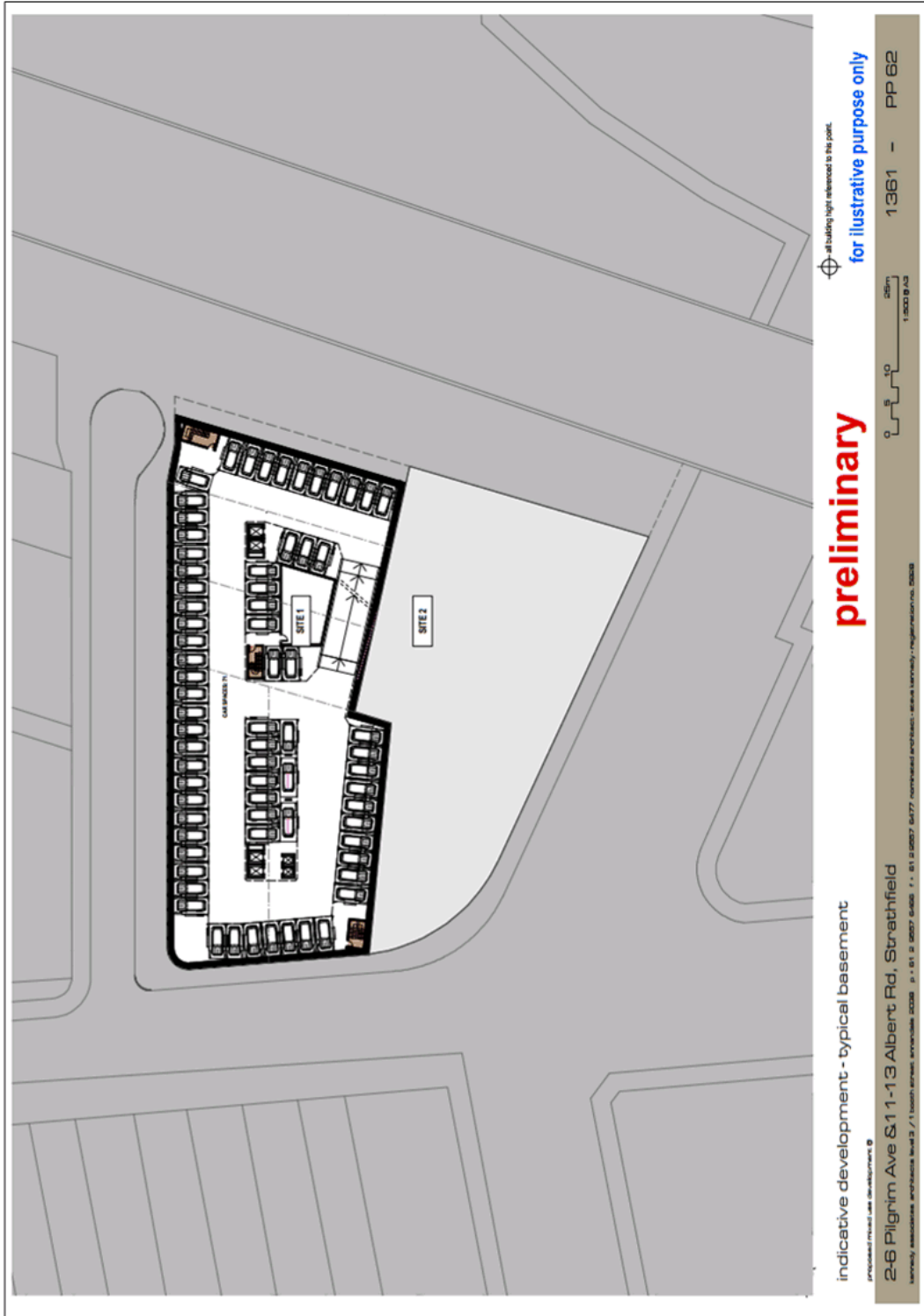
The planning proposal has the capacity to provide some 583 car parking spaces, including 30 spaces for public commuter parking, across 5 basement levels and ground floor, exceeding the requirements of Council's DCP. The concept plans include appropriate loading and servicing facilities and the site has the capacity to meet the requirements of the DCP with regards to both bicycle storage and disabled parking, which can be detailed at the Development Application stage.

The traffic generation associated with the two sites, estimated at a total of 160 vehicle trips in the AM and 149 vehicle trips in the PM (AM – 79 inbound and 81 outbound; PM – 72 inbound and 77 outbound) has been shown to have no noticeable impact on the surrounding road network in terms of intersection delays or queue lengths.

In view of the foregoing, the planning proposal for high-density residential with ground floor retail on the two subject sites is fully supported in terms of traffic and parking impacts.



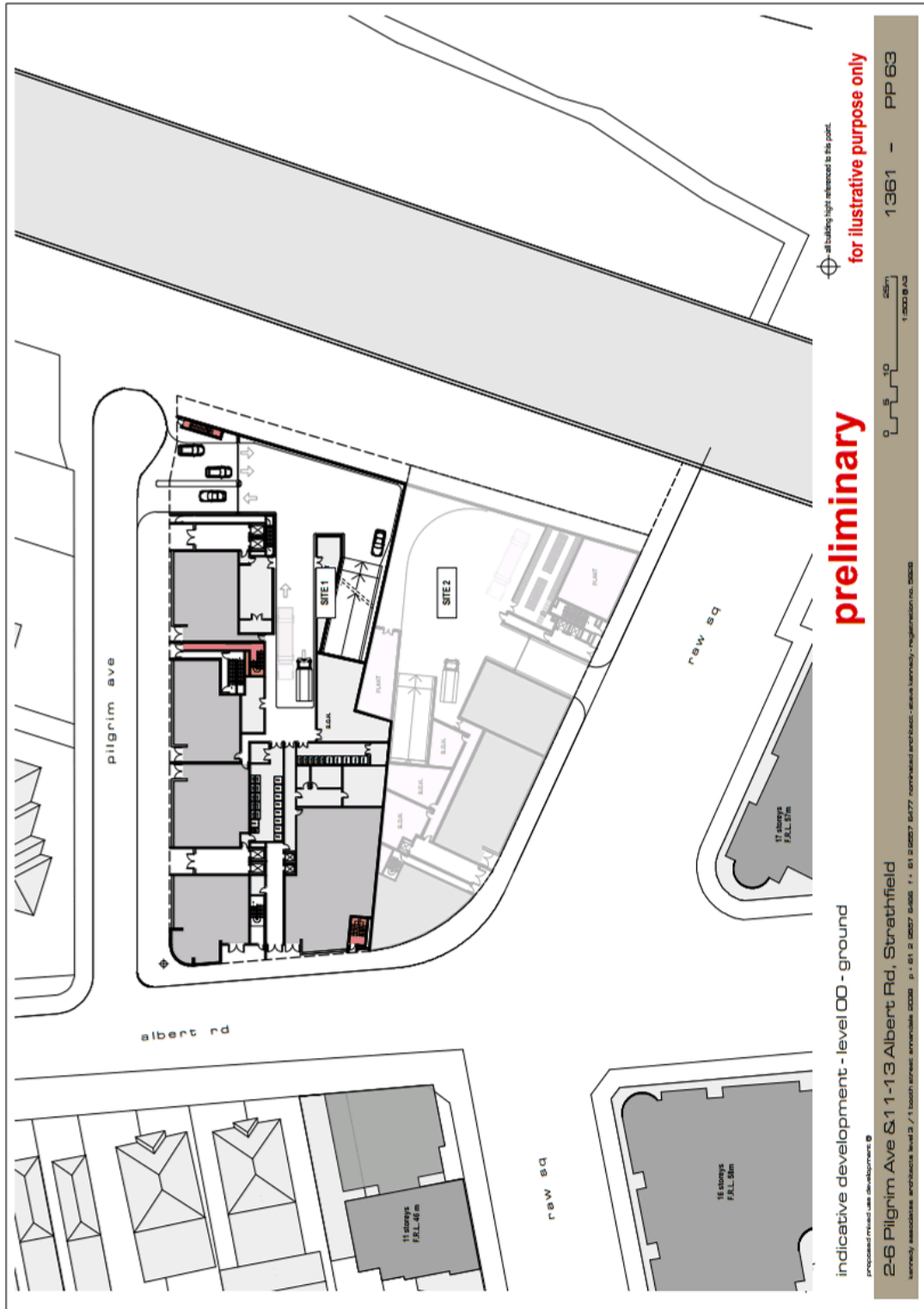
ANNEXURE A: PROPOSED PLANS
(SHEET 1 OF 2)



Mixed-Use Development
2-6 Pilgrim Avenue & 11-13 Albert Road, Strathfield
17156.01FC - 6 February 2018



ANNEXURE A: PROPOSED PLAN
(SHEET 2 OF 2)



all building height referenced to this point.
for illustrative purpose only

preliminary

indicative development - level 00 - ground
PROPOSED MIXED-USE DEVELOPMENT @
2-6 Pilgrim Ave & 11-13 Albert Rd, Strathfield
SAMPLE ASSOCIATION ARCHITECTS LEVEL 3 / 1 SOUTH AVENUE, STRATHFIELD NSW 2159
p. 61 3 0257 5455 f. 61 3 0257 5457 nsw@samplearchitects.com.au sample_architects_nsw_00000

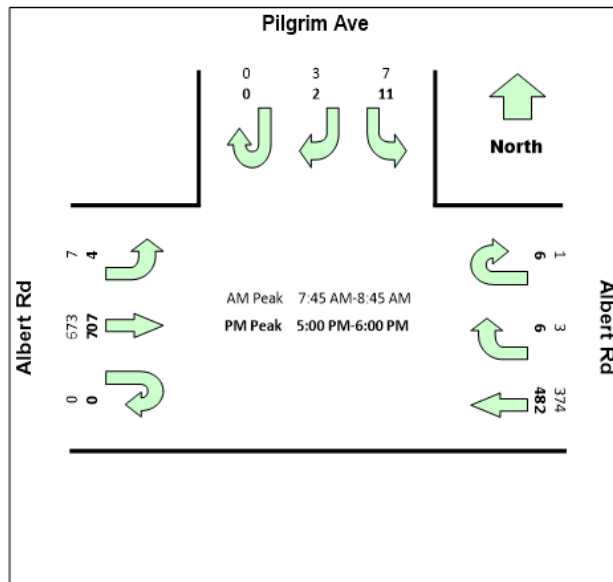
1:361 - PP 63

1:2500 @ A3



**ANNEXURE B: TRAFFIC, PARKING AND SPEED SURVEYS
(SHEET 1 OF 3)**

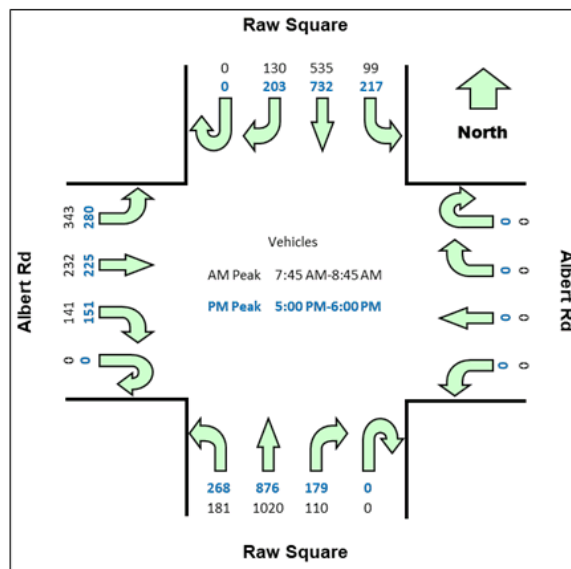
		ABN 18 434 560 435 3 Hepburn Way * Capeline Springs * Victoria * 3023 * Australia Quality data services proven since 2000 Phone: 1300 883 938 * Fax: 1300 882 932				QUALITY ENDORSED COMPANY BY AS/NZS ISO 9001:2008 OH&S SYSTEM CERTIFIED TO AS/NZS ISO 4801:2001						
TURNING MOVEMENT SURVEY Pilgrim Ave and Albert Rd, Strathfield Thursday, 28 May 2015												
Weather: Overcast Suburban: Strathfield Customer: McLaren		Survey Start AM: 7:00 PM: 16:00		Peakhour AM: 7:45 AM-8:45 AM PM: 5:00 PM-6:00 PM								
Time		North Approach Pilgrim Ave			East Approach Albert Rd			West Approach Albert Rd			Hourly Total	
Period Start	Period End	U	R	L	U	R	WB	U	EB	L	Hour	Peak
7:00	7:15	0	2	3	0	1	59	0	98	2	790	
7:15	7:30	0	0	1	1	0	65	0	121	0	901	
7:30	7:45	0	1	0	0	0	75	0	132	0	1012	
7:45	8:00	0	1	1	1	1	71	0	152	2	1068	Peak
8:00	8:15	0	0	3	0	0	101	0	171	1	839	
8:15	8:30	0	0	3	0	2	105	0	186	3		
8:30	8:45	0	2	0	0	0	97	0	164	1		
8:45	9:00	0	0	2	0	0	71	0	159	0		
16:00	16:15	0	1	0	2	2	103	0	133	1	1011	
16:15	16:30	0	0	2	1	3	124	0	117	1	1068	
16:30	16:45	0	2	4	1	3	112	0	125	0	1118	
16:45	17:00	0	0	1	1	2	130	0	137	3	1181	
17:00	17:15	0	1	6	0	2	137	0	151	2	1218	Peak
17:15	17:30	0	1	4	1	2	108	0	182	0		
17:30	17:45	0	0	0	1	0	118	0	190	1		
17:45	18:00	0	0	1	4	2	119	0	184	1		
Peak Time		North Approach Pilgrim Ave			East Approach Albert Rd			West Approach Albert Rd			Peak total	
Period Start	Period End	U	R	L	U	R	WB	U	EB	L		
7:45	8:45	0	3	7	1	3	374	0	673	7	1068	
17:00	18:00	0	2	11	6	6	482	0	707	4	1218	





**ANNEXURE B: TRAFFIC, PARKING AND SPEED SURVEYS
(SHEET 2 OF 3)**

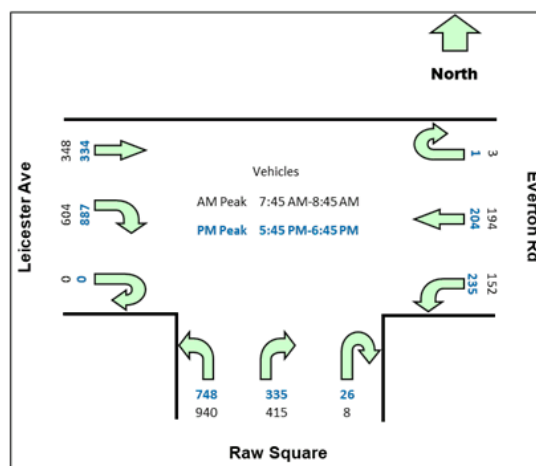
TRANS TRAFFIC SURVEY		TURNING MOVEMENT SURVEY		trafficsurvey.com.au		DNV-GL		DNV-GL		DNV-GL									
Intersection of Raw Square and Albert Rd, Strathfield																			
Date:	Fri 01/12/17	North:	Raw Square	Survey Start	AM: 7:00	PM: 16:00													
Weather:	Overcast	East:	Albert Rd	Vehicular Peakhour	AM: 7:45 AM-8:45 AM	PM: N/A													
Suburban:	Strathfield	South:	Raw Square	AM:	7:45 AM-8:45 AM	PM: N/A													
Customer:	McLaren	West:	Albert Rd	PM:	5:00 PM-6:00 PM	PM: N/A													
All Vehicles																			
Time	North Approach Raw Square	East Approach Albert Rd				South Approach Raw Square				West Approach Albert Rd				Hourly Total					
Period Start	Period End	U	R	SB	L	U	R	WB	L	U	R	NB	L	U	R	EB	L	Hour	Peak
7:00	7:15	0	21	82	20	0	0	0	0	0	22	204	18	0	24	38	44	2373	
7:15	7:30	0	14	99	23	0	0	0	0	0	24	214	40	0	19	44	60	2547	
7:30	7:45	0	34	134	30	0	0	0	0	0	30	234	41	0	33	52	68	2727	
7:45	8:00	0	34	139	30	0	0	0	0	0	37	247	46	0	30	61	83	2791	Peak
8:00	8:15	0	30	104	24	0	0	0	0	0	19	267	37	0	30	51	85	2748	
8:15	8:30	0	31	135	15	0	0	0	0	0	37	269	40	0	45	57	88		
8:30	8:45	0	35	157	30	0	0	0	0	0	17	237	58	0	36	63	87		
8:45	9:00	0	39	131	49	0	0	0	0	0	16	219	34	0	41	47	86		
16:00	16:15	0	42	177	43	0	0	0	0	0	30	231	71	0	31	43	58	2910	
16:15	16:30	0	48	153	47	0	0	0	0	0	30	200	88	0	39	47	87	2995	
16:30	16:45	0	52	162	47	0	0	0	0	0	35	200	56	0	57	57	84	3087	
16:45	17:00	0	37	215	48	0	0	0	0	0	38	175	46	0	43	54	79	3109	
17:00	17:15	0	84	199	59	0	0	0	0	0	37	214	71	0	29	65	73	3131	Peak
17:15	17:30	0	45	183	51	0	0	0	0	0	42	228	78	0	39	58	67	3058	
17:30	17:45	0	41	188	52	0	0	0	0	0	57	226	80	0	33	56	59	3035	
17:45	18:00	0	53	162	55	0	0	0	0	0	43	208	59	0	50	46	81	3047	
18:00	18:15	0	40	179	57	0	0	0	0	0	55	219	59	0	38	42	49	3005	
18:15	18:30	0	59	192	59	0	0	0	0	0	48	204	61	0	40	44	61		
18:30	18:45	0	60	164	68	0	0	0	0	0	48	215	69	0	32	56	72		
18:45	19:00	0	39	134	77	0	0	0	0	0	43	206	60	0	32	52	72		
Peak Time		North Approach Raw Square				East Approach Albert Rd				South Approach Raw Square				West Approach Albert Rd				Peak total	
Period Start	Period End	U	R	SB	L	U	R	WB	L	U	R	NB	L	U	R	EB	L		
7:45	8:45	0	130	535	99	0	0	0	0	0	110	1020	181	0	141	232	343	2791	
17:00	18:00	0	203	732	217	0	0	0	0	0	179	876	268	0	151	225	280	3131	





**ANNEXURE B: TRAFFIC, PARKING AND SPEED SURVEYS
(SHEET 3 OF 3)**

 TURNING MOVEMENT SURVEY Intersection of Raw Square and Everton Rd, Strathfield												
Date:	Fri 01/12/17	North:	N/A	Survey Start	AM: 7:00	PM: 16:00						
Weather:	Overcast	East:	Everton Rd	Vehicular Peakhour	AM: 7:45 AM-8:45 AM		Pedestrians Peakhour					
Suburban:	Strathfield	South:	Raw Square	AM:	7:45 AM-8:45 AM		AM: N/A					
Customer:		West:	Leicester Ave	PM:	5:45 PM-6:45 PM		PM: N/A					
All Vehicles												
Time		East Approach Everton Rd			South Approach Raw Square			West Approach Leicester Av			Hourly Total	
Period Start	Period End	U	WB	L	U	R	L	U	R	EB	Hour	Peak
7:00	7:15	0	34	19	8	64	176	0	96	66	2252	
7:15	7:30	0	34	18	3	69	202	0	115	68	2423	
7:30	7:45	1	37	42	3	73	226	0	153	86	2600	
7:45	8:00	0	47	35	4	92	234	0	164	83	2664	Peak
8:00	8:15	2	39	32	2	109	241	0	124	85	2650	
8:15	8:30	1	54	54	1	105	251	0	126	94		
8:30	8:45	0	54	31	1	109	214	0	190	86		
8:45	9:00	1	44	29	3	111	204	0	175	78		
16:00	16:15	0	34	57	2	86	201	0	203	59	2571	
16:15	16:30	2	57	62	9	87	171	0	177	37	2640	
16:30	16:45	2	54	45	4	90	190	0	212	52	2728	
16:45	17:00	0	54	37	4	65	185	0	259	74	2742	
17:00	17:15	0	51	54	8	89	190	0	260	59	2746	
17:15	17:30	0	42	63	3	86	206	0	213	77	2694	
17:30	17:45	0	47	56	6	82	197	0	219	56	2715	
17:45	18:00	0	43	55	6	110	173	0	209	86	2770	Peak
18:00	18:15	0	41	47	4	79	185	0	225	78	2726	
18:15	18:30	1	55	62	6	69	190	0	242	86		
18:30	18:45	0	65	71	10	77	200	0	211	84		
18:45	19:00	2	40	61	4	85	189	0	185	72		
Peak Time		East Approach Everton Rd			South Approach Raw Square			West Approach Leicester Av			Peak total	
Period Start	Period End	U	WB	L	U	R	L	U	R	EB		
7:45	8:45	3	194	152	8	415	940	0	604	348	2664	
17:45	18:45	1	204	235	26	335	748	0	887	334	2770	





ANNEXURE C: SIDRA RESULTS (SHEET 1 OF 8)

MOVEMENT SUMMARY

Site: 1 [EX AM - ALB / RAW]

Network: N101 [Network1 AM EX]

Albert Road / Raw Square

AM Peak Period

Existing Conditions

Signals - Fixed Time Coordinated Cycle Time = 50 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	191	0.0	191	0.0	0.842	23.0	LOS B	15.9	111.6	0.91	0.94	37.4
2	T1	1074	0.0	1074	0.0	0.842	18.7	LOS B	15.9	111.6	0.92	0.94	13.1
3	R2	116	0.0	116	0.0	0.520	28.9	LOS C	2.8	19.8	0.96	0.78	31.7
Approach		1380	0.0	1380	0.0	0.842	20.1	LOS B	15.9	111.6	0.92	0.93	22.1
North: Raw Square (N)													
7	L2	104	0.0	104	0.0	0.056	5.6	LOS A	0.0	0.0	0.00	0.53	52.3
8	T1	563	0.0	563	0.0	0.443	12.5	LOS A	6.1	42.9	0.77	0.65	29.8
9	R2	137	0.0	137	0.0	0.614	30.6	LOS C	3.5	24.6	0.99	0.83	32.6
Approach		804	0.0	804	0.0	0.614	14.7	LOS B	6.1	42.9	0.71	0.66	34.5
West: Albert Road (W)													
10	L2	361	0.0	361	0.0	0.512	17.7	LOS B	6.0	42.3	0.71	0.77	38.4
11	T1	244	0.0	244	0.0	0.733	25.2	LOS B	5.2	36.1	0.99	0.87	42.2
12	R2	148	0.0	148	0.0	0.733	30.8	LOS C	5.0	34.8	0.99	0.87	31.5
Approach		754	0.0	754	0.0	0.733	22.7	LOS B	6.0	42.3	0.86	0.82	38.6
All Vehicles		2938	0.0	2938	0.0	0.842	19.3	LOS B	15.9	111.6	0.84	0.83	31.6

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 0.0 %

Number of Iterations: 5 (maximum specified: 10)



**ANNEXURE C: SIDRA RESULTS
(SHEET 2 OF 8)**

MOVEMENT SUMMARY

Site: 102 [EX AM - EVRT / RAW]

Network: N101 [Network1 AM EX]

Everton Rd / Raw Square
AM Peak Period
Existing Conditions
Roundabout

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	989	0.0	989	0.0	0.647	7.0	LOS A	5.6	39.4	0.48	0.65	47.0
3	R2	437	0.0	437	0.0	0.647	9.9	LOS A	5.6	39.4	0.48	0.68	33.9
3u	U	8	0.0	8	0.0	0.647	11.6	LOS A	5.6	39.4	0.48	0.68	28.9
Approach		1435	0.0	1435	0.0	0.647	7.9	LOS A	5.6	39.4	0.48	0.66	44.2
East: Everton Rd (E)													
4	L2	160	0.0	160	0.0	0.526	9.2	LOS A	3.4	24.1	0.75	0.93	27.5
5	T1	204	0.0	204	0.0	0.526	9.4	LOS A	3.4	24.1	0.75	0.93	46.7
6u	U	3	0.0	3	0.0	0.526	14.4	LOS A	3.4	24.1	0.75	0.93	31.7
Approach		367	0.0	367	0.0	0.526	9.4	LOS A	3.4	24.1	0.75	0.93	42.2
West: Leicester (W)													
11	T1	366	0.0	366	0.0	0.550	8.9	LOS A	5.2	36.5	0.80	0.76	46.4
12	R2	636	0.0	636	0.0	0.550	12.3	LOS A	5.2	36.5	0.80	0.78	42.6
Approach		1002	0.0	1002	0.0	0.550	11.0	LOS A	5.2	36.5	0.80	0.78	44.1
All Vehicles		2804	0.0	2804	0.0	0.647	9.2	LOS A	5.6	39.4	0.63	0.74	43.9

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).
 Vehicle movement LOS values are based on average delay per movement.
 Intersection and Approach LOS values are based on average delay for all vehicle movements.
 Roundabout Capacity Model: SIDRA Standard.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.
 Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 0.0 %
 Number of Iterations: 5 (maximum specified: 10)



**ANNEXURE C: SIDRA RESULTS
(SHEET 3 OF 8)**

MOVEMENT SUMMARY

Site: 1 [EX PM - ALB / RAW]

Network: N102 [Network2 PM EX]

Albert Road / Raw Square

PM Peak Period

Existing Conditions

Signals - Fixed Time Coordinated Cycle Time = 80 seconds (Practical Cycle Time)

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	282	0.0	282	0.0	0.628	16.0	LOS B	13.2	92.1	0.60	0.64	41.9
2	T1	922	0.0	922	0.0	0.628	12.3	LOS A	13.8	96.5	0.62	0.59	17.4
3	R2	188	0.0	188	0.0	0.451	20.6	LOS B	3.8	26.7	0.85	0.77	36.3
Approach		1393	0.0	1393	0.0	0.628	14.2	LOS A	13.8	96.5	0.65	0.63	30.6
North: Raw Square (N)													
7	L2	228	0.0	228	0.0	0.123	5.6	LOS A	0.0	0.0	0.00	0.53	52.3
8	T1	771	0.0	771	0.0	0.909	43.9	LOS D	20.8	145.4	0.92	1.06	13.1
9	R2	214	0.0	214	0.0	0.920	58.1	LOS E	10.4	73.1	1.00	1.08	23.4
Approach		1213	0.0	1213	0.0	0.920	39.2	LOS C	20.8	145.4	0.76	0.96	21.4
West: Albert Road (W)													
10	L2	295	0.0	295	0.0	0.438	25.2	LOS B	7.6	53.3	0.70	0.76	33.3
11	T1	237	0.0	237	0.0	0.637	35.3	LOS C	7.5	52.5	0.96	0.80	37.8
12	R2	159	0.0	159	0.0	0.637	41.0	LOS C	7.2	50.6	0.96	0.82	27.3
Approach		691	0.0	691	0.0	0.637	32.3	LOS C	7.6	53.3	0.85	0.79	33.9
All Vehicles		3296	0.0	3296	0.0	0.920	27.2	LOS B	20.8	145.4	0.73	0.78	27.5

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 2.9 %

Number of Iterations: 10 (maximum specified: 10)



**ANNEXURE C: SIDRA RESULTS
(SHEET 4 OF 8)**

MOVEMENT SUMMARY

Site: 102 [EX PM - EVRT / RAW]

Network: N102 [Network2 PM EX]

Everton Rd / Raw Square
PM Peak Period
Existing Conditions
Roundabout

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	787	0.0	787	0.0	0.547	6.7	LOS A	4.1	29.0	0.50	0.65	47.0
3	R2	353	0.0	353	0.0	0.547	9.6	LOS A	4.0	28.0	0.50	0.68	33.9
3u	U	27	0.0	27	0.0	0.547	11.3	LOS A	4.0	28.0	0.50	0.68	29.0
Approach		1167	0.0	1167	0.0	0.547	7.7	LOS A	4.1	29.0	0.50	0.66	44.0
East: Everton Rd (E)													
4	L2	247	0.0	247	0.0	0.822	16.5	LOS B	7.4	52.1	0.92	1.17	19.3
5	T1	215	0.0	215	0.0	0.822	16.7	LOS B	7.4	52.1	0.92	1.17	40.2
6u	U	1	0.0	1	0.0	0.822	21.6	LOS B	7.4	52.1	0.92	1.17	25.2
Approach		463	0.0	463	0.0	0.822	16.6	LOS B	7.4	52.1	0.92	1.17	32.9
West: Leicester (W)													
11	T1	352	0.0	352	0.0	0.795	13.2	LOS A	13.3	93.0	0.97	0.94	41.7
12	R2	934	0.0	934	0.0	0.795	17.2	LOS B	13.3	93.0	0.87	0.95	38.1
Approach		1285	0.0	1285	0.0	0.795	16.1	LOS B	13.3	93.0	0.90	0.95	39.2
All Vehicles		2916	0.0	2916	0.0	0.822	12.8	LOS A	13.3	93.0	0.74	0.87	40.1

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).
 Vehicle movement LOS values are based on average delay per movement.
 Intersection and Approach LOS values are based on average delay for all vehicle movements.
 Roundabout Capacity Model: SIDRA Standard.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.
 Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 2.9 %
 Number of Iterations: 10 (maximum specified: 10)



ANNEXURE C: SIDRA RESULTS (SHEET 5 OF 8)

MOVEMENT SUMMARY

Site: 1 [FUT AM - ALB / RAW]

Network: N101 [Network1 AM FUT]

Albert Road / Raw Square

AM Peak Period

Future Conditions

Signals - Fixed Time Coordinated Cycle Time = 50 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	241	0.0	241	0.0	0.871	24.6	LOS B	17.9	125.1	0.92	1.00	36.2
2	T1	1074	0.0	1074	0.0	0.871	20.6	LOS B	17.9	125.1	0.94	1.00	12.0
3	R2	116	0.0	116	0.0	0.520	28.9	LOS C	2.8	19.8	0.96	0.78	31.7
Approach		1431	0.0	1431	0.0	0.871	22.0	LOS B	17.9	125.1	0.94	0.98	21.8
North: Raw Square (N)													
7	L2	104	0.0	104	0.0	0.056	5.6	LOS A	0.0	0.0	0.00	0.53	52.3
8	T1	563	0.0	563	0.0	0.443	12.5	LOS A	6.1	42.9	0.77	0.65	29.8
9	R2	172	0.0	172	0.0	0.770	32.9	LOS C	4.7	33.0	1.00	0.92	31.5
Approach		839	0.0	839	0.0	0.770	15.8	LOS B	6.1	42.9	0.72	0.69	33.9
West: Albert Road (W)													
10	L2	420	0.0	420	0.0	0.595	18.2	LOS B	7.5	52.3	0.75	0.78	38.0
11	T1	244	0.0	244	0.0	0.789	26.3	LOS B	5.7	40.2	1.00	0.91	41.7
12	R2	178	0.0	178	0.0	0.789	32.0	LOS C	5.5	38.6	1.00	0.92	30.7
Approach		842	0.0	842	0.0	0.789	23.5	LOS B	7.5	52.3	0.88	0.85	37.8
All Vehicles		3112	0.0	3112	0.0	0.871	20.7	LOS B	17.9	125.1	0.86	0.87	31.0

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 0.0 %

Number of Iterations: 5 (maximum specified: 10)



**ANNEXURE C: SIDRA RESULTS
(SHEET 6 OF 8)**

MOVEMENT SUMMARY

Site: 102 [FUT AM - EVRT / RAW]

Network: N101 [Network1 AM FUT]

Everton Rd / Raw Square
AM Peak Period
Future Conditions
Roundabout

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	1031	0.0	1031	0.0	0.673	7.2	LOS A	6.3	44.1	0.49	0.66	46.7
3	R2	455	0.0	455	0.0	0.673	10.1	LOS A	6.3	44.0	0.49	0.68	33.6
3u	U	8	0.0	8	0.0	0.673	11.8	LOS A	6.3	44.0	0.49	0.68	28.6
Approach		1494	0.0	1494	0.0	0.673	8.1	LOS A	6.3	44.1	0.49	0.66	43.9
East: Everton Rd (E)													
4	L2	171	0.0	171	0.0	0.553	9.6	LOS A	3.7	26.2	0.78	0.95	26.8
5	T1	204	0.0	204	0.0	0.553	9.9	LOS A	3.7	26.2	0.78	0.95	46.3
6u	U	3	0.0	3	0.0	0.553	14.8	LOS B	3.7	26.2	0.78	0.95	31.3
Approach		378	0.0	378	0.0	0.553	9.8	LOS A	3.7	26.2	0.78	0.95	41.4
West: Leicester (W)													
11	T1	366	0.0	366	0.0	0.573	9.5	LOS A	5.8	40.8	0.84	0.79	45.7
12	R2	660	0.0	660	0.0	0.573	13.0	LOS A	5.8	40.8	0.84	0.81	41.9
Approach		1026	0.0	1026	0.0	0.573	11.8	LOS A	5.8	40.8	0.84	0.80	43.4
All Vehicles		2898	0.0	2898	0.0	0.673	9.6	LOS A	6.3	44.1	0.65	0.75	43.4

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).
 Vehicle movement LOS values are based on average delay per movement.
 Intersection and Approach LOS values are based on average delay for all vehicle movements.
 Roundabout Capacity Model: SIDRA Standard.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.
 Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 0.0 %
 Number of Iterations: 5 (maximum specified: 10)



ANNEXURE C: SIDRA RESULTS (SHEET 7 OF 8)

MOVEMENT SUMMARY

Site: 1 [FUT PM - ALB / RAW]

Network: N101 [Network1 PM FUT]

Albert Road / Raw Square

PM Peak Period

Future Conditions

Signals - Fixed Time Coordinated Cycle Time = 80 seconds (Practical Cycle Time)

Movement Performance - Vehicles

Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	329	0.0	329	0.0	0.667	16.4	LOS B	14.4	101.0	0.63	0.67	41.4
2	T1	922	0.0	922	0.0	0.667	13.2	LOS A	15.3	107.3	0.66	0.63	16.6
3	R2	188	0.0	188	0.0	0.427	20.3	LOS B	3.8	26.8	0.83	0.77	36.5
Approach		1440	0.0	1440	0.0	0.667	14.9	LOS B	15.3	107.3	0.68	0.66	30.5
North: Raw Square (N)													
7	L2	228	0.0	228	0.0	0.123	5.6	LOS A	0.0	0.0	0.00	0.53	52.3
8	T1	771	0.0	771	0.0	0.919	46.3	LOS D	21.0	147.2	0.92	1.08	12.6
9	R2	245	0.0	245	0.0	0.880	51.9	LOS D	11.3	78.9	1.00	1.01	25.0
Approach		1244	0.0	1244	0.0	0.919	39.9	LOS C	21.0	147.2	0.77	0.97	21.4
West: Albert Road (W)													
10	L2	351	0.0	351	0.0	0.503	24.9	LOS B	9.2	64.6	0.71	0.77	33.5
11	T1	237	0.0	237	0.0	0.739	38.0	LOS C	8.5	59.7	0.99	0.87	36.9
12	R2	186	0.0	186	0.0	0.739	43.7	LOS D	8.2	57.3	0.99	0.87	26.2
Approach		774	0.0	774	0.0	0.739	33.4	LOS C	9.2	64.6	0.86	0.82	33.0
All Vehicles		3458	0.0	3458	0.0	0.919	28.0	LOS B	21.0	147.2	0.75	0.81	27.4

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.5 %

Number of Iterations: 10 (maximum specified: 10)



**ANNEXURE C: SIDRA RESULTS
(SHEET 8 OF 8)**

MOVEMENT SUMMARY

Site: 102 [FUT PM - EVRT / RAW]

Network: N101 [Network1 PM FUT]

Everton Rd / Raw Square
PM Peak Period
Future Conditions
Roundabout

Movement Performance - Vehicles													
Mov ID	OD Mov	Demand Flows		Arrival Flows		Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue		Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
		Total veh/h	HV %	Total veh/h	HV %				Vehicles veh	Distance m			
South: Raw Square (S)													
1	L2	826	0.0	826	0.0	0.582	6.8	LOS A	4.6	32.5	0.51	0.65	46.9
3	R2	391	0.0	391	0.0	0.582	9.7	LOS A	4.5	31.3	0.51	0.68	33.8
3u	U	27	0.0	27	0.0	0.582	11.4	LOS A	4.5	31.3	0.51	0.68	28.9
Approach		1244	0.0	1244	0.0	0.582	7.8	LOS A	4.6	32.5	0.51	0.66	43.9
East: Everton Rd (E)													
4	L2	257	0.0	257	0.0	0.863	19.0	LOS B	8.6	60.0	0.95	1.23	17.5
5	T1	215	0.0	215	0.0	0.863	19.2	LOS B	8.6	60.0	0.95	1.23	38.4
6u	U	1	0.0	1	0.0	0.863	24.1	LOS B	8.6	60.0	0.95	1.23	23.5
Approach		473	0.0	473	0.0	0.863	19.1	LOS B	8.6	60.0	0.95	1.23	30.6
West: Leicester (W)													
11	T1	352	0.0	352	0.0	0.845	16.9	LOS B	17.2	120.2	1.00	1.05	38.8
12	R2	956	0.0	956	0.0	0.845	21.4	LOS B	17.2	120.2	0.91	1.06	34.9
Approach		1307	0.0	1307	0.0	0.845	20.2	LOS B	17.2	120.2	0.93	1.06	36.1
All Vehicles		3024	0.0	3024	0.0	0.863	14.9	LOS B	17.2	120.2	0.76	0.92	38.1

Site Level of Service (LOS) Method: Delay (RTA NSW). Site LOS Method is specified in the Network Data dialog (Network tab).
 Vehicle movement LOS values are based on average delay per movement.
 Intersection and Approach LOS values are based on average delay for all vehicle movements.
 Roundabout Capacity Model: SIDRA Standard.
 SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.
 Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.
 Largest change in Average Back of Queue or Degree of Saturation for any lane during the last three iterations: 3.5 %
 Number of Iterations: 10 (maximum specified: 10)

TO: Strathfield Local Planning Panel Meeting - 4 October 2018
REPORT: SLPP – Report No. 2
SUBJECT: DA2015/056/02 - 24 HOMEBUSH ROAD, STRATHFIELD
LOT 11 SECTION 1 DP 400
DA NO. DA2015/056/02

SUMMARY

Proposal: Section 4.56 Modification Application to modify the approved Boarding House incorporating the deletion of the basement and lift core, providing vehicle parking at-grade and modifying the landscaping, window schedule and external material finishes.

Applicant: Design Workshop Australia

Owner: Verani 1888 P/L

Date of lodgement: 20 June 2018

Notification period: 27 June 2018 – 14 July 2018

Submissions received: Two (2) written submissions received

Assessment officer: MLM

Estimated cost of works: Nil

Zoning: R3 – Medium Density Residential - SLEP 2012

Heritage: Heritage Item 139 “Verani” - Victorian villa and is located within Homebush Road Heritage Conservation Area

Flood affected: No

Is a Clause 4.6 variation proposed? No

RECOMMENDATION OF OFFICER: REFUSAL

EXECUTIVE SUMMARY

- 1.0 Approval is sought for modifications to the approved boarding house incorporating the deletion of the basement and lift core, providing vehicle parking at-grade and modifying the landscaping, window schedule and external material finishes.
- 2.0 The plans and documentation submitted as part of the application were notified in accordance with Part L of the Strathfield Consolidated Development Control Plan 2005. Two (2) written submissions were received during this time raising concern for the design, bulk and scale of the development, character of the development, loss of common open space areas, removal of internal lift core and off-street parking.
- 3.0 The proposed development results in a significant departure in off-street car and motorcycle parking required for the site as well as a loss of communal open space and landscaped areas to the rear of the site. It is noted that the parent consent was approved prior to the 1 July 2018 amendment to off-street carparking under the ARH SEPP 2009 which now requires 0.5 spaces to be provided per lodger room as opposed to 0.2 spaces.
- 4.0 The proposed modifications are not considered suitable or appropriate for the site given that it fails to comply with ARH SEPP requirements and presents a significant loss of

DA2015/056/02 - 24 Homebush Road, Strathfield
 Lot 11 Section 1 DP 400 (Cont'd)

amenity for future occupants. Accordingly, the proposed development is recommended for refusal.

BACKGROUND

- 17 November 2015 DA2015/056 for alterations and additions to a Heritage Listed dwelling to provide five (5) boarding rooms and for the construction of a detached four (4) storey building containing (28) boarding rooms with basement parking was refused by Council.
- 13 May 2016 DA2015/056 was approved following a conciliation conference held 23 March, 3 and 13 May 2016 for alterations and additions to an existing two storey heritage listed dwelling to provide 5 boarding rooms, manager's room and demolition of the existing additions to the heritage dwelling and construction of a 4 storey building with 28 boarding rooms and manager's room with common areas over 2 levels of basement car parking.

DESCRIPTION OF THE SITE AND LOCALITY

The subject site is located on the eastern side of Homebush Road, north of the intersection with Beresford Road. The subject site is located approximately 400m from Homebush Station and is south of the railway line. The site contains a frontage width of 15.24m, a depth of 60.96m and a total site area of 929m² (refer to Figure 1).

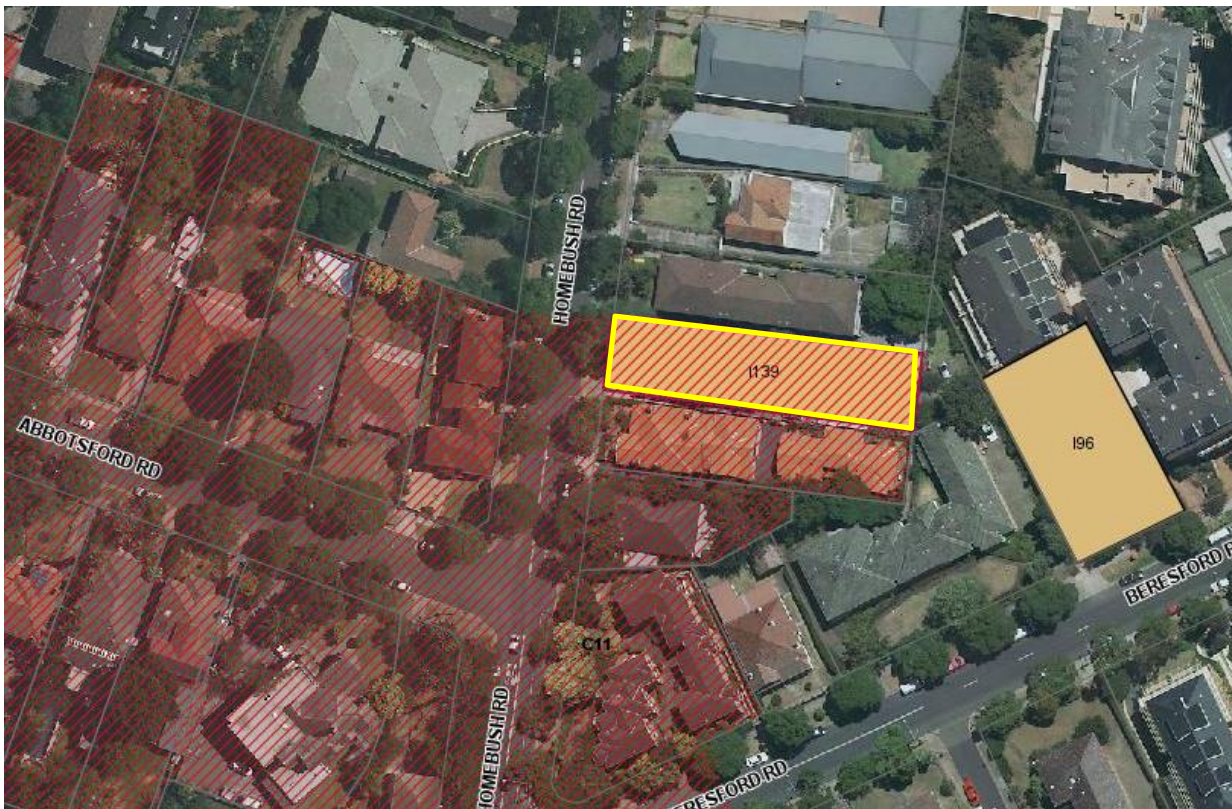


Figure 1: View of subject site including extent of heritage conservation area and heritage listed items within the immediate vicinity of the site.

The site currently contains a Victorian Villa "Verani" (item /139 under the SLEP) and is located within the Homebush Road Heritage Conservation Area and within close proximity of the Abbotsford Road Heritage Conservation Area (refer Figure 1 & 2).

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Lot 11 Section 1 DP 400 (Cont'd)

Vehicular access is provided along the southern side boundary of the site. To the north of the site is a two (2) storey white rendered facebrick residential flat building whilst to the south is a townhouse development containing four (4) dwellings. To the rear of the site are a number of residential flat developments.



Figure 2: Existing heritage listed Victorian Villa located at the subject site.

The streetscape is typified by a mixture of residential flat buildings and dwellings, mostly of brick construction with pitched, tiled roof forms. However, the streetscape is undergoing a state of transition with a number of approvals granted for new residential flat developments with some having already been constructed. The newly approved developments are relatively modern in character providing a flat roof design with contemporary-style finishes such as off-form concrete, alucobond and rendered exterior walls (refer to Figure 3).



Figure 3: View of recently constructed development at 20 Homebush Road.

PROPERTY BURDENS AND CONSTRAINTS

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Lot 11 Section 1 DP 400 (Cont'd)

There are no easements or burdens on the land which could affect, or be affected by, the proposed development.

DESCRIPTION OF THE PROPOSED DEVELOPMENT

The application seeks Council approval for modifications to the approved boarding house incorporating the deletion of the basement and lift core, providing vehicle parking at-grade and modifying the landscaping, window schedule and external material finishes.

The specific elements of the proposal are:

- Removal of basement level and provision of an at-grade carpark to the rear of the site. Seven (7) off-street carparking spaces are proposed including a 2-car stacker and one (1) accessible parking space;
- Relocation of bin room to ground floor and reconfiguration;
- Removal of common room and common open space area in rear portion of building; and
- Modifications to approved landscape plan including loss of (6) canopy trees approved to be planted in the rear in order to accommodate carpark.

REFERRALS

INTERNAL REFERRALS

Waste Comments

Council's Waste Officer has commented on the proposal as follows:

- *“Waste management plan needs to be updated, see below points:*
 - *Bulk collection area must be provided at a rate of 4m² per 10 units.*
 - i) *12.8m² required.*
 - *Waste bin storage area must be provided which accommodates easy access to all waste bins at a rate of 1.1m² of space per bin. Waste bin storage area must accommodate all bins in the one area.*
 - ii) *15.4m² required.”*

The proposed modifications fail to achieve compliance with the abovementioned requirements. In the event of approval, Council's Waste Officer has recommended the imposition of recommended conditions of consent however given that a refusal has been recommended, the non-compliance with Part H of the SCDCP 2005 has been included as a reason for refusal.

Landscaping Comments

Council's Tree Coordinator has commented on the proposal as follows:

“Based on the amended plans, the proposal is not supported primarily due to the noncompliance with SEPP Affordable Rental Housing 2009: 29. (2) (d) (i). In addition, the amended landscape plan results in an inferior outcome to the landscape quality of the proposed development when compared to the original landscape plan LPDA 15 – 341/1 by Conzept Landscape Architects.”

Council's Landscape Architect therefore objects to the proposal.

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Lot 11 Section 1 DP 400 (Cont'd)

Traffic Comments

Council's Traffic Engineer has commented on the proposal as follows:

"I refer to the development application and have the following comments with regards to the application:

I couldn't figure out if the developer is working on behalf of social housing provider.

If not a social housing provider parking needs to be provided at 0.5 per room which is not complaint and application cannot be supported because of parking shortfall."

Council's Traffic Coordinator therefore objects to the proposal.

Heritage Comments

Council's Heritage Advisor has commented on the proposal as follows:

"The modifications sought raise no additional concerns on heritage grounds when compared to the existing approved development, and no additional conditions are recommended."

Council's Heritage Advisor raises no objections to the proposal.

Building Comments

Council's Building Surveyor has commented on the proposal stating that *"there are no issues with the proposal and that BCA issued will be dealt with at BCA stage."*

Council's Building Surveyor raises no objections to the proposal.

SECTION 4.15 CONSIDERATIONS – EP&A Act, 1979

In determining a development application, the consent authority is to take into consideration the following matters of consideration contained within Section 4.15 of the Environmental Planning and Assessment Act, 1979 as relevant to the development application:

4.15(1)(a) the provisions of:

(i) any environmental planning instrument

State Environmental Planning Policy No 55 – Remediation of Land

SEPP 55 applies to the land and pursuant to Section 4.15 is a relevant consideration.

A review of the available history for the site gives no indication that the land associated with this development is contaminated. There were no historic uses that would trigger further site investigations.

The objectives outlined within SEPP55 are considered to be satisfied.

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017

The State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017 replaces the repealed Clause 5.9 of SLEP 2012 (Preservation of Trees and Vegetation).

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 Lot 11 Section 1 DP 400 (Cont'd)

The intent of this SEPP is consistent with the objectives of the repealed Standard where the primary aims/objectives are related to the protection of the biodiversity values of trees and other vegetation on the site.

In this instance, the proposed modifications will not result in any removal of existing trees. Conditions of consent have already been imposed under the parent consent to ensure the Council street tree remain protected during the construction phases of the development.

State Environmental Planning Policy (Affordable Rental Housing) 2009

The proposed development has been lodged under Part 2, Division 3 of the ARH SEPP which relates to the provision of 'boarding houses'.

An assessment of the development against the development standards for boarding houses under the ARH SEPP is presented in the table below.

Clause	Development Control	Required	Proposal	Compliance
26	Permissibility	Permissible in the following zones: <ul style="list-style-type: none"> • R2 Low Density Residential • R3 Medium Density Residential • R4 High Density Residential • B1 Neighbourhood Centre • B2 Local Centre • B4 Mixed Use 	Within a prescribed zone (R3 – Medium Density Residential) under SLEP	Yes
27(2)	Location and access to facilities	If it is located in R2 – must be within an 'accessible area'	N/A – not located in R2 zone	N/A
29 Note: Unable to refuse based on compliance these standards	FSR	Max as per SLEP 2012: PLUS incentive of 0.5:1 if the max FSR is > 2.5:1 Max as per SLEP: 1.45:1 Max permitted: 1.95:1	0.957:1 (888.9m ²)	Yes.
	Height	Max as per SLEP: 14m	Unchanged at 13.61m	Yes.
	Landscaping	Front setback is compatible with streetscape	No change to front setback which importantly contributes to the setting of the heritage item.	Yes.
	Solar Access	At least one (1) common area receives at least three (3) hours of direct sunlight between 9am and 3pm, mid-winter	Centrally located common area receives 2 hours solar access	No – Refer below.
	Private Open Space	Lodgers: Min 20m ² with min. dimension of 3m	Only 8.3m ² private open space area provided. Useable space is only 2.3m x 3.52m Manager 8.9m ² POS.	No – Refer below. Yes.

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 Lot 11 Section 1 DP 400 (Cont'd)

	Parking	In an 'accessible area': 0.5 spaces per room (32 rooms) = 16 spaces required + 1 manager's space =17 total	(7) at-grade spaces proposed to the rear.	No – Refer below.
	Dwelling size	Single: min 12m ² Other: min 16m ² May have a kitchen/bathroom however is not required to.	12.24m ² to 21.2m ² All have private kitchenettes and bathroom facilities.	Yes. Yes.
30	Standards for Boarding Houses	One (1) communal living room required where there are five (5) or more rooms No boarding room > 25m ² No boarding room occupied by > two (2) adult lodgers Adequate bathroom and kitchen facilities available Boarding House Manager where capacity of > 20 lodgers One (1) bicycle space and one (1) motorcycle space per five (5) boarding rooms. = 7 (6.4) spaces each required.	One (1) common living room provided. Max 21.2m ² (room G02). Conditioned under parent consent Adequate communal bathrooms, kitchens and laundry provided. Manager room provided. Ten (10) bicycle spaces provided however only one (1) motorcycle space provided.	Yes. Yes. Yes. Yes. No - Refer below.
30A	Character	A consent authority must not consent to development to which this division applies unless it has taken into consideration whether the design of the development is compatible with the character of the local area.	The proposal is compatible with the medium density character of the area which typically provide residential flat buildings.	Yes.

Comments: The proposal seeks an at-grade carpark to the rear of the site in lieu of a basement carpark. It is appreciated that at the time consent was granted for the parent application, a rate of 0.2 parking spaces per lodger room applied pursuant to the ARH SEPP 2009. However, on 1 July 2018, an amendment was made to the legislation requiring a minimum 0.5 parking spaces per lodger room to be provided. There are no savings provisions to the amendment hence, the new off-street parking rates were required to be applied in the assessment of the subject application. A minimum of (17) off-street parking spaces, as well as a minimum of (7) motorcycle and (7) bicycle spaces are therefore required to be provided on the site.

The proposed modifications result in only seven (7) carparking spaces as well as only one (1) motorcycle space provided on the site. This presents a significant departure from the minimum (17) car spaces and (6) motorcycle spaces required as per the ARH SEPP 2009. Whilst it is acknowledged that the site is located within an accessible area, the site must comply with the

DA2015/056/02 - 24 Homebush Road, Strathfield
 Lot 11 Section 1 DP 400 (Cont'd)

minimum carparking rates which are already substantially reduced from that of the minimum off-street parking spaces required for typical residential flat developments located in the streetscape and wider LGA. Accordingly, the proposed modifications to the off-street parking are not considered suitable for the site and are therefore unable to be supported.

Clause 29(2)(c) of the ARH SEPP 2009 requires at least one (1) common area to receive a minimum three (3) hours of direct sunlight between 9am and 3pm, mid-winter. Based on the solar access diagrams approved under the original application, the central common room only receives 2 hours solar access resulting in a departure from the minimum 3 hour requirements. This was considered acceptable under the parent application as there was a second common room to the rear of the site which achieved the minimum 3 hours solar access as required. Given that the common room to the rear has been deleted, the central common room must comply with the minimum 3 hours requirements. Accordingly, the departure is not considered acceptable.

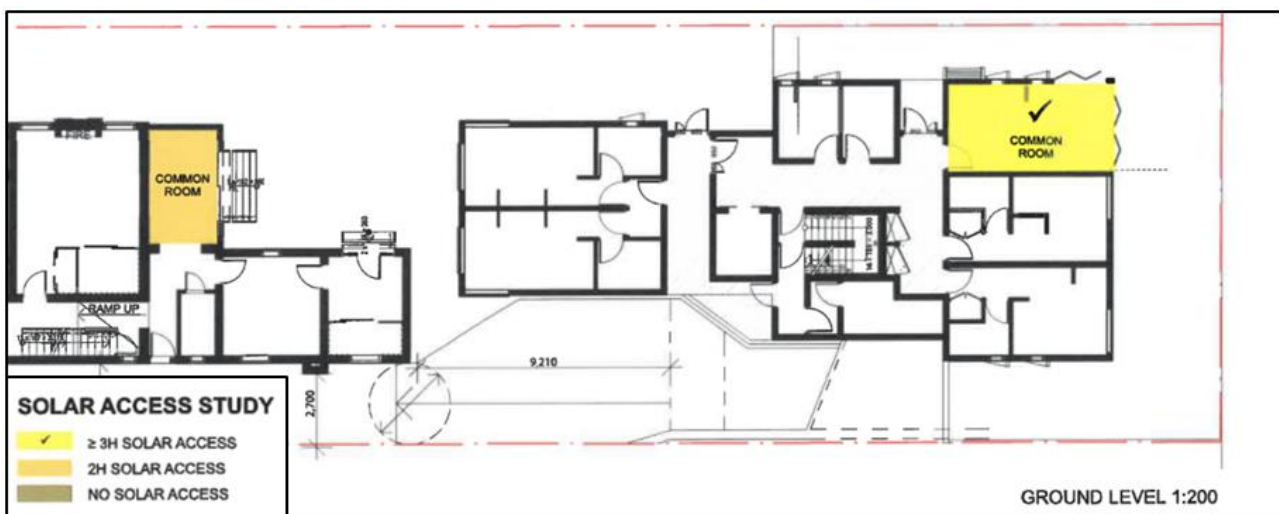


Figure 4: Excerpt from court approved plans demonstrating solar access to common rooms.

STRATHFIELD LOCAL ENVIRONMENTAL PLAN (SLEP) 2012

An assessment of the proposal against the general aims of SLEP 2012 is included below:

Cl. 1.2(2)	Aims	Complies
(a)	To achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield	No
(b)	To promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development	Yes
(c)	To promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community	Yes
(d)	To provide opportunities for economic growth that will enhance the local community	Yes
(e)	To promote future development that integrated land use and transport planning, encourages public transport use, and reduced the traffic and environmental impacts of private vehicle use	Yes
(f)	To identify and protect environmental and cultural heritage	Yes
(g)	To promote opportunities for social, cultural and community activities	Yes
(h)	To minimise risk to the community by identifying land subject to flooding and restricting incompatible development	Yes

Comments: The proposed modifications are contrary to Clause 1.2(2)(a) of the SLEP 2012 as the design fails to achieve a high quality urban form. This is due to the significant loss of landscaped

**DA2015/056/02 - 24 Homebush Road, Strathfield
Lot 11 Section 1 DP 400 (Cont'd)**

space, quality communal open space area and off-street parking which was originally to be provided for the site. The loss of such spaces will dramatically reduce residential amenity for future occupants of the site.

Permissibility

The subject site is Zoned R3 – Medium Density Residential under Strathfield Local Environmental Plan (SLEP) 2012.

Boarding houses are permissible within the R3 – Medium Density Residential Zone with consent and are defined under SLEP 2012 as follows:

“Boarding house means a building that:

- (a) is wholly or partly let in lodgings, and
- (b) provides lodgers with a principal place of residence for 3 months or more, and
- (c) may have shared facilities, such as a communal living room, bathroom, kitchen or laundry, and
- (d) has rooms, some or all of which may have private kitchen and bathroom facilities, that accommodate one or more lodgers,

but does not include backpackers’ accommodation, a group home, hotel or motel accommodation, seniors housing or a serviced apartment.”

The proposed development for the purpose of a boarding house is consistent with the definition above and is permissible within the R3 – Medium Density Residential Zone with consent.

Zone Objectives

An assessment of the proposal against the objectives of the R3 – Medium Density Residential Zone is included below:

Objectives	Complies
• To provide for the housing needs of the community within a medium density residential environment.	Yes
• To provide a variety of housing types within a medium density residential environment.	Yes
• To enable other land uses that provide facilities or services to meet the day to day needs of residents.	Yes

Part 4: Principal development standards

No modifications are sought to the overall building height of the development.

Part 5: Miscellaneous Provisions

The relevant provisions contained within Part 5 of the SLEP 2012 are addressed below as part of this assessment:

5.10 Heritage Conservation

Clause 5.10 of the SLEP seeks to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views. The subject site is identified as a Heritage Listed Item “Verani” – Victorian villa, Item 1139, and is located within the Homebush Road Conservation Area (C11) under Schedule 5 of the SLEP 2012. The site is also in the vicinity of a listed item to the rear, “Wairuna” – Victorian Italianate style house, Item 196, located at 11-13 Beresford Road.

DA2015/056/02 - 24 Homebush Road, Strathfield
 Lot 11 Section 1 DP 400 (Cont'd)

Council's Heritage Advisor has commented on the proposal noting that the "*modifications sought do not alter the impact of the proposal on the nearby heritage property or the Homebush Road Conservation Area.*" It is further noted that "*the proposal lessens the potential for damage to the item arising from excavation associated with the approved 2 level basement; however the amendments do not alter the overall impact of the proposal on the setting of the item, impact on view from nearby heritage items, or the presentation of the site to the Homebush Road Conservation Area.*"

Part 6: Local Provisions

The relevant provisions contained within Part 6 of the SLEP 2012 are addressed below as part of this assessment:

6.1 Acid sulfate soils

The subject site is identified as having Class 5 soils and is not located within 500m of Class 1, 2, 3 or 4 soils. Therefore, the proposed development was not required to be accompanied by an Acid Sulfate Soils Management Plan and has satisfied the requirements of Clause 6.1 of the SLEP, 2012.

6.2 Earthworks

No earthworks are proposed as part of this modification application however, conditions relating to earthworks for the basement have been previously conditioned under the parent consent.

6.4 Essential services

The proposed development has been designed having consideration for the supply of water, electricity, sewerage, waste, stormwater and suitable vehicular access. Standard conditions of consent have been imposed under the parent consent to ensure that these services are provided for the site.

4.15 (1)(a)(ii) any draft environmental planning instruments

There are no applicable draft planning instruments that are or have been placed on public exhibition, to consider as part of this assessment.

4.151)(a)(iii) any development control plan

STRATHFIELD CONSOLIDATED DEVELOPMENT CONTROL PLAN (SCDCP) 2005

The following is an assessment of the proposal's compliance with the relevant provisions contained within SCDCP 2005.

Heritage and Conservation

Cl. 1.11	Aims	Complies
A	To preserve and enhance the visual and environmental amenity of heritage items and heritage conservation areas within the municipality of Strathfield	Yes
B	Ensure all new development affecting heritage items and conservation areas is designed to be compatible in setting, scale, setbacks, form, materials and character with the building and surrounding area	Yes
C	Ensure that development in the vicinity of a heritage item or conservation area does not have any adverse impact on the heritage significance or setting and that development is compatible in setting, scale, setbacks, form, materials and character with the item or conservation area	Yes
D	Conserve archaeological sites and places of Aboriginal significance	Yes

DA2015/056/02 - 24 Homebush Road, Strathfield
Lot 11 Section 1 DP 400 (Cont'd)

Cl. 1.11 Controls	Complies
(1) A Statement of Heritage Impact is required for proposed development: a) affecting a heritage item; b) within a heritage conservation area; or c) in the vicinity of an item or heritage conservation area	Yes
(2) This statement must set out the heritage significance of the structure or place and assess the extent to which carrying out of the proposed development would affect the significance of the heritage item or heritage conservation area concerned and outline measures to minimise any identified impact	Yes

Comments: As previously discussed, the proposed modifications involve removal of the approved basement to provide at-grade carparking to the rear of the property. As previously mentioned, Council's Heritage Advisor has advised that the proposed works will not result in any adverse impact to the presentation of the heritage building in the streetscape.

PART H - WASTE MANAGEMENT (SCDCP 2005)

As previously mentioned, Council's Waste Officer has identified that the:

- "Waste management plan needs to be updated, see below points:
 - Bulk collection area must be provided at a rate of 4m² per 10 units.
 - iii) 12.8m² required.
 - Waste bin storage area must be provided which accommodates easy access to all waste bins at a rate of 1.1m² of space per bin. Waste bin storage area must accommodate all bins in the one area.
 - iv) 15.4m² required."

Based on the plans submitted to Council, it appears that there are two (2) separate bin storage rooms proposed which provide a combined area of 10.65m². This presents a 2.15m² departure from the minimum requirements with less than 1.1m² space provided per bin as required above. Further, the proposal also fails to provide any bulk collection space on the site. The bulk collection area should be a minimum 12.8m². As such, the proposal fails to achieve compliance with the waste management requirements under Part H of the SCDCP 2005.

4.15 (1)(a)(iia) any planning agreement or draft planning agreement

No planning agreement has been entered into under Section 7.4 of the *Environmental Planning and Assessment Act 1979*.

4.15 (1)(a)(iv) matters prescribed by the regulations

Clause 92 of the *Environmental Planning and Assessment (EP&A) Regulation 2000* requires Council to take into consideration the provisions of the Government Coastal Policy and Australian Standard AS2601–1991: *The Demolition of Structures*, in the determination of a development application.

Having regard to these prescribed matters, the proposed development is not located on land subject to the Government Coastal Policy as determined by Clause 92(1)(a)(ii) however does involve the demolition of a building for the purposes of AS 2601 – 1991: *The Demolition of Structures*.

Should this application be approved, appropriate conditions of consent are included within the recommended to ensure compliance with any relevant regulations.

DA2015/056/02 - 24 Homebush Road, Strathfield
Lot 11 Section 1 DP 400 (Cont'd)

4.15(1)(b) *the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality*

As previously discussed, the proposed removal of the basement carpark to provide at-grade carparking presents a significant departure in off-street parking for both cars and motorcycles required for future occupants of the site. The proposed modifications also result in a consequential loss in deep soil zone, communal open space and enclosed communal areas on the site. Whilst the centrally located communal open space area has been retained, it is undersized and will receive restricted solar access throughout the day contrary to the requirements of the ARH SEPP.

The proposal therefore results in a significant loss of amenity for future lodgers of the site and accordingly is not supported.

4.15 (1)(c) *the suitability of the site for the development*

The proposed modifications are not considered to be suitable to the site in that the proposal fails to comply with the minimum off-street parking spaces required and results in a significant loss of quality landscaping and common open space areas on the site. This will result in a significant loss of amenity for future occupants of the site and is therefore considered unsuitable.

4.15 (1)(d) *any submissions made in accordance with this Act or the regulations*

The application was notified in accordance with Part L of the SCDCP 2005 from 27 June 2018 to 14 July 2018, with two (2) submissions received, raising the following concerns:

1. *The proposal is an over development of the site and the proposed design, bulk and scale of the boarding house are excessive and is out of character with existing development in the locality.*

Assessing officer's comments: The proposed development results in the same height and FSR and will retain a similar bulk and scale to what was originally approved.

2. *The residents will lose a 15.6m² common enclosed area and 33.46m² common spaces. The remaining common space – size undefined on the plans – is located to the rear of the existing building with no internal access from the building. Communal space was already minimal in this development.*

Assessing officer's comments: The retained common open space areas on the site are non-compliant with the ARH SEPP 2009 and result in a significant loss of amenity for future occupants to the site. The proposed removal of the common room to the rear is not supported.

3. *Access to the upper floors will be by stairs only – a third world solution not appropriate for Strathfield in the 21st century.*

Assessing officer's comments: Council's Building Surveyor has reviewed the modified plans and has agreed all BCA issues can be dealt with at BCA stage. Hence, lifts are not required to service the building.

4. *The amended plan severely impacts the amenity of residents and modifications as requested should be rejected and a new development application made.*

Assessing officer's comments: As discussed throughout the report, the loss in landscaped space and communal open space areas have detracted from the amenity of future residents on the site and is therefore unsupported.

DA2015/056/02 - 24 Homebush Road, Strathfield
Lot 11 Section 1 DP 400 (Cont'd)

5. *The proposal reduces the car spaces to 7 (Court approval states 8). The SEE claims to comply with the requirements in clause (ia) requiring, for 33 boarding rooms, 17 spaces + 1 managers space = 18 spaces. Any parking variation to the original DA should comply with current requirements.*

Assessing officer's comments: As previously discussed, the proposal presents a significant departure from the minimum (18) off-street parking requirements as per the ARH SEPP. As such, the proposed modifications are inadequate and are unable to be supported.

4.15 (1)(e) the public interest

The public interest is served through the detailed assessment of this development application under the relevant local planning controls and legislation and consideration of any submissions received relating to it by Council. The proposed modifications are considered to be contrary to the public interest as the proposal results in a significant loss of amenity for future occupants of the site.

SECTION 7.11 CONTRIBUTIONS

Section 7.11 of the EP&A Act 1979 relates to the collection of monetary contributions from applicants for use in developing key local infrastructure. The Act reads as follows:

- “(1) If a consent authority is satisfied that development for which development consent is sought will or is likely to require the provision of or increase the demand for public amenities and public services within the area, the consent authority may grant the development consent subject to a condition requiring:*
- (a) the dedication of land free of cost, or*
 - (b) the payment of a monetary contribution,*
- or both.*
- (2) A condition referred to in subsection (1) may be imposed only to require a reasonable dedication or contribution for the provision, extension or augmentation of the public amenities and public services concerned.”*

STRATHFIELD DIRECT SECTION 7.11 CONTRIBUTIONS PLAN

Section 7.11 Contributions are not applicable to the proposed development in accordance with the Strathfield Direct Development Contributions Plan 2010-2030 as they have been levied previously under the parent consent.

CONCLUSION

The application has been assessed having regard to Section 4.15 of the Environmental Planning and Assessment Act 1979, the Strathfield Local Environmental Plan 2012 and the Strathfield Development Control Plan 2005 and is considered to be unsatisfactory for approval.

RECOMMENDATION

That Development Application No. 2015/056/02 to modify the approved Boarding House incorporating the deletion of the basement and lift core, providing vehicle parking at-grade and modifying the landscaping, window schedule and external material finishes at 24 Homebush Road, Strathfield be **REFUSED**, for the following reasons:

1. The proposed modification fails to comply with the minimum (17) off-street car parking

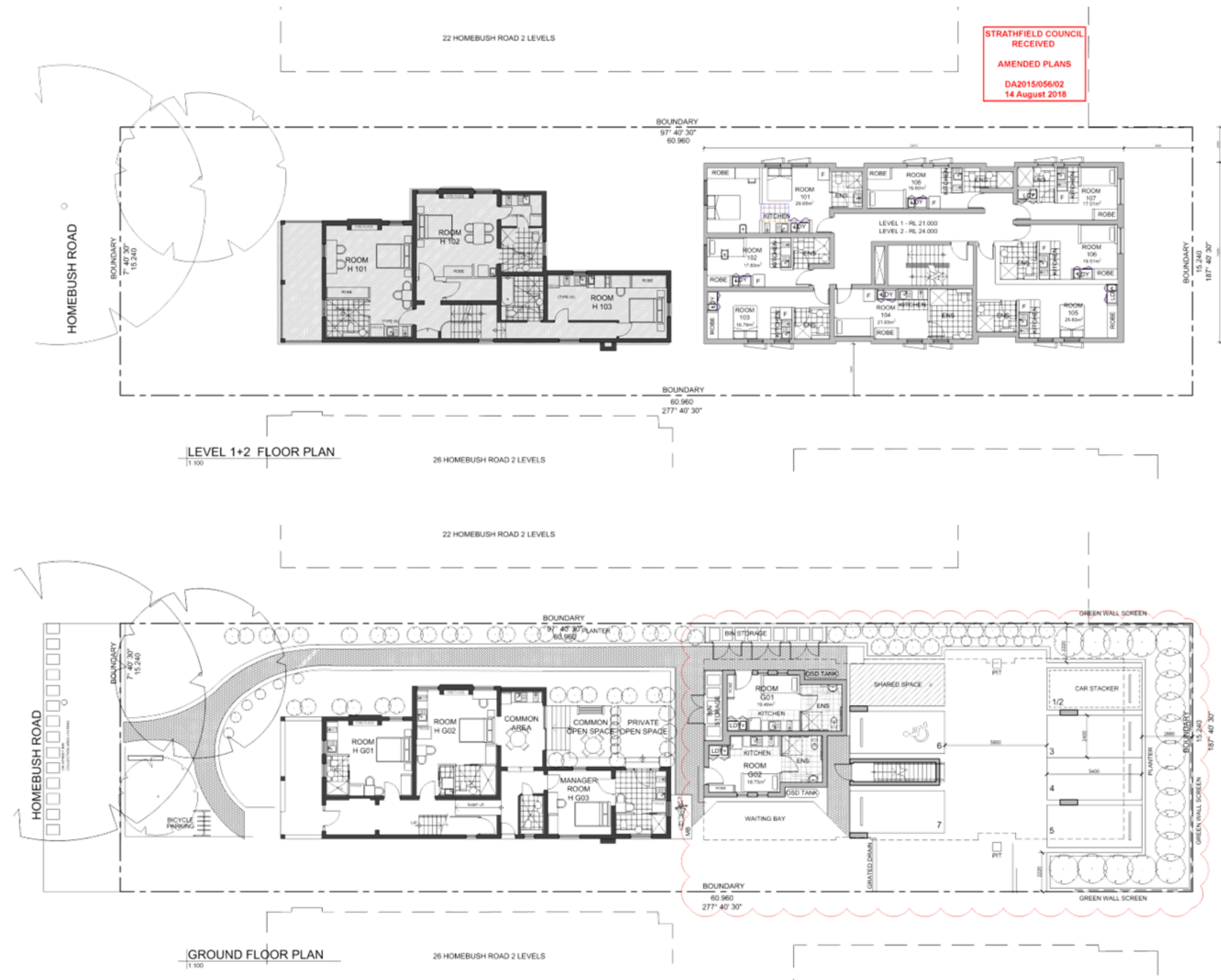
DA2015/056/02 - 24 Homebush Road, Strathfield
Lot 11 Section 1 DP 400 (Cont'd)

requirements as per Clause 29(e) of the *Affordable Rental Housing SEPP 2009* which requires a minimum 0.5 spaces to be provided for the each boarding room (Section 4.15(a)(i) of the *Environmental Planning and Assessment Act 1979*).

2. The proposed modification fails to comply with the minimum seven (7) motorcycle spaces required to be provided as per Clause 30(h) of the *Affordable Rental Housing SEPP 2009* which requires a minimum 1 space to be provided per 5 lodger rooms (Section 4.15(a)(i) of the *Environmental Planning and Assessment Act 1979*).
3. The proposed modification fails to comply with the minimum three (3) hours direct solar access required to be provided to the communal living room pursuant to Clause 29(2)(c) of the *Affordable Rental Housing SEPP 2009* (Section 4.15(a)(i) of the *Environmental Planning and Assessment Act 1979*).
4. The proposed modification fails to comply with the minimum waste storage and bulk storage collection areas as required to be provided under Part H 'Waste Minimisation and Management Plan' of the SCDP 2005 (Section 4.15(a)(iii) of the *Environmental Planning and Assessment Act 1979*).
5. The proposed modification is inconsistent with the aims of Clause 1.2(2) of the Strathfield Local Environmental Plan 2012 which requires the development to achieve a high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield (Section 4.15(a)(i) of the *Environmental Planning and Assessment Act 1979*).
6. The proposed modification is likely to result in unreasonable adverse impacts on the amenity of residents both on and off the site due to insufficient on-site parking. The proposed modification would also likely result in adverse amenity impacts for future residents due to substantial loss of landscaped space and common open space areas on the site. This includes loss of solar access to the common room (Section 4.15(1)(b) of the *Environmental Planning and Assessment Act 1979*).
7. The proposed modification is not considered to be suitable for the site (Section 4.15 (1) (c) of the *Environmental Planning and Assessment Act 1979*).
8. The proposed modification is not in the public interest (Section 4.15 (1) (d)(e) of the *Environmental Planning and Assessment Act 1979*).

ATTACHMENTS

1. [↓](#) Architectural Plans



DISCLAIMER
The engineer, architect and designer (Design Approval Development Application) (DADA) are not responsible for any and all errors or omissions in the design or construction of any work. Copyright © 2018.

REF.	DATE	AMENDMENT	BY
1	24.08.2018	ISSUE FOR INFORMATION	SL
2	26.08.2018	FOR APPROVAL	SL
3	02.09.2018	AMENDED FLOOR LAYOUT & STAIRS	SL

CONSTRUCTION NOTES

General Requirements:

- Window sizes are appropriate and are to be verified on site prior to fabrication.
- All glass fixtures to be suitable to current glazing code.
- All fixtures to be suitable for intended production structure.
- All windows to comply with current wind loads.
- All window attachments to be complete with weather strips, gaskets, flashings and fixings to comply.
- To not locate window openings off ceiling.
- All windows to be fully opened and specified by engineer.
- Engineer certifies in accordance with engineering details.
- All DR connections to be confirmed with.
- Provide full fix at areas to achieve adequate drainage treatment to outside.
- Building to provide tapping to ensure acceptance by structural and hydraulic engineers.
- Planning to AS2031, building to detail and confirm all pipe systems and appliances with client prior to any work commencing.
- All gas calculations to comply with Australian Standards, shop drawings to be provided for approval prior to any fabrication.
- To be confirmed by structural engineer.
- Waterproofing to all areas to be suitable for purpose. All waterproofing to be confirmed by specialist hydraulic consultant.
- All materials and products to be provided for all waterproofing.
- All steel fixings, bolts and structural steel to be stainless steel unless otherwise noted.
- Support timber to be checked by builder. All timbers to be suitable for outdoor exposure.
- All mortar and concrete to be air entrained.
- Sound transmission and insulation. Builder to achieve BCA compliance for sound control in accordance with relevant BCA.
- Service ducts and pipes to comply with BCA requirements.
- Builder to engage an acoustic engineer for specific specialist advice for sound control.
- Builder to provide full certificate of hotting material used and warranty of supply and installation.
- All service ducts/penetrations to be fully sealed and the rated in BCA and Australian standard requirements.
- All steel fixings to be hot dip galvanized in accordance with BCA/Australian standards. (20% separation to all fixings and corners in accordance with BCA).
- All masonry work to be in accordance with BCA.
- "Homing" or "Homing" type management barriers to be used to cover all aspects of termite protection to comply with AS 3602.
- Cladding to comply with AS 1562.1.
- Structural ventilation system to be constructed in compliance with Part 3.5.5. ventilation of the BCA.
- Full compliance with Section 3.1 - requirements.
- Full compliance with AS/NZS 3600 - requirements.
- Builder shall fabricate and install all steel structural elements from steel fabricator and shall ensure 100% cover prior to commencing work.
- Minimum 30mm gap system - 2 pack epoxy system.
- 1 - waterproof membrane 2 - primer 3 - concrete 4 - concrete 5 - concrete.

ACCESS CONSULTANTS

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ROSWAY NSW 2126
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HERITAGE 21
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BILL MUIR CONSULTING LANDSCAPE ARCHITECT
10/105 CONNORS ST
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FAX: 0417 774 281
EMAIL: bill@billmuir.com.au

STRUCTURAL ENGINEER / DESIGN / TRADES

E PLANNING
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FAX: 0437 864 076
EMAIL: eplan@eplan.com.au

WASTE CONSULTANT

ELEPHANTS FOOT
SUITE 26, LEVEL B
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WARRANE QLD 4000
PHONE: 07 5594 2789
FAX: 07 5594 2789
EMAIL: info@elephantsfoot.com.au

LEGEND

- EXISTING
- PROPOSED

FOR APPROVAL

DESIGN WORKSHOP AUSTRALIA
Wollongong 81a Preston Highway, Fairy Meadow
NSW 2527 1807
P.O. Box 3091, Baginbora NSW 2519
Sydney
Email: info@designworkshop.com.au
Web: http://www.designworkshop.com.au

PROJECT PROPOSED BOARDING HOUSE

ADDRESS 24 HOMEBUSH RD, STRATHFIELD

CLIENT Verghis 1988 Pty Ltd
Level 25, BT Tower 1 Market St, Sydney NSW 2000
Tel: +61 2 9205 9200
Email: info@verghis1988.com.au

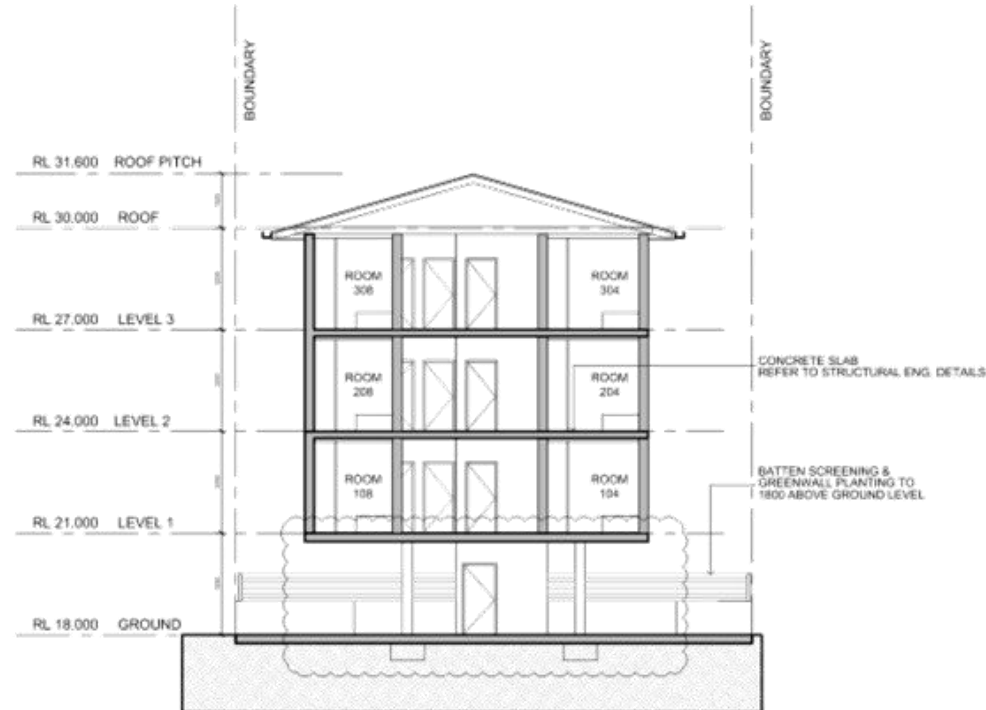
DRAWING NAME GROUND & LEVEL 1/2 PLAN

SCALE 1:100

DESIGN DRAWN: CV / SL



SECTION A - A LOOKING NORTH



SECTION B - B LOOKING EAST

REF.	DATE	AMENDMENT	BY
C	26.08.2018	ISSUE FOR INFORMATION	SL
D	16.08.2018	FOR APPROVAL	SL
E	13.08.2018	ADDITIONAL INFORMATION	SL

CONSTRUCTION NOTES

General Requirements:

- Windows shall be appropriate and are to be verified on site prior to installation.
- All glass furniture to be suitable to current glazing code.
- All frames to be suitable for the intended production distribution.
- All windows to comply with current wind loads.
- All windows are to be in accordance with weather strips, gaskets, double seals and fly screens to comply.
- Do not locate window openings of opening.
- All windows to be fire-rated glass as specified by engineer.
- Exterior glazing to be in accordance with engineering details.
- All DR conditions to be complied with.
- Provide full 100% air seal to all windows/doors/external openings.
- Slabbing to provide adequate structural support and hydraulic engineers.
- Provide to AS/NZS 1562 to ensure that all pipe systems and appliances with clean up to site with connections.
- All gas calculations to comply with Australian Standards, also change to be provided for approval prior to site installation.
- To be submitted by structural engineer.
- Waterproofing to all areas to be suitable for purpose. All waterproofing to be certified by specialist hydraulic consultant.
- All materials and quantities to be provided for all waterproofing.
- All steel mesh, mesh and electric mesh to be suitable for the intended use.
- Support system to be selected by submitter. All details to be suitable for outdoor exposure.
- All water and drainage to be as required.
- Sound transmission and insulation. Refer to achieve R10 compliance for sound control in accordance with relevant BCA.
- Service ducts and pipes to comply with R10 requirements.
- Submitter to provide an acoustic engineer to provide specialist advice for sound control.
- Submitter to provide full details of roofing materials used and warranty of supply and installation.
- All service ducts/ventilators to be fully sealed and the rated in R10 and Australian Standard requirements.
- All steel mesh to be as required. Service mesh in accordance with BCA Australian Standards. 25mm separation to all floor and ceiling in accordance to BCA.
- All water service ducts to be as required.
- "Honeycomb" service management barriers to be used to cover all aspects of service protection to comply with AS/NZS 1562.1.
- Stairing to comply with AS/NZS 1562.1 and other relevant Australian Standards in compliance with AS/NZS 1562.1.
- Electrical ventilation system to be constructed in compliance with Part 3.5.5. Ventilation of the BCA.
- Full compliance with Section 11 requirements.
- Full compliance with AS/NZS 1562.1 requirements.
- Submitter to provide full details of all other relevant information from the submitter on the site. 1800 above ground level to be in accordance with BCA.
- Reinforcement to be as required. 2 pack epoxy system.
- 1. waterproof membrane 2. other waterproof 3. concrete membrane.

All Standards and Codes including but not limited to the BCA/NCC & any referenced AS/NZS standard to apply & be complied with as required.

ACCESS CONSULTANTS

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STONEMASTER CIVIL & TRAFFIC

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 EMAIL: eplan@epplan.com.au

PAVING CONSULTANT

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 WINDSOR NSW 4002 EMAIL: info@elephantsfoot.com.au

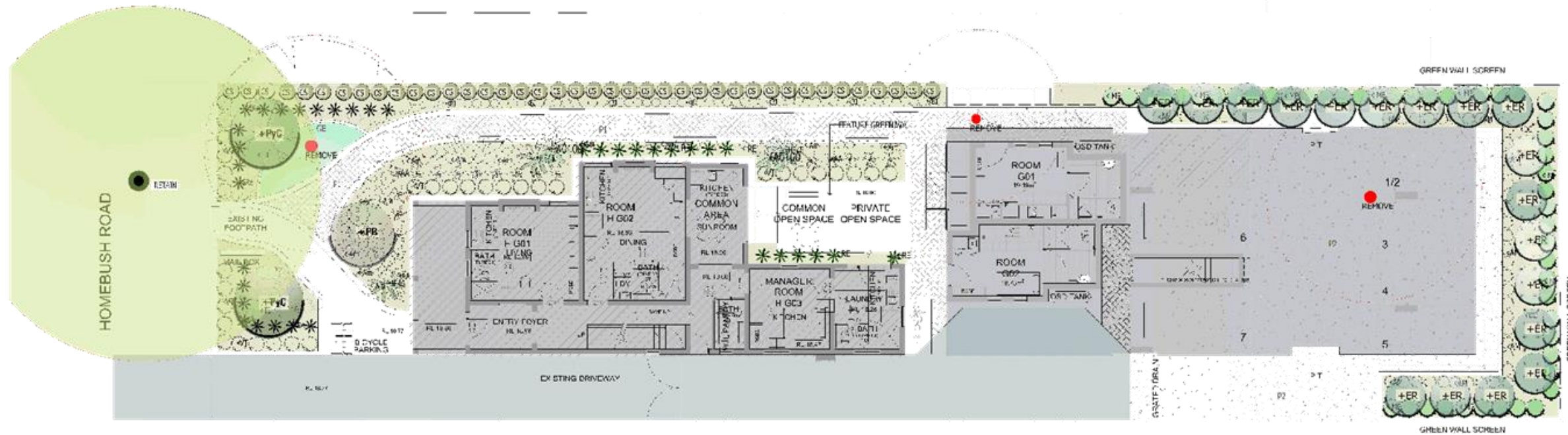
LEGEND

P FIXED GLAZING
 FB FACE BRICK
 GP GRAVEL GLAZING
 R ROOF
 WS WHEEL STOP

FOR APPROVAL

DESIGN WORKSHOP AUSTRALIA
 Wollongong
 8 Tuross Highway, Fairy Meadow
 NSW 2527
 P.O. Box 2051, Sutherland NSW 2259
 Sydney
 Email: info@designworkshop.com.au
 Web: http://www.designworkshop.com.au

PROJECT: PROPOSED BOARDING HOUSE
 ADDRESS: 24 HOMEBUSH RD, STRATHFIELD
 CLIENT: Versh 1988 Pty Ltd
 Level 24, BT Tower 1 Market St, Sydney NSW 2000
 Tel: +61 2 9293 8259
 Email: info@versh1988.com.au
 DRAWING NAME: SECTIONS DETAILS
 SCALE: 1:100
 DESIGN: [Signature] DATE: 01/08/18



STRATHFIELD COUNCIL RECEIVED

AMENDED PLANS

DA2015/056/02

14 August 2018

LEGEND

- EXISTING POSITION (TOP)
- REMOVE TO REPAIR BY ARCHITECT
- REMOVE - EXISTING DRIVE
- DESIGN 1 - V.H.
- EXISTING GROUND LEVEL
- AUTOMATIC IRRIGATION AS SPEC. NIP HYDRANT LOCATIONS FOR TANK OFF POINT. ENSURE ALL ACCESS CONDUITING UNDER PAVED AREAS.
- SIR WALTER BUFFALO TURF
- LANDSCAPE - PLANTING
- GRASS EXPOSE
- BRICK PAVING TO MATCH EXISTING PATI.
- P2 - BLACK OXIDE COLOURED CONCRETE, 100mm THICK REINFORCED WITH 10mm METAL AGGREGATE FINISH
- SELECTED PAVING

PLANTING SCHEDULE

SYMB.	BOTANICAL NAME	COMMON NAME	SIZE	NO.
BRHS				
AC75	Archirodonnax curvinghamiana	Bangalow Palm	100 Linc	2
ER	Elaeocarpus reticulatus	Blackberry Ash	75 Litre	18
HM5	Howea forsteriana	Kentia Palm	45 Litre	3
PR	Platanus rutilans	Franseria	25 Litre	1
PyC	Pyrus calleryana	Chintclear Pear	25 Litre	2
S. RUBS				
AA	Agave attenuata	Mexican Agave	5 Litre	22
AM	Artemisia australis minor	Artemisia Magic	5 Litre	46
BJ	Banksia japonica	Japanese Banksia	5 Litre	69
CS	Cassia toranqua	Beetroot Emily	5 Litre	40
MF	Metrosideros collina	Ti Tree	5 Litre	50
KP	Rhapis excelsa	Jack Palm	7.5 Litre	15
SP	Sporobolus vaginatus	3rd of Paradise	5 Litre	21
VT	Viburnum tinus	Laurel Tree	5 Litre	33
GROUND COVERS				
Am	Agapanthus 'peter pan'	Queen White	140mm	150
DC	Dianella caerulea	Seaflora	140mm	44
TJ	Thalictrum aquilegifolium	Star Jasmine	110mm	11

AUTOMATIC DRIPPER FEED IRRIGATION

A DRIPPER FEED IRRIGATION SYSTEM SHALL BE DESIGNED AND INSTALLED BY THE LANDSCAPE CONTRACTOR. ELEVATION SHOP DRAWINGS WILL BE REQUIRED. REQUIRED AT COMPLETION OF LANDSCAPE CONSTRUCTION.

A DRIPPER FEED IRRIGATION SHALL BE PROVIDED TO ALL GROUND BASE PLANTING, PLANTER BOXES, AND TO ALL TURFED AREAS.

IF AVAILABLE THE SYSTEM SHALL BE CONNECTED TO ON SITE RAINWATER TANKS. A BACKUP CONNECTION POINT FOR THE IRRIGATION SHALL BE TO THE MAIN SUPPLY AND SHALL BE FITTED WITH A BACKFLOW VALVE.

AN ELECTRICAL SUPPLY TO THE CONNECTION POINT SHALL BE CO-ORDINATED WITH THE BUILDING CONTRACTOR.

AN AUTOMATIC TIMER CONTROL DEVICE SHALL BE CONNECTED AND THE LOCATION OF THE CONTROLLER SHALL BE CO-ORDINATED WITH THE BUILDING CONTRACTOR.

ALL FILTERING DEVICES AT CONNECTIONS AND BACKFLOW PREVENTION VALVES SHALL BE PROVIDED WITH EVERY NECESSARY TO COMPLY WITH THE REQUIREMENTS OF THE RELEVANT WATER BOARD AUTHORITIES.

THE CONTRACTOR IS RESPONSIBLE FOR CO-ORDINATION WITH THE BUILDING CONTRACTOR TO ENSURE THAT CONDUITS FOR PROPOSED PIPES OF CONCRETED AREAS HAVE BEEN INSTALLED.

MAINTENANCE

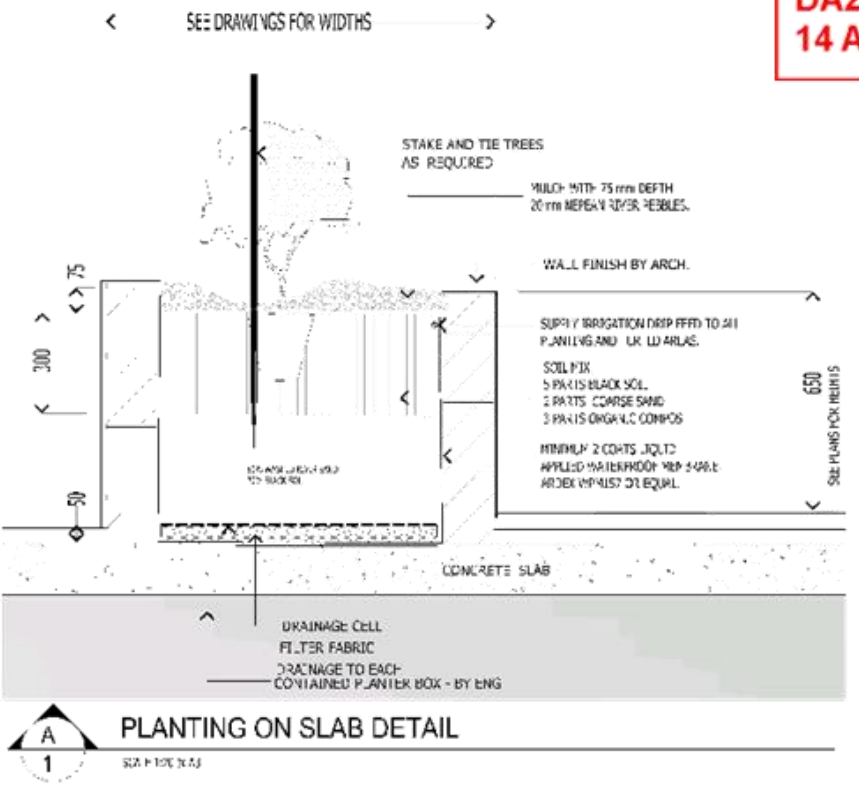
MAINTENANCE SHALL MEAN CONTINUOUS CARE OF THE LANDSCAPE AREAS BY ACCEPTED "BEST PRACTICE" PRACTICES AS WELL AS RECTIFYING ANY DEFECTS THAT BECOME APPARENT IN THE WORKS UNDER WORKMANSHIP.

THE MAINTENANCE PERIOD SHALL BE IN TWO PARTS:

1. PLANTING ESTABLISHMENT PERIOD OF 15 WEEKS.
2. CONTINUOUS CARE AND MAINTENANCE FOR RESPONDING WITH THE DEFECTS LIABILITY PERIOD 12 MONTHS FROM THE DATE OF PRACTICAL COMPLETION.

THE PLANTING ESTABLISHMENT PERIOD COMMENCES AT THE DATE OF PRACTICAL COMPLETION.

THE MAINTENANCE PERIOD SHALL INCLUDE THE FOLLOWING: WEEDING, MOWING, FERTILIZING, TYPING, WEEDING AND AS REQUIRED WATERING, MOWING, FERTILIZING, TYPING, EDGING, WEEDING, PEST AND DISEASE CONTROL, STAKING AND TIEING, REPLANTING, CALLUSING, PRUNING, GRASS COLLECTING AND MOWING, RAKE AND LEAF REMOVAL, AND ALL OTHER WORK AS REQUIRED.



NOTES

1. FOR PLANTING POINTS IN PARK AND GARDEN AREAS - SEE ARCHITECTURE.
2. ROOMS TO BE COVERED BY ARCHITECT SHALL BE COVERED BY ARCHITECT. UNLESS OTHERWISE SPECIFIED BY ARCHITECT.
3. DATA POINTS TO BE INSTALLED TO PLANTING SYSTEMS, PLANTING POINTS AND TURF AREAS. SHOWING POSITION AND DEPTH.
4. SEE ARCHITECTURE FOR ALL OTHER DATA POINTS TO BE INSTALLED.
5. LANDSCAPE AND PLANTING DRAWINGS ARE TO BE COMPLETED.

01	AGAPANthus	1-15-2018
02	TI TREE	12-06-2018
03	ARTISTOPHY	20-03-2018
04	ARTISTOPHY	20-03-2018

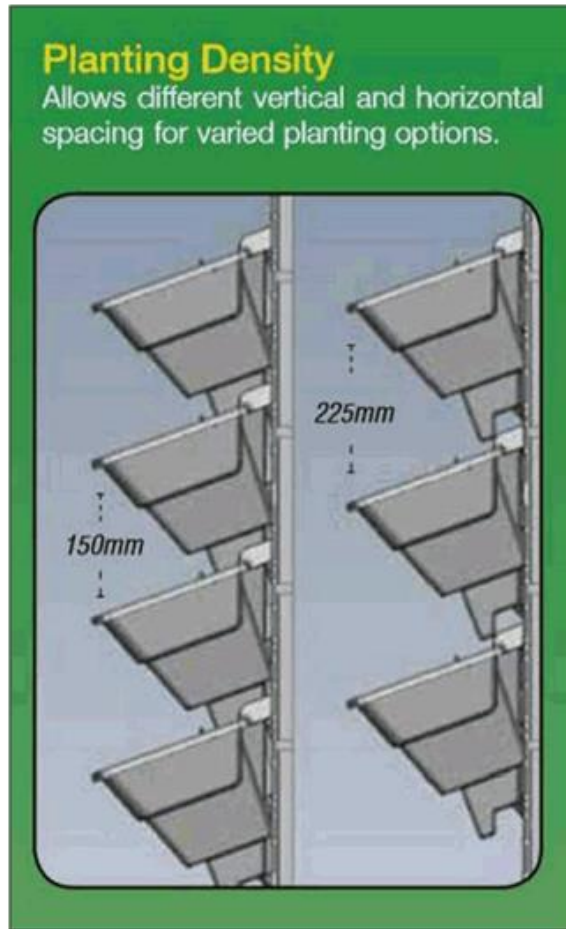
Project: 24 Homebush Road, Strathfield
Client: Vesta 1628 Pty Ltd

LA LANDSCAPE ARCHITECT
16125 Outrigger Street, MULLOOLBA, QLD
PHONE: 0411 274 200
EMAIL: b.la@planning.com.au

SCALE: 1:100 (A1), 1:200 (A2)
May 2018
SHEET 1 OF 2

LA DA.01 LANDSCAPE CONCEPT PLAN

EMLICH 'VERSIWALL' GREEN WALL SYSTEM



NOTES

- FOR DRAINAGE POINTS IN PAVING AND PLANTER BOXES - SEE HYDRAULIC DINGS
- 80mm SOIL COVER FIBRODUM OVER ALL CONCRETE STRUCTURES IN LANDSCAPE AREAS. GREATER WHERE INDICATED ON PLANS AND DETAILS
- INSTALL DEEPER FEED IRRIGATION TO PLANTING BEDS, PLANTING BOXES, AND TURF AREAS SHOP DRAWING BY CONTRACTOR.
- FOR ALL HARD SURFACES AND DETAILING REFER TO ARCH. DRAWINGS.
- LANDSCAPE AND DRAINAGE DRAWINGS ARE COMPATIBLE

NO	DESCRIPTION	DATE	BY
01	For Dwg	13-08-2018	WJL
02	Preliminary	20-09-2014	WJL
rev. no	description	date	by

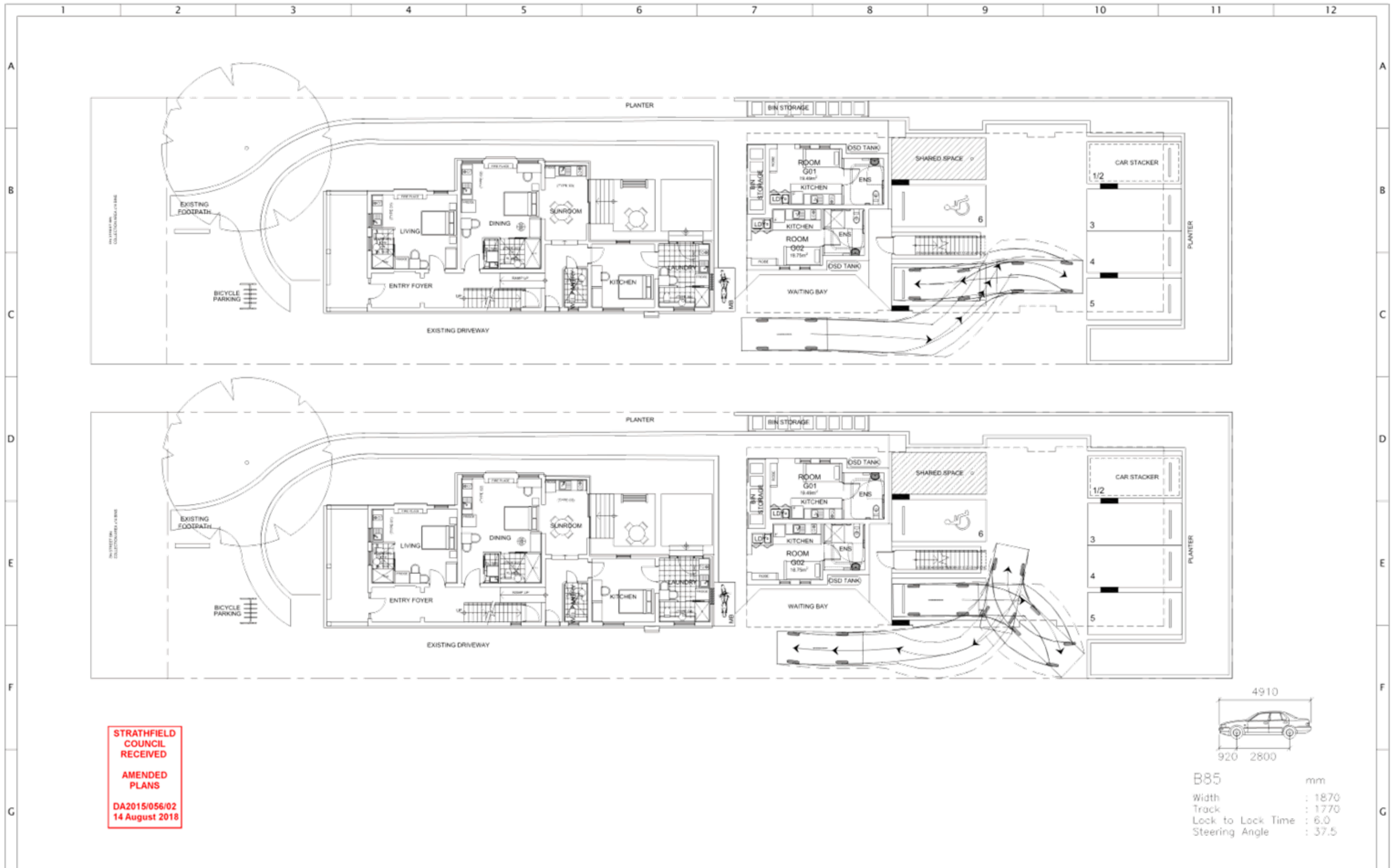
Project:
Sect 96 Building Development
24 Homebush Road, Strathfield.

Client: Verani 1888 Pty Ltd

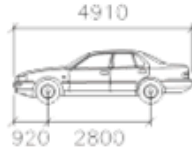
BILL MUIR LA LANDSCAPE ARCHITECT
16/105 Central Street, WOLLONGONG
NSW 2522 E. 0411 774001
bill@optusnet.com.au

6/21 ON BOX
May 2018
SHEET 2 OF 2

LA.DA.02 GREEN WALL CONCEPT

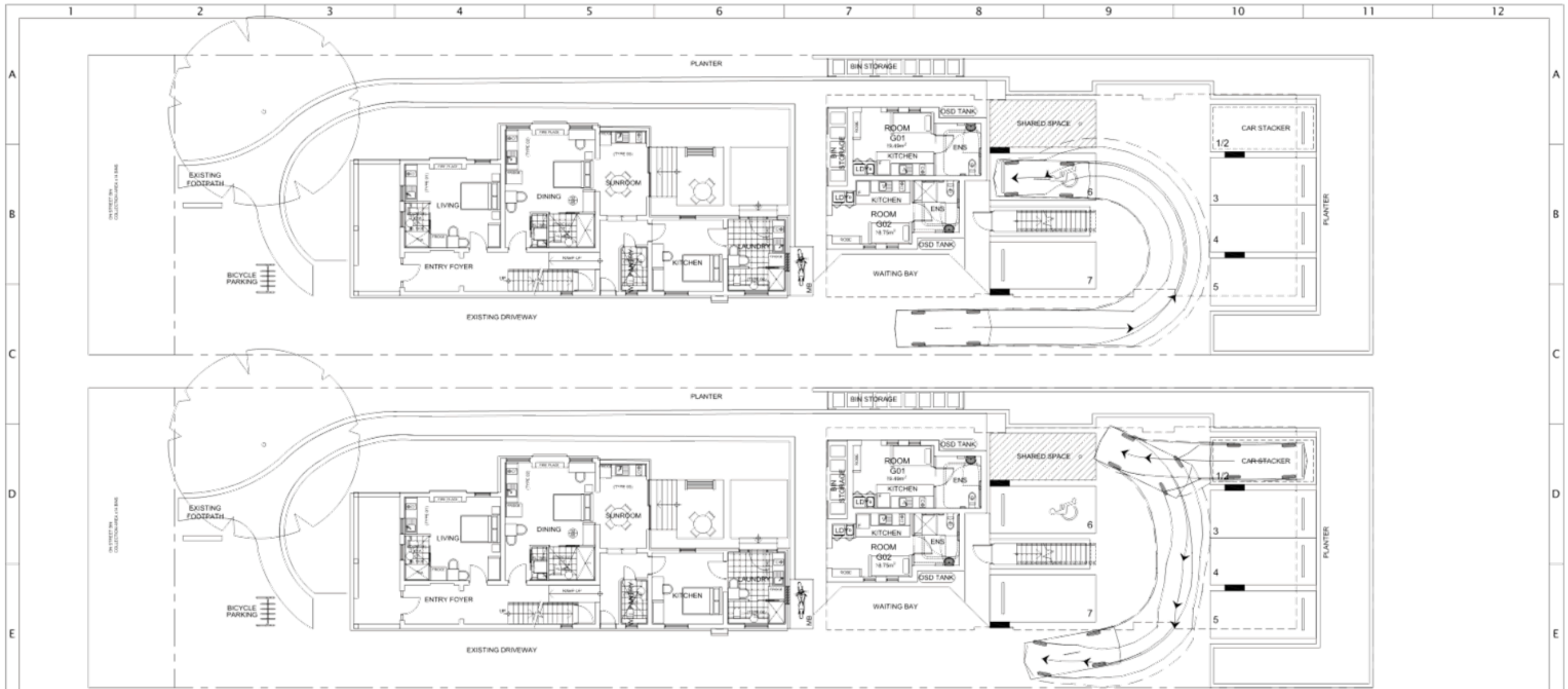


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 14 August 2018

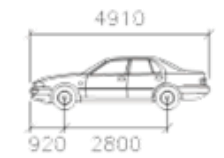


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 Width : 1870
 Track : 1770
 Lock to Lock Time : 6.0
 Steering Angle : 37.5

ISSUE	AMENDMENT	DATE	SCALE 1:100 @ A1			ATB CONSULTING ENGINEERS CIVIL & STRUCTURAL	11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au		Title	B85 TURNING CIRCLES	SCALES	1:100 @ A1, 1:200 @ A3	DATE PLOTTED	06/08/18			
A	ISSUE FOR DA APPROVAL	06/08/18	DO NOT SCALE IF IN DOUBT ASK						A1	THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS	Project	PROPOSED BOARDING HOUSE	DESIGNED	M.S.	CHECKED		DATE CHK'D
									At	24 HOMEBUSH ROAD STRATHFIELD	PROJECT No	18055	DWG	T1	SHEET - OF	REVISION	A
									Client	VERANI							

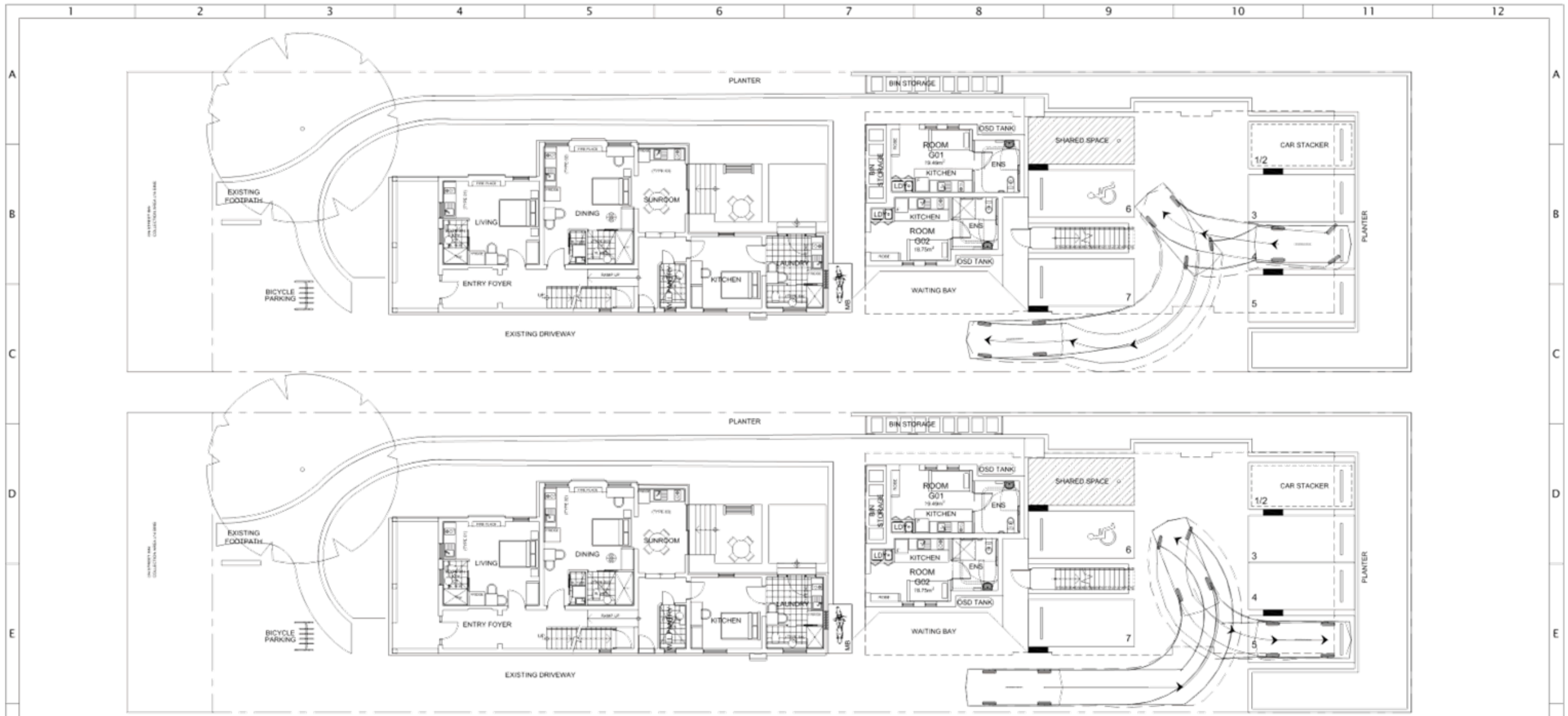


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AMENDED PLANS
 DA2015/056/02
 14 August 2018



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 Width : 1870
 Track : 1770
 Lock to Lock Time : 6.0
 Steering Angle : 37.5

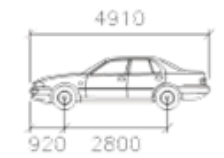
ISSUE	AMENDMENT	DATE	SCALE 1:100 @ A1			11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au		Title	B85 TURNING CIRCLES	SCALES	1:100 @ A1, 1:200 @ A3	DATE PLOTTED	06/08/18		
A	ISSUE FOR DA APPROVAL	06/08/18	DO NOT SCALE IF IN DOUBT ASK THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty Ltd. Unauthorised copying of part or whole of the documents is a breach of copyright.					Project	PROPOSED BOARDING HOUSE	DRAWN	M.S.	CHECKED	G.U.	DATUM	A.H.D.
								At	24 HOMEBUSH ROAD STRATHFIELD			PROJECT No	18055		
								Client	VERANI	DWG	T2	SHEET - OF		REVISION	A



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RECEIVED

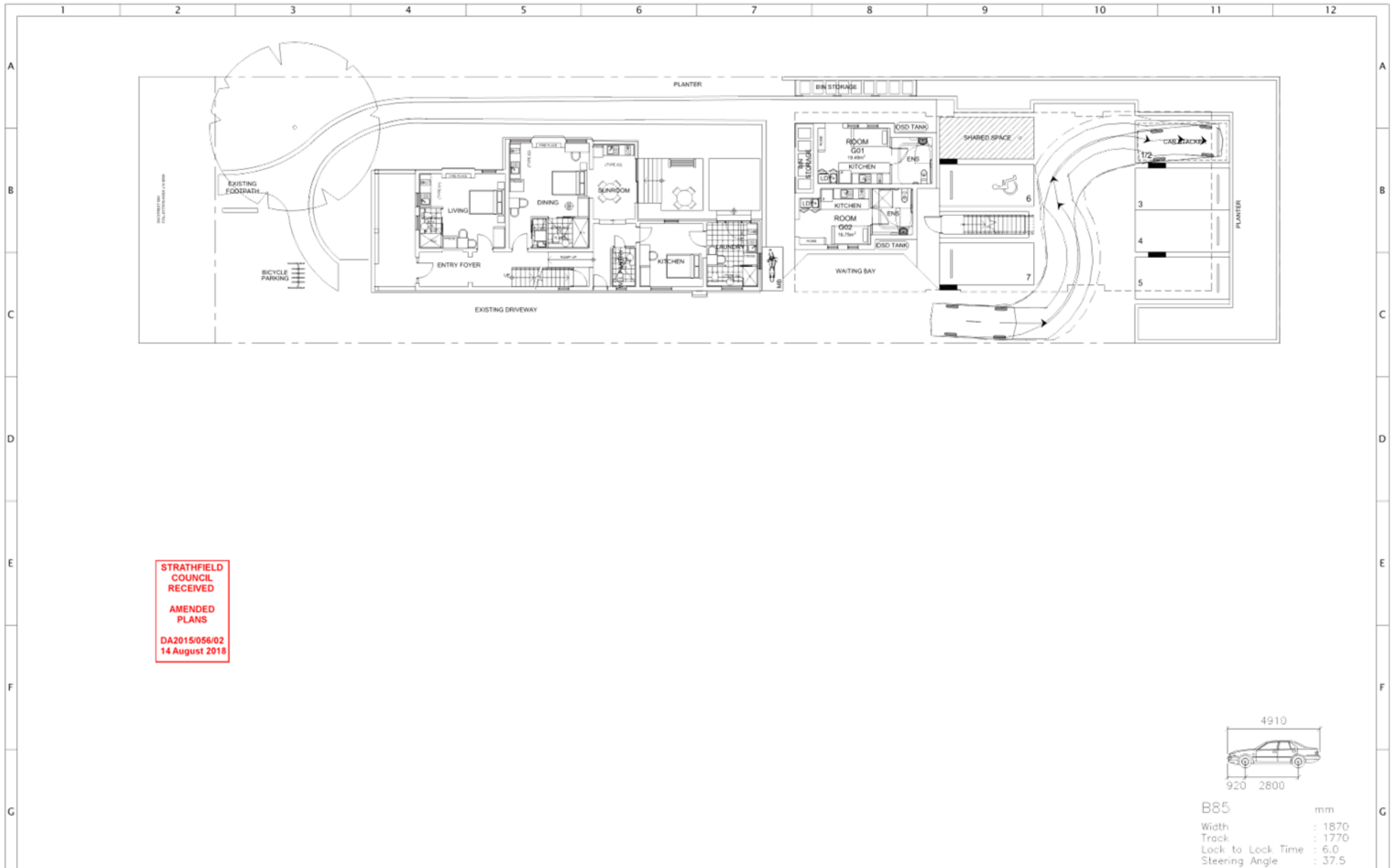
AMENDED
PLANS

DA2015/056/02
14 August 2018



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 Width : 1870
 Track : 1770
 Lock to Lock Time : 6.0
 Steering Angle : 37.5

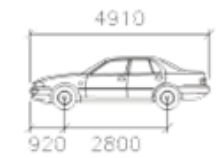
ISSUE A	AMENDMENT ISSUE FOR DA APPROVAL	DATE 06/08/18	SCALE 1:100 @ A1 DO NOT SCALE IF IN DOUBT ASK THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty Ltd. Unauthorised copying of part or whole of the documents is a breach of copyright.		ATB CONSULTING ENGINEERS CIVIL & STRUCTURAL	11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au		Title B85 TURNING CIRCLES Project PROPOSED BOARDING HOUSE At 24 HOMEBUSH ROAD STRATHFIELD Client VERANI	SCALES 1:100 @ A1, 1:200 @ A3 DRAWN M.S. DESIGNED G.U. CHECKED PROJECT No 18055 DWG T3 SHEET OF REVISION A	DATE PLOTTED 06/08/18 DATUM A.H.D. DATE CHKD
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RECEIVED**

**AMENDED
PLANS**

**DA2015/056/02
14 August 2018**



B85 mm

Width : 1870
Track : 1770
Lock to Lock Time : 6.0
Steering Angle : 37.5

ISSUE	AMENDMENT	DATE	SCALE 1:100 @ A1			ATB CONSULTING ENGINEERS CIVIL & STRUCTURAL	11 VICTORIA STREET WOLLONGONG NSW 2500 TELEPHONE: 02 42 266 646 Email: info@atbconsulting.com.au		Title	B85 TURNING CIRCLES	SCALES	1:100 @ A1, 1:200 @ A3	DATE PLOTTED	06/03/18				
A	ISSUE FOR DA APPROVAL	06/08/18	DO NOT SCALE IF IN DOUBT ASK THIS DRAWING SHALL BE READ IN CONJUNCTION WITH SPECIFICATIONS COPYRIGHT: Concepts and information contained in these engineering drawings and related documents are the copyright of ATB Consulting Engineers Pty Ltd. Unauthorised copying of part or whole of the documents is a breach of copyright.						Project	PROPOSED BOARDING HOUSE	DRAWN	M.S.	DATUM	A.H.D.				
										At	24 HOMEBUSH ROAD STRATHFIELD	CHECKED		DATE CHKD				
										Client	VERANI	PROJECT No	18055	DWG	T4	SHEET - OF	REVISION	A

TO: Strathfield Local Planning Panel Meeting - 4 October 2018
REPORT: SLPP – Report No. 3
SUBJECT: DA2018/064 - 3 HOMEBUSH ROAD, STRATHFIELD
LOT 20 DP 1101067
DA NO. DA2018/064

SUMMARY

Proposal: Demolition of existing residential flat building and construction of a boarding house comprised of (36) rooms accommodating up to (72) people over two (2) levels of basement parking under the Affordable Rental Housing SEPP 2009.

Applicant: Samsara Enterprises Pty Ltd

Owner: MRS. KL & MRS. SL Chhabra

Date of lodgement: 11 May 2018

Notification period: 5 June 2018 – 29 June 2018

Submissions received: Three (3) written submissions received

Assessment officer: LG

Estimated cost of works: \$4,663,952.00

Zoning: R3 – Medium Density Residential - SLEP 2012

Heritage: n/a

Flood affected: Yes

Is a Clause 4.6 variation proposed? No

RECOMMENDATION OF OFFICER: **APPROVAL**

EXECUTIVE SUMMARY

- 1.0 Approval is sought for the demolition of an existing residential flat building and construction of a boarding house comprised of (36) rooms accommodating up to (72) people over two (2) levels of basement parking under the Affordable Rental Housing SEPP 2009. The proposal includes (35) double lodger rooms and one (1) room allocated for an on-site manager.
- 2.0 The plans and documentation submitted as part of the application were notified in accordance with Part L of the Strathfield Consolidated Development Control Plan 2005. Three (3) submissions were received during this time raising concerns in relation to off-street parking, overdevelopment of the site, communal spaces, open walkways and adaptable housing.
- 3.0 The proposal is satisfactory with respect to the relevant legislative provisions of the ARH SEPP, the Strathfield Local Environmental Plan (SLEP) 2012 and relevant DCP 2005 controls. The layout of the boarding house is considered to provide acceptable sized rooms, common areas and bathroom facilities whilst providing an acceptable level of articulation to ensure view corridors are maintained between buildings.

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

- 4.0 The subject site is located within close proximity to the Strathfield and Homebush Town Centre, providing close proximity to the Strathfield Bus Terminus, local services, supermarkets and restaurants. The subject site is located within a medium density residential setting providing a suitable location for boarding house accommodation. Accordingly, the subject site is recommended for approval.

DESCRIPTION OF THE SITE AND LOCALITY

The subject site is legally described as Lot 20 in DP1101067 and is commonly known as 3 Homebush Road, Strathfield. The site is located on the western side of Homebush Road north of the intersection of Homebush Road and Burlington Road and south of the railway line.

The site is a rectangular-shaped allotment with a skew to the south-eastern corner of the site fronting Homebush Road. The site provides a frontage width of 15.585m, a depth between 50.77m – 54.05m and a total site area of 796.7m². The site slopes from west to east and has gradual slope toward Homebush Road.

Existing development on the site comprises a three (3) storey residential flat building with vehicular access provided via the northern portion of the site fronting Homebush Road. The site currently contains (8) x 2 bedroom low rental residential units and is comprised of a red facebrick with painted white coloured banding. The current streetscape is characterised by traditional-style facebrick medium density residential development with some modern-style medium density developments having been approved and or/constructed within the streetscape.



Figure 1: Locality plan

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)



Figure 2: View of existing development on subject site



Figure 3: View of recently constructed development at 20 Homebush Road.

PROPERTY BURDENS AND CONSTRAINTS

There are no easements or burdens on the land which could affect, or be affected by, the proposed development.

DESCRIPTION OF THE PROPOSED DEVELOPMENT

The application seeks Council approval for demolition of an existing residential flat building and construction of a boarding house comprised of (36) rooms including one (1) manager's room accommodating up to (72) people over two (2) levels of basement parking under the Affordable Rental Housing SEPP 2009.

The specific elements of the proposal are:

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

- Excavation to accommodate two (2) levels of basement parking;
- Construction of a part four (4) part three (3) storey boarding house comprised of (35) lodger rooms and one (1) manager's room; and
- Associated landscaping and stormwater works

A photomontage of the proposed development is provided in Figure 4 below:



Figure 4: Photomontage of proposed development as viewed from Homebush Road.

REFERRALS

INTERNAL REFERRALS

Engineering Comments

Council's Engineer has commented on the proposal as follows:

"I have reviewed the stormwater drainage concept plan prepared by ALPHA Engineering and Development rev A drawing no. A8180-COVER – A8180-SW06 dated 08.05.2018 and can advise the following:

- *Powell's Creek and Saleyards Creek revised flood study indicates that the subject site is affected by overland flow of stormwater of the 1 in 100yr ARI storm event. A Flood Impact Assessment Report in accordance with Council's interim Flood Prone Land Policy needs to be submitted along with the development application."*
- *The edge of the driveway crossing, including apron and layback shall be located a minimum of 1m clear of any existing stormwater pits or poles and 2m clear of the trunk of any trees within the road reserve.*

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

Based on the issues highlighted above, the concept plan is not feasible and cannot be supported in its current format."

It was requested that the Applicant resolve the flood issues raised including amendment to the architectural plans and submission of a Flood Impact Assessment Report. Following submission of the requested amended and additional information the following comments were made:

"From an engineering perspective, the concept plan is feasible and there are no objections to its approval subject to the following conditions attached."

Council's Engineer offered no objections to the proposal, subject to the imposition of recommended conditions of consent including compliance with the submitted Flood Impact Report.

Waste Comments

The application was forwarded to Council's Waste Officer for comment. No concerns were raised subject to the imposition of conditions of consent.

Landscaping Comments

Council's Tree Coordinator has commented on the proposal as follows:

- *"Tree 1 is a Council asset that would usually be retained until alternative solutions have been exhausted. The dead wood can be removed as part of a routine maintenance program.*
- *It is not usually appropriate to remove a neighbour's tree to facilitate the development of an adjoining property. Having a medium significance and a medium retention value scale they are more than suitable to be retained and protected. I do not support the removal of these 3 trees and the applicant is to amend their design in order to retain and protect all 3 trees."*

Additional information was submitted to Council by way of an amended Arborist Report and Tree Management Plan demonstrating that Tree 1 and Tree 2 will be retained. Council's Tree Coordinator has raised no further objection to the proposal subject to the imposition of conditions of consent including compliance with the submitted Arborist Report and Tree Management Plan.

Traffic Comments

Council's Traffic Engineer has commented on the proposal as follows:

"I have referred to the development application referenced above and reviewed the application from the traffic perspective. I have the following comments with regards to this application:

- *The ramp does not allow two vehicles to pass simultaneously. While it may be assumed that only one vehicle will be using the ramp at any time, there are no engineering controls planned to be installed to indicate if any vehicle is on the ramp already. This may lead to two vehicles hitting each other head on or require reversing, neither of which is safe.*
- *Shared space is in front of lift and this is not acceptable. It has to align as per Australian Standards.*
- *Car space Number 11 is not the end space and should comply with standards."*

Amended plans were submitted to Council during the assessment process demonstrating that the abovementioned matters have been addressed. Council's Traffic Engineer offered no objections to the proposal, subject to the imposition of recommended conditions of consent. This has included a condition requiring a convex mirror to be provided upon the ramp to ensure that vehicles entering and exiting the site at the same time do not collide (refer to Condition 36).

DA2018/064 - 3 Homebush Road, Strathfield
 Lot 20 DP 1101067 (Cont'd)

SECTION 4.15 CONSIDERATIONS – EP&A Act, 1979

In determining a development application, the consent authority is to take into consideration the following matters of consideration contained within Section 4.15 of the Environmental Planning and Assessment Act, 1979 as relevant to the development application:

4.15(1)(a) the provisions of:

(i) any environmental planning instrument

State Environmental Planning Policy No 55 – Remediation of Land

SEPP 55 applies to the land and pursuant to Section 4.15 is a relevant consideration.

A review of the available history for the site gives no indication that the land associated with this development is contaminated. There were no historic uses that would trigger further site investigations.

The objectives outlined within SEPP55 are considered to be satisfied.

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017

The State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017 replaces the repealed Clause 5.9 of SLEP 2012 (Preservation of Trees and Vegetation).

The intent of this SEPP is consistent with the objectives of the repealed Standard where the primary aims/objectives are related to the protection of the biodiversity values of trees and other vegetation on the site.

In this instance, the proposal seeks to retain the Council street tree as well as Tree 2 located on the adjoining property to the east. The proposal results in the removal of Tree 3 located along the western adjoining boundary. This is considered acceptable as the tree is relatively insignificant and the proposal will accommodate high quality landscaping including provision of new canopy tree plantings in the front and rear setbacks of the site.

The proposal was referred to Council’s Tree Coordinator who outlined specific conditions to be imposed with any development consent in order to ensure the protection of these trees. A special condition of consent has also been recommended requiring the landscape plan to be amended to reflect the retention of the abovementioned trees as well as the relocation of the common open space area from the south-western portion of the site to the north-western portion of the site (refer Condition 1).

State Environmental Planning Policy (Affordable Rental Housing) 2009

The proposed development has been lodged under Part 2, Division 3 of the ARH SEPP which relates to the provision of ‘boarding houses’.

An assessment of the development against the development standards for boarding houses under the ARH SEPP is presented in the table below.

Clause	Development Control	Required	Proposal	Compliance
26	Permissibility	Permissible in the following zones: <ul style="list-style-type: none"> • R2 Low Density Residential • R3 Medium Density 	Within a prescribed zone (R3 –	Yes.

DA2018/064 - 3 Homebush Road, Strathfield
 Lot 20 DP 1101067 (Cont'd)

		Residential <ul style="list-style-type: none"> • R4 High Density Residential • B1 Neighbourhood Centre • B2 Local Centre • B4 Mixed Use 	Medium Density Residential) under `SLEP	
27(2)	Location and access to facilities	If it is located in R2 – must be within an ‘accessible area’	N/A – not located in R2 zone	N/A.
29 Note: Unable to refuse based on compliance these standards	FSR	Max as per SLEP 2012: 1.45:1 (1,155.21m ²) PLUS incentive of 0.5:1 if the max FSR is > 2.5:1 = 1.95:1 (1,553.57m ²)	1.245:1 (991.96m ²).	Yes.
	Height	Max as per SLEP: 14m	12.3m.	Yes.
	Landscaping	Front setback is compatible with streetscape	9m front setback is compatible with the existing varied front setbacks provided in the street ranging between 8-10m.	Yes.
	Solar Access	At least one (1) common area receives at least three (3) hours of direct sunlight between 9am and 3pm, mid-winter	A common area is located to the rear north-western corner of the site which is capable of achieving a minimum 3 hours direct solar access throughout mid-winter.	Yes.
	Private Open Space	Lodgers: Min 20m ² with min. dimension of 3m	A common open space area directly adjoins the common lounge in the rear north-western portion of the site which is 27.3m ² in size. A portion of the space however fails to meet the minimum 3m depth requirements.	No, refer discussion below.
	Parking	In an ‘accessible area’:	(18) off-street	Yes.

DA2018/064 - 3 Homebush Road, Strathfield
 Lot 20 DP 1101067 (Cont'd)

	Dwelling size	<p>0.5 spaces per room 35 lodger rooms are proposed, therefore 18 (17.5) spaces are required.</p> <p>1 additional parking space is required for the on-site manager</p> <p>Other: min 16m²</p> <p>May have a kitchen/bathroom however is not required to.</p>	<p>carparking spaces have been provided in the basement.</p> <p>No additional off-street parking space has been provided for the on-site manager.</p> <p>All double rooms achieve a minimum 16m² room size.</p> <p>All lodger rooms have been provided with kitchen, bathroom and laundry facilities.</p>	<p>No, refer discussion below.</p> <p>Yes.</p> <p>Yes.</p>
30	Standards for Boarding Houses	<p>One (1) communal living room required where there are five (5) or more rooms</p> <p>No boarding room > 25m²</p> <p>No boarding room occupied by > two (2) adult lodgers</p> <p>Adequate bathroom and kitchen facilities available</p> <p>Boarding House Manager where capacity of > 20 lodgers</p> <p>One (1) bicycle space and one (1) motorcycle space per five (5) boarding rooms. 35 rooms therefore 7 spaces required for each.</p>	<p>A communal living room is provided in the rear portion of the site.</p> <p>No room is greater than 24.6m².</p> <p>Condition to be imposed.</p> <p>Adequate communal bathrooms, kitchens and laundry have been provided.</p> <p>Max. 70 lodgers therefore manager room has been provided.</p> <p>8 bicycle spaces and 8 motorcycle spaces proposed.</p>	<p>Yes.</p> <p>Yes.</p> <p>Yes, subject to condition.</p> <p>Yes.</p> <p>Yes.</p> <p>Yes.</p>

DA2018/064 - 3 Homebush Road, Strathfield
 Lot 20 DP 1101067 (Cont'd)

30A	Character	A consent authority must not consent to development to which this division applies unless it has taken into consideration whether the design of the development is compatible with the character of the local area.	The proposal is compatible with the medium density character of the area which typically provide residential flat buildings with some recently approved boarding house developments. The proposed development is relatively modern in style yet has sought a neutral colour palette which not only provides visual interest but is generally sympathetic to the transitioning streetscape.	Yes.
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Private Open Space Area

Clause 29(2)(d)(i) of the ARH SEPP 2009 requires a minimum 20m² private open space area to be provided for lodgers which achieves a minimum dimension of 3m. The proposal provides a communal open space area to the rear north-western corner of the site which achieves a minimum 27.3m² open space area. Notwithstanding, whilst the portion along the rear western boundary achieves a minimum depth of 3.4m, the remaining portion along the northern boundary of the site is only 1.8m in depth thus failing to achieve compliance with the minimum 3m SEPP requirements. If the portion along the northern side boundary were to be excluded, only 16.5m² common open space is provided for the site which is also non-compliant with the SEPP.

It is acknowledged that the proposal has sought to achieve reasonable amenity for lodgers of the site by allocating a minimum 8m² private open space area to each of the ground floor lodger rooms and providing all upper level eastern facing rooms with a balcony. In light of this, the non-compliance is considered acceptable in this instance.

Off-Street Parking

Clause 29(2)(e)(ii) of the ARH SEPP 2009 requires not more than 1 parking space to be provided for each person employed in connection with the development and who is resident on site. Whilst the proposal provides adequate off-street parking for lodgers within the basement, a space has not been provided for the on-site manager who will reside on site. In light of this, an opportunity has been recognised for the bicycle parking bay within basement level 2 to be redesigned as a carparking space and for the bicycle parking to be allocated upon the ground level of the development, predominantly along the southern side boundary of the site. Council's Traffic Engineer has reviewed the suggested re-allocation of the space and has raised no objections to the reconfiguration.

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

Having consideration for the above, a condition of consent is recommended requiring the bicycle parking bay on level 2 of the basement to be redesigned as a carparking space. The bicycle parking will be required to be provided on the at-grade preferably along the southern side boundary of the site beside the walkway (refer to Condition 2).

STRATHFIELD LOCAL ENVIRONMENTAL PLAN (SLEP) 2012

An assessment of the proposal against the general aims of SLEP 2012 is included below:

Cl. 1.2(2)	Aims	Complies
(a)	<i>To achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield</i>	Yes
(b)	<i>To promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development</i>	Yes
(c)	<i>To promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community</i>	Yes
(d)	<i>To provide opportunities for economic growth that will enhance the local community</i>	Yes
(e)	<i>To promote future development that integrated land use and transport planning, encourages public transport use, and reduced the traffic and environmental impacts of private vehicle use</i>	Yes
(f)	<i>To identify and protect environmental and cultural heritage</i>	Yes
(g)	<i>To promote opportunities for social, cultural and community activities</i>	Yes
(h)	<i>To minimise risk to the community by identifying land subject to flooding and restricting incompatible development</i>	Yes

Comments: The proposed development is considered appropriate for the site and will integrate well with surrounding approved and recently constructed residential buildings in the streetscape. The proposed boarding house is an appropriate use of the site and will promote affordable rental accommodation for residents in the LGA. The proposed development is simplistic in its design yet incorporates high quality material finishes with a neutral colour palette so that it remains compatible with both the existing and more recently constructed buildings in the streetscape as well as potential future development in the street.

Permissibility

The subject site is Zoned R3 – Medium Density Residential under Strathfield Local Environmental Plan (SLEP) 2012.

Boarding houses are permissible within the R3 – Medium Density Residential Zone with consent and are defined under SLEP 2012 as follows:

“Boarding house means a building that:

- (a) *is wholly or partly let in lodgings, and*
- (b) *provides lodgers with a principal place of residence for 3 months or more, and*
- (c) *may have shared facilities, such as a communal living room, bathroom, kitchen or laundry, and*
- (d) *has rooms, some or all of which may have private kitchen and bathroom facilities, that accommodate one or more lodgers,*

but does not include backpackers’ accommodation, a group home, hotel or motel accommodation, seniors housing or a serviced apartment.”

The proposed development for the purpose of a boarding house is consistent with the definition above. Conditions of consent have been imposed to ensure the boarding house operates within

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

the confines of its approved use in accordance with the abovementioned boarding house definition provided by the SLEP 2012 (refer to Condition 4).

Zone Objectives

An assessment of the proposal against the objectives of the R3 – Medium Density Residential Zone is included below:

Objectives	Complies
• To provide for the housing needs of the community within a medium density residential environment.	Yes
• To provide a variety of housing types within a medium density residential environment.	Yes
• To enable other land uses that provide facilities or services to meet the day to day needs of residents.	Yes

Comments: The proposed boarding house development achieves the objectives of the R3 zone given that the development is suited to the medium density residential environment and is compatible with prevailing residential flat developments within the streetscape.

Part 4: Principal development standards

An assessment of the proposal against the relevant provisions contained within Part 4 of the SLEP 2012 is provided below.

Height of building

Cl.	Standard	Controls	Proposed	Complies
4.3	<i>Height of building</i>	14m	12.3m	Yes

	Objectives	Complies
(a)	<i>To ensure that development is of a height that is generally compatible with or which improves the appearance of the existing area</i>	Yes
(b)	<i>To encourage a consolidation pattern that leads to the optimum sustainable capacity height for the area</i>	Yes
(c)	<i>To achieve a diversity of small and large development options.</i>	Yes

Floor space ratio

The FSR controls applying to the ARH SEPP 2009 override the provisions of the SLEP 2012. Refer to SEPP 2009 assessment table for reference.

Part 5: Miscellaneous Provisions

There are no provisions contained within Part 5 of the SLEP 2012 which are relevant to the proposal.

Part 6: Local Provisions

The relevant provisions contained within Part 6 of the SLEP 2012 are addressed below as part of this assessment:

6.1 Acid sulfate soils

The subject site is identified as having Class 5 soils and is not located within 500m of Class 1, 2, 3 or 4 soils. Therefore, the proposed development was not required to be accompanied by an Acid Sulfate Soils Management Plan and has satisfied the requirements of Clause 6.1 of the SLEP, 2012.

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6.2 Earthworks

Appropriate conditions of consent are recommended so as to ensure compliance with the sediment erosion control plan. Conditions of consent that include the preparation of a dilapidation report to be provided to adjoining property owners has also been recommended (refer to Conditions 32, 34 and 37).

6.3 Flood planning

The site is affected by overland flows in accordance with Council's Cooks River and Cox's Creek flood studies. Additional documentation including a Flood Impact Report and amended plans were submitted to Council during the assessment process having consideration for the flood affectation of the site. Modifications have been made to the proposal with respect to Council's Interim Flood Prone Land Policy including raising the height of the basement access ramp crest as well as habitable floor levels of the development. Council's Development Engineer has reviewed the proposed development and accompanying documentation and is satisfied that the proposal complies with Council's requirements subject to standard conditions of consent (refer to Condition 39).

6.4 Essential services

The proposed development has been designed having consideration for the supply of water, electricity, sewerage, waste, stormwater and suitable vehicular access. Standard conditions of consent have been imposed under the parent consent to ensure that these services are provided for the site.

4.15 (1)(a)(ii) any draft environmental planning instruments

There are no applicable draft planning instruments that are or have been placed on public exhibition, to consider as part of this assessment.

4.151)(a)(iii) any development control plan

STRATHFIELD CONSOLIDATED DEVELOPMENT CONTROL PLAN (SCDCP) 2005

The following is an assessment of the proposal's compliance with the relevant provisions contained within SCDCP 2005.

PART H - WASTE MANAGEMENT (SCDCP 2005)

An at-grade bin storage area has been provided along the southern side boundary of the site, however only (9) general waste bins have been provided. This is not compliant with the minimum (10) x 240L general waste bins and (4) x 240L recycling bins required to be provided for the site in accordance with Part H of the SCDCP 2005. Accordingly, a condition of consent is recommended to ensure that a larger bin area is provided along the southern side boundary of the site which achieves compliance with the minimum bin numbers required as per Part H of the SCDCP 2005 (refer to Conditions 46, 47, 48 and 49).

4.15 (1)(iia) any planning agreement or draft planning agreement

No planning agreement has been entered into under section 7.4 of the *Environmental Planning and Assessment Act 1979*.

(i) matters prescribed by the regulations

Clause 92 of the *Environmental Planning and Assessment (EP&A) Regulation 2000* requires Council to take into consideration the provisions of the Government Coastal Policy and Australian

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Standard *AS2601-1991: The Demolition of Structures*, in the determination of a development application.

Having regard to these prescribed matters, the proposed development is not located on land subject to the Government Coastal Policy as determined by Clause 92(1)(a)(ii) however does involve the demolition of a building for the purposes of *AS 2601 - 1991: The Demolition of Structures*.

Should this application be approved, appropriate conditions of consent are included within the recommended to ensure compliance with any relevant regulations.

(ii) any coastal zone management plan

The NSW Government projects sea levels to rise by 40cm in 2050 and by 90cm in 2100 above the relative mean sea level in 1990. These planning benchmarks are to be considered in the assessment of development applications through the applicable coastal zone management plan or alternatively the provisions of the *NSW Coastal Planning Guideline: Adapting to Sea Level Rise*.

Although Council is not subject to a coastal zone management plan, the sea level rise planning benchmarks have also been established in order to assess the likely increase in the frequency, duration and height of flooding and as a consequence likely property and infrastructure damage on affected and potentially affected land. Council is therefore required to consider the impact of sea level rise and resultant flooding from Powell's Creek and Cook's River which are tributaries of Sydney Harbour (Parramatta River) and Botany Bay respectively.

The proposed development is located on a site that is affected by the existing 1 in 100 year flood event or is likely to be as a result of the planning benchmarks for sea level rise mentioned above. Refer to Clause 6.3 assessment under Part 6 of the SLEP 2012 discussion.

4.15(1)(b) the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality

Solar Access

Due to the east-west orientation of the site combined with the overall height, bulk and scale of the building, the proposal will result in an unavoidable degree of overshadowing to the southern adjoining residential flat building. A series of solar access diagrams and elevation shadow diagrams were submitted to Council during the assessment process demonstrating the overshadowing impact the proposed development will have upon the southern-adjointing building.

As evidenced in the figures below (refer Figure 5), the living areas and balconies of the existing adjoining building are orientated to the south fronting Burlington Road. All bedrooms and bathrooms are orientated along the northern elevation of the building.

Whilst the proposed development will predominantly overshadow the northern-orientated portions of the units throughout the day, the living spaces located upon the southern portion of the building will receive some solar access with the shadow continuing to move throughout the day during mid-winter. The most heavily affected units are Units 102 and 104 which will receive a minimum 2 hours solar access during mid-winter. Unit 101 will receive 2.5 hours with all other units receiving between 3-6 hours solar access. It is noted however that the existing building at 3 Homebush Road currently overshadows the southern adjoining units and that overshadowing particularly to Units 101, 102 and 104 will remain relatively unchanged as a result of the proposed development. In this instance, the proposed extent of overshadowing is considered acceptable.

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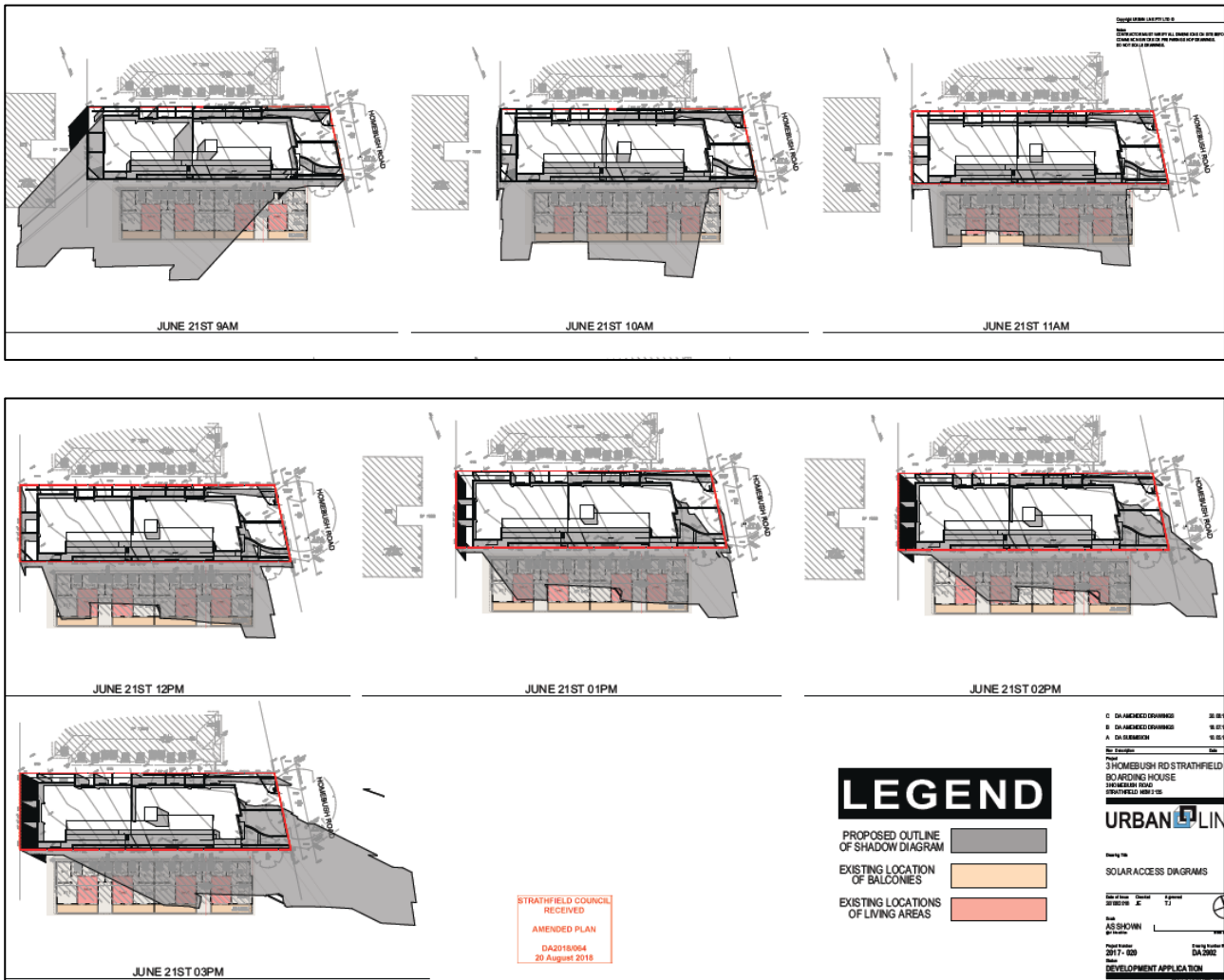


Figure 5: Solar Access Diagrams demonstrating extent of overshadowing to the southern adjoining property.

Overall, the proposed boarding house development has been designed with respect to residents immediately adjoining the site and achieves an appropriate outcome for the site.

4.15 (1)(c) the suitability of the site for the development

The subject site is located within close proximity to the Strathfield Railway Station as well as the Homebush Railway Station.

The site is therefore located within close proximity to bus services as well as a plethora of restaurants, shops and communal facilities to meet the needs of local residents.

The provision of low cost rental housing in the form of a boarding house is considered a suitable form of development for the subject site. Overall, the proposed development is considered to be of a high quality construction which will positively contribute to the streetscape without adversely impacting upon the amenity of future lodgers and adjoining residents.

The proposed development is therefore considered suitable for the site.

4.15 (1)(d) any submissions made in accordance with this Act or the regulations

The application was notified in accordance with Part L of the SDCDP 2005 from 5 June 2018 to 29 June 2018, with three (3) written submissions received, raising the following concerns:

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1. *Off-street Parking – Concern is raised that there is a shortfall of 9 spaces required as per the SEPP 2009.*

Assessing officer's comments: Amended plans were submitted during the assessment process demonstrating that an additional second level of basement parking has been provided so as to achieve the minimum (18) off-street parking spaces for lodger residents. Whilst an additional parking space is required for the on-site manager, a condition of consent has been recommended for the bicycle parking bay to be allocated as a carparking space and for the bicycle parking to be provided at-grade.

2. *Overdevelopment - The proposal is an overdevelopment of the site with very poor residential amenity. The proposal is not in keeping with the streetscape and does not meet compliance with the requirements under Part C Multiple Unit Housing of the SCDCP 2005.*

Assessing officer's comments: The requirements of Part C of the SCDCP 2005 do not apply to boarding house developments. Notwithstanding, an assessment against the relevant controls have been made whereby the development generally achieves general compliance with such requirements. This indicates that the development is not only suitable for the site but is appropriate to the transitioning character of the streetscape in terms of bulk, height, scale and design.

3. *Communal Room – The proposal provides a “Communal Room” with a useable space of about 28m² for 72 residents. This does not provide any residential amenity. Further the room will not achieve 3 hours solar access.*

Assessing officer's comments: During the assessment process, amended plans were submitted to Council demonstrating that the communal room and open space areas have been relocated from the rear south-western boundary to the rear north-western boundary of the site. The communal areas (both internal and external) will be capable of achieving a minimum 3 hours direct solar access throughout mid-winter.

4. *The building design with open walkways to access the rooms is reminiscent of 1950s public housing. These open walkways would be noisy, would have to be lit and would impact on the neighbouring residents.*

Assessing officer's comments: The open walkways are to be utilised as walkways only and will unlikely be utilised as gathering spaces so as to generate unacceptable noise to the detriment of adjoining residents. The open walkways will also allow improved ventilation to the building and is considered appropriate for the development.

Notwithstanding, timber louvers have been provided along various sections of the walkway so as to minimise any potential adverse amenity impacts for adjoining neighbours. A condition of consent will be recommended to ensure any lighting external to the building is appropriate and does not result in any unnecessary glare for adjoining residents (refer Condition 14).

5. *The proposal fails to provide 15% of the rooms in the development as Adaptable Housing as required under Part C Multiple Unit Housing of the SCDCP 2005.*

Assessing officer's comments: As previously discussed, the provisions of Part C of the SCDCP 2005 do not apply to the development and can only be utilised as a 'rule of thumb' assessment. Accordingly, the Applicant cannot be enforced to provide adaptable rooms within this development. Notwithstanding, units G.04 and 1.05 have been proposed as adaptable units.

4.15 (1)(e) the public interest

The public interest is served through the detailed assessment of this development application under the relevant local planning controls and legislation and consideration of any submissions

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received relating to it by Council. The proposed development is not considered to be contrary to the public interest.

SECTION 7.11 CONTRIBUTIONS

Section 7.11 of the EP&A Act 1979 relates to the collection of monetary contributions from applicants for use in developing key local infrastructure. The Act reads as follows:

- “(1) If a consent authority is satisfied that development for which development consent is sought will or is likely to require the provision of or increase the demand for public amenities and public services within the area, the consent authority may grant the development consent subject to a condition requiring:*
- (a) the dedication of land free of cost, or*
 - (b) the payment of a monetary contribution, or both.*
- (2) A condition referred to in subsection (1) may be imposed only to require a reasonable dedication or contribution for the provision, extension or augmentation of the public amenities and public services concerned.”*

STRATHFIELD DIRECT SECTION 7.11 CONTRIBUTIONS PLAN

Section 7.11 Contributions are applicable to the proposed development in accordance with the Strathfield Direct Development Contributions Plan 2010-2030 as follows:

Provision of Community Facilities	\$35,755.42
Provision of Major Open Space	\$162,676.45
Provision of Local Open Space	\$55,752.76
Provision Roads and Traffic Management	\$16,180.46
Administration	\$3,083.10
TOTAL	\$273,448.19

CONCLUSION

The application has been assessed having regard to Section 4.15 of the Environmental Planning and Assessment Act 1979, the Strathfield Local Environmental Plan 2012 and the Strathfield Development Control Plan 2005 and is considered to be satisfactory for approval, subject to the recommended conditions of consent.

RECOMMENDATION

That Development Application No. 2018/064 for demolition of existing residential flat building and construction of a boarding house comprised of (36) rooms accommodating up to (72) people over two (2) levels of basement parking under the Affordable Rental Housing SEPP 2009 at 3 Homebush Road, Strathfield be **APPROVED**, subject to the following conditions:

SPECIAL CONDITIONS (SC)

1. AMENDED LANDSCAPE PLAN (SC)

An amended landscape plan prepared by a qualified landscape architect or landscape consultant to a scale of 1:100 or 1:200, conforming to all relevant conditions of consent is to be submitted to the satisfaction of the Principal Certifying Authority with the Construction Certificate application.

The plan must include the following information:

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- i) details demonstrating compliance with SCDCP 2005 in relation to landscaping requirements;
- ii) location of all existing and proposed landscape features including materials to be used;
- iii) all trees to be retained, removed or transplanted;
- iv) existing and proposed finished ground levels;
- v) top and bottom wall levels for both existing and proposed retaining and free standing walls; and
- vi) a detailed plant schedule which includes proposed species listed by botanical (genus and species) and common names, quantities of each species, pot sizes and the estimated size at maturity. A minimum of 25% of species shall be locally sourced indigenous species. Species shall be predominantly selected from Council's Recommended Tree List.
- vii) details demonstrating that the common open space area has been relocated to the north-western portion of the site as dictated by the endorsed architectural plans.

Consideration within the design should be given to the scale of planting in proportion to the proposed development, consistency with the existing landscape character of the area, potential views, solar access and privacy for neighbouring development.

(Reason: Landscape quality.)

2. RE-ALLOCATION OF PARKING SPACE AND BICYCLE PARKING BAY (SC)

The bicycle parking bay on level 2 of the basement shall be redesigned as a carparking space which achieves compliance with AS/NZS 2890.1:2004. Bicycle parking is to be provided at-grade preferably along the southern side boundary of the site. Amended plans demonstrating compliance with these requirements shall be submitted to and approved by Council and a Principal Certifying Authority **prior to the issue of a Construction Certificate.**

(Reason: To achieve compliance with requirements of the ARH SEPP 2009)

GENERAL CONDITIONS (GC)

3. APPROVED PLANS AND REFERENCE DOCUMENTATION (GC)

The development, except where modified by a condition(s) of consent, is to be carried out in accordance with the following plans and reference documentation:

Plans affixed with Council's 'Development Consent' stamp relating to Development Consent No. 2018/064:

Drawing No.	Title/Description	Prepared by	Issue / Revision & Date	Date received by Council
DA1003	Demolition Plan	Urbanlink	C	20 August 2018
DA2001	Basement 02	Urbanlink	C	20 August 2018
DA2002	Basement 01	Urbanlink	C	20 August 2018
DA2003	Ground Floor Plan	Urbanlink	C	20 August 2018
DA2004	Level 01	Urbanlink	C	20 August 2018
DA2005	Level 02	Urbanlink	C	20 August 2018

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DA2005	Level 03	Urbanlink	C	20 August 2018
DA2005	Roof Plan	Urbanlink	C	20 August 2018
DA3001	North & South Elevations	Urbanlink	C	20 August 2018
DA3002	East & West Elevations and Streetscape	Urbanlink	C	20 August 2018
DA4001	Section AA & Ramp Section	Urbanlink	C	20 August 2018
DA6301	Colour Schedule	Urbanlink	C	20 August 2018
LS01	Landscape Plan	Melissa Wilson Landscape Architects	B	19 July 2018
A8180 - COVER	Stormwater Plans - Cover	Alpha Engineering	C	19 July 2018
A8180 – SW01	Sediment and Erosion Control Plan	Alpha Engineering	C	19 July 2018
A8180-SW02	Basement 2 Drainage Plan	Alpha Engineering	C	19 July 2018
A8180-SW03	Basement 1 Drainage Plan	Alpha Engineering	C	19 July 2018
A8180-SW04	Ground Floor Drainage Plan	Alpha Engineering	C	19 July 2018
A8180-SW05	Roof Drainage Plan	Alpha Engineering	C	19 July 2018
A8180-SW06	Stormwater Sections & Details	Alpha Engineering	C	19 July 2018

Reference Documentation affixed with Council's 'Development Consent' stamp relating to Development Consent No. 2018/064:

Title / Description	Prepared by	Issue/Revision & Date	Date received by Council
Flood Impact Report	Alpha Engineering	Ref.: A8180 - REV C Issue Date: 18th of JULY 2018	19 July 2018
Arboricultural Impact Assessment and Tree Management Plan	Redgum Horticultural	Prepared 20 June 2018	20 August 2018
Traffic Report	Varga Traffic Planning	18 July 2018 Ref 17262	19 July 2018

In the event of any inconsistency, the conditions of this consent shall prevail.

(Reason: To ensure that the form of the development undertaken is in accordance with the determination of Council.)

4. **BOARDING HOUSE - APPROVED USE (GC)**

- The boarding house must be operated in accordance with the Boarding Houses Act 2012.

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- The boarding house is required to be registered on a register administered by NSW Fair Trading within 28 days where a proprietor takes over an existing, or begins operating a new, registrable boarding house.

Council will carry out initial (within the first 12 months) and ongoing inspections in accordance with the Boarding Houses Act 2012 subject to payment of inspection fees as detailed in Council's Fees and Charges schedule.

- The boarding house is to comply with the approved Plan of Management required by this consent.
- The use of the premises as a boarding house must comply always with the following:
 - a) A copy of the Plan of Management and House Rules must be annexed to every tenancy/occupation agreement for a room;
 - b) A copy of the approved Plan of Management and House Rules must be clearly displayed within the common room in the building at all times;
 - c) All tenancy/occupation agreements for rooms within the premises must be for a minimum period of three (3) months;
 - d) The premises must be used exclusively as a boarding house containing a maximum total of **72** residents including 70 lodgers and 2 residents in the on-site manager's room, with **2** lodger(s) permitted per lodger room;
 - e) The premises must not be adapted for use as backpacker's accommodation, serviced apartments or a residential flat building;
 - f) All common rooms/areas and recreation rooms/areas must be maintained at all times for the use of the lodgers;
 - g) Each self-contained room be fitted out with washing up facilities, a cooktop, microwave oven, fridge and storage space with such utilities being maintained in working order at all times;
 - h) All rooms within the boarding house must be connected to a centralised electricity, water and gas (if installed) service and room tariffs must not include a separate charge for these services; and
 - i) The communal room shall be provided with an oven and cooktop.
- The Plan of Management must be updated to reflect the requirements outlined within this condition.
- Any proposed amendments to the Management Plan are to be submitted to Council for consideration and approval prior to any amendments being made.
- If the operator of the boarding house alters from the operator nominated in the Plan of Management, a new Plan of Management is to be submitted to Council within 28 days of the change in operator unless it is confirmed in writing that the new operator will comply with the Plan of Management approved as part of this consent.

(Reason: To ensure the development is the permanent place of residence for occupants and ensure occupants abide by the rules and regulations identified in the Plan of Management.

5. **BUILDING HEIGHT - MAXIMUM RL TO BE COMPLIED WITH (GC)**

The height of the building must not exceed RL25.5 to the roof ridge of the building.

(Reason: To ensure the approved building height is complied with.)

6. **CONSTRUCTION HOURS (GC)**

No construction or any other work related activities shall be carried out on the site outside

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the hours of 7.00 am to 5.00 pm Mondays to Fridays and 8am to 1pm Saturdays.

No building activities are to be carried out at any time on a Sunday or public holiday.

Where the development involves the use of jackhammers/rock breakers and the like or other heavy machinery, such equipment may only be used between the hours of 7.00 am - 5.00 pm Monday to Friday only.

(Reason: To maintain amenity to adjoining land owners.)

7. CONSTRUCTION WITHIN BOUNDARIES (GC)

The development including but not limited to footings, walls, roof barges and guttering must be constructed wholly within the boundary of the premises. No portion of the proposed structure shall encroach onto the adjoining properties. Gates must be installed so they do not open onto any footpath or adjoining land.

(Reason: Approved works are to be contained wholly within the subject site.)

8. DEMOLITION – GENERALLY (GC)

Alteration and demolition of the existing building is limited to that documented on the approved plans (by way of notation). No approval is given or implied for removal and/or rebuilding of any portion of the existing building which is not shown to be altered or demolished.

(Reason: To ensure compliance with the approved development.)

9. DEMOLITION - SITE SAFETY FENCING (GC)

Site fencing is to be erected to a minimum height of 1.8m complying with WorkCover Guidelines, to exclude public access to the site, prior to any demolition works occurring and maintained for the duration of the demolition works.

If applicable, a separate Hoarding Application for the erection of an A class (fence type) or B class (overhead type) hoarding/fencing along the street frontage(s) complying with WorkCover requirements must be obtained including:

- payment to Council of a footpath occupancy fee based on the area of footpath to be occupied and Council's Schedule of Fees and Charges before the commencement of work; and
- provision of a Public Risk Insurance Policy with a minimum cover of \$10 million in relation to the occupation of and works within Council's road reserve, for the full duration of the proposed works, must be obtained with a copy provided to Council.

(Reasons: Statutory requirement and health and safety.)

10. LANDSCAPING - CANOPY TREES IN FRONT SETBACK (GC)

New development shall provide at least two (2) canopy trees within the front setback adjacent the front boundary (existing trees may be included). New canopy trees shall be at least three (3) metres high at the time of planting and capable of reaching a mature height of 10m.

Species shall be selected from Council's Recommended Tree List (Available from Council's

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website or from the Customer Service Centre). Compliance with this condition is to be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To ensure the landscape character of the locality is maintained.)

11. **LANDSCAPING - IRRIGATION OF COMMON AND PRIVATE LANDSCAPE AREAS (GC)**

All common and private landscape areas including all planters of new multi-unit, commercial, mixed-use and industrial development are to have full coverage by a fully automatic irrigation system. The design, materials and installation are to be in accordance with Sydney Water Codes and satisfy all relevant Australian Standards. Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior occupation of the premises.

(Reason: To ensure appropriate landscape maintenance.)

12. **LANDSCAPING - TREES PERMITTED TO BE REMOVED (GC)**

The following listed trees are permitted to be removed to accommodate the proposed development:

All trees recommended to be removed as submitted in the arborist report by Redgum Horticultural prepared 20 June 2018 received by Council 20 August 2018 are permitted to be removed to accommodate the proposed development.

All trees permitted to be removed by this consent shall be replaced (2 trees for each tree removed) by species selected from Council's Recommended Tree List and must have a minimum mature height of 10 metres.

Replacement trees shall be a minimum 50 litre container size. Trees are to conform to the NATSPEC guide and Guide for assessing the quality of and purchasing of landscape trees by Ross Clarke, 2003. Trees are to be true to type, healthy and vigorous at time of delivery and planting, shall be pest and disease free, free from injury and wounds and self-supporting; and shall be maintained until maturity.

All trees are to be staked and tied with a minimum of three (3) hardwood stakes. Ties are to be hessian and fixed firmly to the stakes, one tie at half the height of the main stem, others as necessary to stabilise the plant.

Root deflection barriers having a minimum depth of 600mm are to be installed adjacent to all footpaths and driveways.

Soil conditioner/fertilizer/moisture retention additive/s are to be applied in accordance with manufacturer's recommendations, and mixed into the backfilling soil after planting tree/s.

A minimum 75mm depth of organic mulch shall be placed within an area 0.5m from the base of the tree.

Details demonstrating compliance shall be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To ensure appropriate planting back onto the site.)

13. **SITE ACCESS AND UTILITY SERVICES – TREE PRESERVATION (GC)**

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All street trees and trees on private property are protected under Council's planning controls.

All healthy trees and shrubs identified for retention on the approved drawings are to be suitably protected in accordance with Australian Standard AS4970 - 2009, Protection of Trees on Development Sites.

All protected trees are to be pruned in accordance with AS 4373 – 2007, Pruning of amenity trees, by and Australian Qualification framework level 3 qualified arborist.

Existing utility connections are to be maintained so that they do not impact on the canopy or root systems of Council's trees. No new utility services (including the temporary or permanent relocation of domestic power pole and connections) are to be located within the canopy or root zone of Councils Street trees without the written consent of Council.

The pruning of a Council tree for site access without Council's written consent is prohibited. Applicants are to make an application for street tree pruning 28 days prior to commencement of all demolition or site works. Applications for site access pruning are to include precise detailed information and support documentation regarding the branches to be pruned. As a general rule clearance pruning of branches of greater than 50mm diameter and a clearance height of 3.5 metres will not be approved.

(Reason: To ensure the protection of Council trees and all protected trees to be retained on the site.)

14. LIGHTING (GC)

Any lighting of the premises shall be installed and maintained in accordance with Australian Standard AS 4282-1997: Control of the Obtrusive Effects of Outdoor Lighting so as to avoid annoyance to the occupants of adjoining premises or glare to motorists on nearby roads.

No flashing, moving or intermittent lighting, visible from any public place may be installed on the premises or external signage associated with the development, without the prior approval of Council.

(Reason: To protect the amenity of surrounding development and protect public safety.)

15. MATERIALS – CONSISTENT WITH SUBMITTED SCHEDULE (GC)

All external materials, finishes and colours are to be consistent with the schedule submitted and approved by Council with the development application.

(Reason: To ensure compliance with this consent.)

16. PRINCIPAL CERTIFYING AUTHORITY (PCA) IDENTIFICATION SIGN (GC)

Prior to commencement of any work, signage must be erected in a prominent position on the work site identifying:

- i) the Principal Certifying Authority (PCA) by showing the name, address and telephone number of the PCA;
- ii) the Principal Contractor by showing the Principal Contractor's name, address and telephone number (outside of work hours) for that person; and
- iii) the sign must state that unauthorised entry to the work site is prohibited.

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Any such sign is to be maintained while the work is being carried out, but must be removed when the work has been completed.

This clause does not apply to building work, subdivision work or demolition work that is carried out inside an existing building that does not affect the external walls of the building.

(Reason: Statutory requirement.)

17. PRIVACY - SCREEN PLANTING TO SIDE AND REAR BOUNDARIES (GC)

Evergreen shrubs with a minimum 5 litre container size achieving a minimum mature height of 3m shall be planted along the side and rear boundaries to provide suitable privacy screening. Screen planting shall be maintained at all times. Details of the selected plants shall be submitted to and approved by the Principal Certifying Authority. Compliance with this condition shall be certified by the Principal Certifying Authority, prior to occupation of the premises.

(Reason: To maintain privacy amenity to neighbouring residences.)

18. SITE MANAGEMENT (DURING DEMOLITION AND CONSTRUCTION WORKS)

All of the following are to be satisfied/complied with during demolition, construction and any other site works:

- i) All demolition is to be carried out in accordance with Australian Standard AS 2601-2001.
- ii) Demolition must be carried out by a registered demolition contractor.
- iii) A single entrance is permitted to service the site for demolition and construction. Protection pads are to be installed to the kerb, gutter and nature strip where trucks and vehicles enter the site.
- iv) No blasting is to be carried out at any time during construction of the building.
- v) Care must be taken during demolition/ excavation/ building/ construction to prevent any damage to adjoining buildings.
- vi) Adjoining owner property rights and the need for owner's permission must be observed at all times, including the entering onto land for the purpose of undertaking works.
- vii) Any demolition and excess construction materials are to be recycled wherever practicable.
- viii) The disposal of construction and demolition waste must be in accordance with the requirements of the Protection of the Environment Operations Act 1997.
- ix) All waste on the site is to be stored, handled and disposed of in such a manner as to not create air pollution (including odour), offensive noise or pollution of land and/or water as defined by the Protection of the Environment Operations Act 1997. All excavated material should be removed from the site in an approved manner and be disposed of lawfully to a tip or other authorised disposal area.
- x) All waste must be contained entirely within the site.
- xi) A site plan including temporary waste storage locations, construction material storage areas and vehicular access to and from the site must be included with application.
- xii) Section 143 of the Protection of the Environment Operations Act 1997 requires waste to be transported to a place which can lawfully accept it. All non-recyclable demolition materials are to be disposed of at an approved waste disposal depot in accordance with legislation.
- xiii) Details as to the method and location of disposal of demolition materials (weight dockets, receipts, etc.) should be kept on site as evidence of approved methods of disposal or recycling and must be presented to Council and/or Principal Certifying

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- Authority upon request.
- xiv) All materials on site or being delivered to the site are to generally be contained within the site. The requirements of the Protection of the Environment Operations Act 1997 must be complied with when placing/stockpiling loose material, disposing of concrete waste, or other activities likely to pollute drains or water courses.
 - xv) Any materials stored on site must be stored out of view or in such a manner so as not to cause unsightliness when viewed from nearby lands or roadways.
 - xvi) Public footways, include nature strips and roadways adjacent to the site must be maintained and cleared of obstructions during construction. No building materials, waste containers or skips may be stored on the road reserve or footpath without prior separate approval from Council, including payment of relevant fees.
 - xvii) Building operations such as brick-cutting, washing tools or paint brushes, and mixing mortar not be performed on the roadway or public footway or any other locations which could lead to the discharge of materials into the stormwater drainage system.
 - xviii) All site waters during excavation and construction must be contained on site in an approved manner to avoid pollutants entering into waterways or Council's stormwater drainage system.
 - xix) Any work must not prohibit or divert any natural overland flow of water.
 - xx) Toilet facilities for employees must be provided in accordance with WorkCover NSW.

(Reason: To ensure that demolition, building and any other site works are undertaken in accordance with relevant legislation and policy and in a manner which will be non-disruptive to the local area.)

19. SYDNEY WATER - STAMPED PLANS PRIOR TO COMMENCEMENT (GC)

The approved plans must be submitted to a Sydney Water Quick Check agent or Customer Centre to determine whether the development will affect Sydney Water's sewer and water mains, stormwater drains and/or easements, and if further requirements need to be met. Plans will be appropriately stamped.

For Quick Check agent details please refer to the web site www.sydneywater.com.au (see Building Developing and Plumbing then Quick Check) or telephone 13 20 92. The consent authority or a Certifying Authority must ensure that a Quick Check agent/Sydney Water has stamped the plans before the commencement of work.

(Reason: Compliance with Sydney Water requirements.)

20. UTILITIES AND SERVICES - PROTECTION OF (GC)

Any footings or excavation to be located or undertaken adjacent to Council's stormwater must be designed to address the following requirements:

- i) all footings and excavation must be located wholly within the site and clear of any easement boundaries;
- ii) all footings and excavation must be located a minimum of 1000mm from the centreline of the pipeline or 500mm from the outside of the pipeline, whichever is the greater distance from the centreline; and
- iii) footings must extend to at least the depth of the invert of the pipeline unless the footings are to be placed on competent bedrock.

If permanent excavation is proposed beneath the invert of the pipeline, suitable means to protect the excavation and proposed retaining structures from seepage or other water flow from the pipeline and surrounding subsoil, must be provided. The design must be prepared

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by a qualified practicing Structural/Civil Engineer.

Construction plans must be approved by the appropriate utility's office (e.g. council, Sydney Water Corporation) to demonstrate that the development complies with the utility's requirements.

The applicant must provide written evidence (e.g. compliance certificate, formal advice) of compliance with the requirements of all relevant utility providers (e.g. Water, Energy, Telecommunications, Gas).

(Reason: To ensure protection of Council assets.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE (CC)

21. ACCESS - ACCESS FOR PEOPLE WITH DISABILITIES (CC)

Access for people with disabilities must be provided in accordance with the requirements of the Building Code of Australia, relevant Australian Standards and with regard to the Disability Discrimination Act 1992. Plans shall be notated demonstrating compliance and approved by the Principal Certifying Authority, prior to the issue of a Construction Certificate.

Note: Disability (Access to Premises - Buildings) Standards 2010 - As of 1 May 2011, if access is provided to the extent covered by this Standard, then such access cannot be viewed as unlawful under the Disability Discrimination Act 1992.

(Reason: To provide equitable access for people(s) with disabilities in accordance with the relevant legislation and Australian Standards.)

22. ACCESS - DISABLED TOILETS (CC)

Plans and details of the disabled toilet/s complying with the relevant Australian Standards, the Building Code of Australia, and with regard to the Disability Discrimination Act 1992. Plans shall be notated demonstrating compliance and approved by the Principal Certifying Authority, prior to the issue of a Construction Certificate.

Note: Disability (Access to Premises - Buildings) Standards 2010 - As of 1 May 2011, if access is provided to the extent covered by this Standard, then such access cannot be viewed as unlawful under the Disability Discrimination Act 1992.

(Reason: To provide equitable access for people(s) with disabilities in accordance with the relevant legislation and Australian Standards.)

23. BICYCLE STORAGE PROVISION (CC)

On-site provision for bicycle storage facilities shall be in accordance with the provisions of Clause 30 of the ARH SEPP. Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Compliance with SCDP 2005 and to encourage sustainable transport options.)

24. BOARDING HOUSE - CONSTRUCTION OF (CC)

The construction of the boarding house shall comply with all applicable legislation/regulation and standards, including:

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- i) Boarding Houses Act 2012;
- ii) Boarding Houses Regulation 2013;
- iii) Local Government Act 1993; and
- iv) Local Government Regulation 2005.

Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior to occupation of the premises.

(Reason: To ensure compliance with relevant legislation.)

25. BUILDING CODE OF AUSTRALIA - COMPLIANCE WITH (CC)

All architectural drawings, specifications and related documentation shall comply with the Building Code of Australia (BCA). All work must be carried out in accordance with the requirements of the Building Code of Australia (BCA).

In the case of residential building work for which the Home Building Act 1989 requires there to be a contract of insurance in force in accordance with Part 6 of that Act, such a contract of insurance is to be in force before any building work authorised to be carried out by the consent commences.

Details demonstrating compliance with this condition are to be submitted to the Principle Certifying Authority, prior to issue of the Construction Certificate.

(Reason: This is a 'prescribed' condition under clause 98(1) of the Environmental Planning and Assessment Regulation 2000.)

26. CAR PARKING - BASEMENT CAR PARKING REQUIREMENTS (CC)

Details demonstrating compliance with the following is to be submitted to the Principal Certifying Authority, prior to issue of the Construction Certificate:

- i) Minimal internal clearance of the basement is 2.2m in accordance with BCA requirements.
- ii) Driveways shall comply with Australian/New Zealand Standard AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking and achieve a maximum gradient of 1:4.
- iii) Basement entries and ramps/driveways within the property are to be no more than 3.5m wide.
- iv) Provision of pump-out systems and stormwater prevention shall be in accordance with Council's Stormwater Management Guide.

(Reason: To ensure suitable development.)

27. CAR PARKING - DISABLED CAR PARKING SPACES (CC)

Two (2) of the car parking spaces provided as part of the total requirement shall be reserved for disabled persons. These spaces shall be a minimum of 2.4m wide x 5.4m long and located near pedestrian access routes designed for disabled persons. Each space shall be clearly marked as such. A shared area 2.4 m wide x 5.4m long shared area should be provided. Bollard shall be placed in this shared area.

Car parking for people with disabilities shall be provided in accordance with the Building Code of Australia, relevant Australian Standards and with regard to the Disability Discrimination Act 1992. Prior to the issue of a Construction Certificate, the plans shall be notated to demonstrate compliance.

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The above details shall be submitted to and approved by the Principal Certifying Authority prior to the issue of a Construction Certificate.

(Reason: To ensure adequate parking for persons with a disability.)

28. CAR PARKING - VEHICULAR ACCESS RAMPS (CC)

Vehicular access ramps shall comply with the provisions of AS/NZS 2890.1:2004. Plans to be submitted shall contain the following details:

- i) Longitudinal section along the extreme wheel paths of each driveway/access ramp at a scale of 1:25 demonstrating compliance with the scraping provisions of AS/NZS 2890.1:2004. It shall include all levels and grades, including those levels stipulated as boundary levels, both existing and proposed. It shall extend from the centre-line of the roadway through to the parking area.
- ii) Sections showing the clearance to the underside of any overhead structure (minimum headroom requirements 2200mm minimum for standard headroom clearance or 2400mm where disabled access provisions are to be provided) demonstrating compliance with the provisions of AS/NZS 2890.1:2004.
- iii) Longitudinal section along the gutter line showing how it is intended to transition the vehicular crossing into the existing kerb and gutter. Boundary levels shall generally run parallel with the kerb levels.
- iv) Location of verge trees, street furniture and service installations.
- v) Superimposition of vehicle turning circles for access into parking spaces.
- vi) Certification that the design complies with AS/NZS 2890.1:2004 by a Qualified Engineer.

The certification referred to in (vi) above shall be submitted to the Principal Certifying Authority prior to the issue of a Construction Certificate (for the design) and to the Principal Certifying Authority prior to the issue of any Occupation Certificate for the 'as-built works'.

(Reason: To ensure adequate vehicular access can be achieved.)

29. CAR PARKING - COMPLIANCE WITH AS/NZS 2890.1:2004 (CC)

Car parking dimensions must comply with the Australian/New Zealand Standard AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking. Details demonstrating compliance are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: To ensure compliance with Australian Standards relating to parking of vehicles.)

30. CAR PARKING - VEHICULAR CIRCULATION, AISLE AND RAMP WIDTHS (CC)

The aisle widths, internal circulation, ramp widths and grades of the car park shall comply with the Roads and Traffic Authority Guidelines and AS/NZS 2890.1:2004 - Off-Street Car Parking Code.

Vehicular manoeuvring paths are to be prepared demonstrating that all vehicles can enter or depart the site in a forward direction without encroaching on required car parking spaces.

(Reason: To ensure compliance with Australian Standards relating to manoeuvring, access and parking of vehicles.)

31. COMMENCEMENT OF WORKS (NO WORKS UNTIL A CC IS OBTAINED)

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Building work, demolition or excavation must not be carried out until a Construction Certificate has been issued by either Strathfield Council or a Principal Certifying Authority.

Demolition of any part of a building triggers 'commencement of erection of building' pursuant of section 4.19 of the EP&A Act 1979. Accordingly, demolition works must not commence until a Construction Certificate has been issued, a Principal Certifying Authority has been appointed and a Notice of Commencement has been issued.

(Reason: To ensure compliance with statutory provisions.)

32. CONSTRUCTION AND ENVIRONMENTAL MANAGEMENT PLAN (CC)

The applicant must prepare and submit a Construction and Environmental Management Plan (CEMP) to the Principal Certifying Authority, including:

- i) Detailed information on any approvals required from other authorities prior to or during construction.
- ii) Traffic management, including details of:
 - ingress and egress of vehicles to the site;
 - management of loading and unloading of materials;
 - the location of heavy vehicle parking off-site; and
 - designated routes for vehicles to the site.
- iii) The proposed areas within the site to be used for a builder's site office and amenities, the storage of excavated material, construction materials and waste containers during the construction period.
- iv) Erosion and sediment control, detailing measures and procedures consistent with the requirements of Council's guidelines for managing stormwater, including:
 - the collection and treatment of stormwater and wastewater generated on site prior to discharge; and
 - procedures to prevent run-off of solid material and waste from the site.
- v) Waste management, including:
 - details of the types and estimated volumes of waste materials that will be generated;
 - procedures for maximising reuse and recycling of construction materials; and
 - details of the off-site disposal or recycling facilities for construction waste.
- vi) Dust control, outlining measures to minimise the generation and off-site transmission of dust and fine particles, such as watering or damp cloth fences.
- vii) A soil and water management plan, which includes:
 - measures to minimise the area of soils exposed at any one time and conserve top soil;
 - identification and protection of proposed stockpile locations;
 - preservation of existing vegetation and revegetation;
 - measures to prevent soil, sand, sediments leaving the site in an uncontrolled manner;
 - measures to control surface water flows through the site in a manner that diverts clean run-off around disturbed areas, minimises slope gradient and flow distance within disturbed areas, ensures surface run-off occurs at non-erodible velocities, and ensures disturbed areas are promptly rehabilitated;
 - details of sediment and erosion control measures in place before work commences;
 - measures to ensure materials are not tracked onto the road by vehicles entering or leaving the site; and
 - details of drainage to protect and drain the site during works.
- viii) Asbestos management procedures:

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- Anyone who removes, repairs or disturbs bonded or a friable asbestos material must hold a current removal licence from Workcover NSW holding either a Friable (Class A) or a Non- Friable (Class B) Asbestos Removal License which ever applies and a current WorkCover Demolition License where works involve demolition. To find a licensed asbestos removalist please see www.workcover.nsw.gov.au
- Removal of asbestos by a person who does not hold a Class A or Class B asbestos removal license is permitted if the asbestos being removed is 10m² or less of non-friable asbestos (approximately the size of a small bathroom). Friable asbestos materials must only be removed by a person who holds a current Class A asbestos license.
- Before starting work, a work site-specific permit approving each asbestos project must be obtained from WorkCover NSW. A permit will not be granted without a current WorkCover licence. All removal, repair or disturbance of or to asbestos material must comply with the following:
 - The Work Health and Safety Act 2011;
 - The Work Health and Safety Regulation 2011;
 - How to Safety Remove Asbestos Code of Practice – WorkCover 2011; and
 - Safe Work Australia Code of Practice for the Management and Control of Asbestos in the Workplace.
- Following completion of asbestos removal works undertaken by a licensed asbestos removalist re-occupation of a workplace must not occur until an independent and suitably licensed asbestos removalist undertakes a clearance inspection and issues a clearance certificate.
- The developer or demolition contractor must notify adjoining residents at least two (2) working days (i.e. Monday to Friday exclusive of public holidays) prior to the commencement of asbestos removal works. Notification is to include, at a minimum:
 - the date and time when asbestos removal works will commence;
 - the name, address and business hours contact telephone number of the demolisher, contractor and/or developer;
 - the full name and license number of the asbestos removalist/s; and
 - the telephone number of WorkCover's Hotline 13 10 50
 - warning signs informing all people nearby that asbestos removal work is taking place in the area. Signs should be placed at all of the main entry points to the asbestos removal work area where asbestos is present. These signs should be weatherproof, constructed of light-weight material and adequately secured so they remain in prominent locations. The signs should be in accordance with AS 1319-1994 Safety signs for the occupational environment for size, illumination, location and maintenance; and
 - appropriate barricades installed as appropriate to prevent public access and prevent the escape of asbestos fibres. Barricades must be installed prior to the commencement of asbestos removal works and remain in place until works are completed.

(Reason: Safety, amenity and protection of public infrastructure and the environment.)

33. COUNCIL PERMITS – FOR ALL ACTIVITIES ON COUNCIL LAND (CC)

Works Permit

(as per Section 68 of the Local Government Act 1993 and Section 138 and 139 of the Roads Act 1993)

A Works Permit is required for construction of a vehicular crossing (driveway), new stormwater down pipe connection to kerb and gutter, new footpath and/or stormwater

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connection. A Works Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Standing Plant Permit

This permit must be applied for where it is intended to park a concrete pump, crane or other plant on the roadway or footpath. A Standing Plant Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Skip Bin Permit

This permit must be applied for if you intend to place a skip bin on the roadway or footpath. A Skip Bin Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Temporary Full or Part Road Closure Permit

This permit must be applied for if you require a full or a part road closure to take place to assist in your construction works. Please use the Works Permit Application Form, which is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Hoarding/Fencing Permit

This permit must be applied for if you intend to erect a Class A (fence type) or Class B (overhead type) hoarding/fencing along the street frontage(s). A Hoarding Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Work Zone Permit

This permit must be applied for if you require permanent parking along the kerbside at the front of the site during construction works. A Work Zone Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Ground Anchoring Permit

This permit must be applied for, for the installation of ground anchors under Council's footway/road reserve. It does not cover ground anchors under private properties. A separate approval is required to be obtained from Roads and Maritime Services (RMS) if it is proposed to install ground anchors under a State or Classified Regional Road (please refer to the end of this application form for more information).

(Reason: Council requirement.)

34. DILAPIDATION REPORT - PRE-COMMENCEMENT (CC)

Subject to access being granted, a pre-commencement Dilapidation Report is to be undertaken on all adjoining properties, which in the opinion of a suitably qualified engineer, could be potentially affected by the construction of the project. The Dilapidation Report shall be carried out prior to the issue of a Construction Certificate.

The Dilapidation Report is to be prepared by a suitably Qualified Engineer with current Corporate Membership with the Institution of Engineers, Australia or Geotechnical Practitioner. The report shall include a photographic survey of adjoining properties detailing

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the physical condition of those properties, both internally and externally, including walls, ceilings, roof, structural members and other such items.

If access for undertaking the dilapidation survey is denied by an adjoining owner, the applicant must demonstrate, in writing, to Council's satisfaction attempts have been made to obtain access and/or advise the affected property owner of the reason for the survey and these attempts have been unsuccessful. Written concurrence must be obtained from Council in such circumstances.

The Report shall cover structural and geotechnical factors likely to arise from the development. A copy of this Report shall be submitted to Council as a record. The person having the benefit of the development consent must, at their own cost, rectify any damage caused to other properties during the construction of the project.

(Reason: To ensure no damage to adjoining properties occurs.)

35. DRIVEWAY WIDTH - MULTI-UNIT DEVELOPMENT (CC)

The internal driveway must be a minimum 5.50m wide (clear width) for the first 6 metres inside the property so as to allow entering & exiting vehicles to pass within the site. Should the driveway narrow after this point it is then to be designed with a minimum 1.5m x 1.5m splay to allow the passing to work. Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior the issue of a Construction Certificate.

(Reason: Safety and traffic management.)

36. DRIVEWAY DESIGN – CONVEX MIRROR AND STOP SIGN ON EXIT (CC)

The applicant shall install a stop sign and a convex mirror at the exit from the site. The stop sign must be accompanied by the associated line marking. The devices shall be designed and constructed in accordance with the provision of all relevant Australian Standards. The building plans shall indicate compliance with this requirement, prior to the issue of a Construction Certificate.

(Reason: Traffic safety and management.)

37. EROSION AND SEDIMENTATION CONTROL PLAN (CC)

An Erosion and Sediment Control Plan is to be prepared where construction or excavation activity requires the disturbance of the soil surface and existing vegetation. Details including drawings and specifications must provide adequate measures for erosion and sediment control to ensure:

- i) Compliance with the approved Soil and Water Management Plan.
- ii) Removal or disturbance of vegetation and top soil is confined to within 3m of the approved building area (no trees to be removed without approval).
- iii) All uncontaminated run-off is diverted around cleared or disturbed areas.
- iv) Silt fences or other devices are installed to prevent sediment and other debris escaping from the cleared or disturbed areas into drainage systems or waterways.
- v) All erosion and sediment controls are fully maintained for the duration of demolition/development works.
- vi) Controls are put into place to prevent tracking of sediment by vehicles onto adjoining roadways.
- vii) All disturbed areas are rendered erosion-resistant by turfing, mulching, paving or similar.
- viii) All water pumped or otherwise removed from excavations or basement areas is

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filtered to achieve suspended solids/non filterable residue levels complying with the Australian Water Quality guidelines for Fresh and Marine Waters.

- ix) Pumped or overland flows of water are discharged so as not to cause, permit or allow erosion before the commencement of work (and until issue of the occupation certificate).

Details of the proposed soil erosion and sedimentation controls are to be submitted to the Principal Certifying Authority with the Construction Certificate Application. Under no circumstances are any works to commence, prior to these details being approved by the Principal Certifying Authority and the controls being in place on the site.

(Reason: Environmental protection.)

38. EXCAVATION - AFFECTING ADJOINING LAND (CC)

If an excavation associated with the approved development extends below the level of the base of the footings of a building on an adjoining allotment of land, the person having the benefit of the development consent must, at the person's own expense, comply with the requirements of clause 98E of the Environmental Planning and Assessment Regulation 2000, including:

- i) protect and support the adjoining premises from possible damage from the excavation, and
- ii) where necessary, underpin the adjoining premises to prevent any such damage.

The condition referred to in subclause (i) does not apply if the person having the benefit of the development consent owns the adjoining land or the owner of the adjoining land has given consent in writing to that condition not applying. Details shall be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Structural safety.)

39. FLOODING - COMPLY WITH FLOOD IMPACT REPORT (CC)

The applicant shall comply with the flood recommendations provided in the Flood Impact Report prepared by ALPHA Engineering and Development ref: A8180 – REV C dated 18 July 2018. A certificate from a suitably qualified Engineer shall be submitted to the Certifying Authority stating compliance with these recommendations, prior to the issue of a Construction Certificate.

(Reason: To mitigate flood risk and associated damage.)

40. SECTION 7.11 CONTRIBUTION PAYMENT - DIRECT CONTRIBUTIONS PLAN (CC)

In accordance with the provisions of Section 7.13 of the Environmental Planning and Assessment Act 1979 and the Strathfield Direct Development Contributions Plan 2010-2030, a contribution in the form of cash, cheque or credit card (financial transaction fee applies) shall be paid to Council for the following purposes:

Provision of Community Facilities	\$35,755.42
Provision of Major Open Space	\$162,676.45
Provision of Local Open Space	\$55,752.76
Provision Roads and Traffic Management	\$16,180.46
Administration	\$3,083.10
TOTAL	\$273,448.19

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The total amount of the contribution is valid as at the date of determination and is subject to quarterly indexation. Contributions shall be indexed at the time of payment in accordance with clause 2.14 of the Strathfield Direct Development Contributions Plan 2010-2030.

Contributions must be receipted by Council and submitted to the Accredited Certifier, prior to the issue of any Construction Certificate.

Please present a copy of this condition when paying the contribution at the Customer Service Centre so that it can be recalculated.

Note: A copy of Strathfield Council's Section 7.11 Direct Development Contributions Plan may be downloaded from Council's website.

(Reason: To enable the provision of public amenities and services required/anticipated as a consequence of increased demand resulting from the development.)

41. **SECURITY PAYMENT - DAMAGE DEPOSIT FOR COUNCIL INFRASTRUCTURE (CC)**

A security (damage deposit) of **\$24,000.00** (calculated in accordance with Council's adopted Fees and Charges) shall be paid to Council, prior to the issue of a Construction Certificate.

The deposit is required as security against any damage to Council property during works on the site. The applicant must bear the cost of all restoration works to Council's property damaged during the course of this development. All building work must be carried out in accordance with the Building Code of Australia.

Payment may be accepted in the form of cash, bank guarantee, cheque or credit card (financial transactions fees apply). Note: Additional fees apply for the lodgement of a bank guarantee in lieu of cash bond applies in accordance with Council's adopted Fees and Charges.

Any costs associated with works necessary to be carried out to rectify any damages caused by the development, shall be deducted from the Damage Deposit.

Note: Should Council property adjoining the site be defective e.g. cracked footpath, broken kerb etc., this should be reported in writing, or by photographic record, submitted to Council at least seven (7) days prior to the commencement of any work on site. This documentation will be used to resolve any dispute over damage to infrastructure. It is in the applicant's interest for it to be as full and detailed as possible.

The damage deposit shall be refunded upon completion of all works upon receipt of a Final Occupation Certificate stage and inspection by Council.

(Reason: Protection of Council infrastructure.)

42. **STORMWATER DRAINAGE (CC)**

The following stormwater precautions are required to be provided on the site:

- i) Grated drains shall be provided along the driveway/basement entry at the vehicular crossing(s) and are to connect to the internal drainage system.
- ii) Overflow paths shall be provided to allow for flows in excess of the capacity of the pipe/drainage system draining the site.
- iii) Allowance shall be made for surface runoff from adjacent properties, and to retain existing surface flow path systems through the site. Any redirection or treatment of

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- these flows shall not adversely affect any other properties
- iv) Boundary fencing shall be erected in such a manner as not to interfere with the natural flow of ground and surface water to the detriment of any other party.

(Reason: To ensure the site is drained appropriately and does not interfere with the natural flow of flood waters.)

43. **TRAFFIC - CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CC)**

A Construction Traffic Management Plan (CTMP) is to be prepared by an appropriately qualified Traffic Management Consultant and submitted to and approved by Council's Engineering Section, prior to the commencement of any works including demolition.

The following matters should be addressed in the CTMP (where applicable):

- i) description of the demolition, excavation and construction works;
- ii) site plan/s showing the site, roads, footpaths, site access points and vehicular movements;
- iii) size, type and estimated number of vehicular movements (including removal of excavated materials, delivery of materials and concrete to the site);
- iv) proposed route(s) from the arterial (state) road network to the site and the proposed route from the site back to the arterial road network;
- v) impacts of the work and vehicular movements on the road network, traffic and pedestrians and proposed methods to safely manage pedestrians and construction related vehicles in the frontage roadways;
- vi) any Traffic Control Plans (TCP's) proposed to regulate traffic and pedestrian movements for construction activities (such as concrete pours, crane installation/removal etc.);
- vii) proposed hours of construction related activities and vehicular movements to and from the site;
- viii) current/proposed approvals from other Agencies and Authorities (including Roads and Maritime Services, Police and State Transit Authority);
- ix) any activities proposed to be located or impact upon Council's road, footways or any public place;
- x) measures to maintain public safety and convenience;
- xi) any proposed road and/or footpath closures;
- xii) turning areas within the site for construction and spoil removal vehicles, allowing a forward egress for all construction vehicles on the site;
- xiii) locations of work zones (where it is not possible for loading/unloading to occur on the site) in the frontage roadways accompanied by supporting documentation that such work zones have been approved by the Local Traffic Committee and Council;
- xiv) location of any proposed crane and concrete pump and truck standing areas on and off the site (and relevant approvals from Council for plant on road);
- xv) a dedicated unloading and loading point within the site for all construction vehicles, plant and deliveries;
- xvi) material, plant and spoil bin storage areas within the site, where all materials are to be dropped off and collected;
- xvii) on-site parking area for employees, tradespersons and construction vehicles as far as possible;
- xviii) proposed areas within the site to be used for the storage of excavated material, construction materials and waste and recycling containers during the construction period; and
- xix) how it is proposed to ensure that soil/excavated material is not transported onto surrounding footpaths and roadways.

(Reason: To mitigate traffic impacts on the surrounding area during the construction period.)

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44. **TREE BONDS (CC)**

A tree bond of **\$6,600.00** (calculated in accordance with Council's adopted Fees and Charges) shall be paid to Council, prior to the issue of a Construction Certificate.

The deposit is required as security against any damage to existing trees to be retained on Council's road reserve, during works on the site. The applicant must bear the cost of all restoration works to Council's property damaged during the course of this development.

Payment may be accepted in the form of cash, bank guarantee, cheque or credit card (financial transactions fees apply). Note: Additional fees apply for the lodgement of a bank guarantee in lieu of cash bond applies in accordance with Council's adopted Fees and Charges.

A refund of the Tree Bond must be made in writing.

Tree Bonds may be forfeited if a tree is dead, made dangerous or has been terminally damaged, or will be held until tree/s have fully recovered from the construction damage or were replacement/planted trees have become fully established and are over 6 metres in height.

(Reason: To ensure the protection of trees to be retained on Council's Road Reserve.)

45. **VEHICULAR CROSSINGS - WORKS PERMIT FOR CONSTRUCTION OF (CC)**

Full-width, heavy-duty concrete vehicular crossing(s) shall be provided across the footpath at the entrance(s) and/or exit(s) to the site and designed in accordance with Council's guidelines and specifications. In this regard, a Works Permit is to be obtained (available from Council's Customer Services Centre or downloaded from Council's website), and the appropriate fees and charges paid, prior to the lodgement of a Construction Certificate.

(Reason: To ensure appropriate access to the site can be achieved.)

46. **BIN ALLOCATION (CC)**

(a) Multi Dwelling Housing Developments requiring communal bin storage and the residential component of shop top housing developments.

The development will require the provision of the following waste and recycling facilities:

- Domestic Waste – [10] x [240L] mobile bins.
- Domestic Recycling – [4] x [240L] mobile bins.

Applicants requesting 660L bins must provide onsite collection and have a large hardstand for bin presentation.

(Reason: To ensure sufficient space and waste collection containers provided.)

47. **WASTE AND RECYCLING STORAGE ROOMS (CC)**

Waste (including grease arrestor rooms) and recycling storage rooms must meet all of the following conditions:

- i) The waste and recycling storage room must be of adequate dimensions to comfortably accommodate the required number of waste and recycling bins

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- 240L bin must have 1.1m² of space per bin
- 660L bin must have 2.03m² of space per bin
- ii) The layout of the waste and recycling storage room must allow easy unobstructed access to all bins (stacked bin arrangements are not acceptable) and allow the bins to be easily removed for servicing purposes.
- iii) A smooth concrete floor graded and drained to a floor waste connected to the sewer of the Water Board.
- iv) The walls being cement rendered with the intersection of the walls and floor being coved to a radius of not less than 25mm.
- v) The door being close fitting to prevent the access of rats and mice.
- vi) A cold water hose cock being provided for the cleaning of containers and the room itself.
- vii) Ventilation being provided by means of direct connection to the outside air to the satisfaction of Council.
- viii) The ceiling of all waste storage rooms must be constructed of a rigid smooth faced non-absorbent material. The ceiling must be a minimum of 2.4m high to accommodate Councils standard bins. This enables access for use, cleaning and enables the lids of bins to be fully opened.
- ix) Contain a sign, minimum size 600mm x 600mm, directing residents not to place recyclables in garbage carts and encouraging residents to recycle. Sample artwork and signs available from Council.
- x) Arrangements must be in place regarding the regular maintenance and cleaning of waste management facilities.
- xi) A caretaker or individual(s) shall be nominated as being responsible for transferring the bins to the collection point and back into the waste storage room/area.
- xii) Detailed plans of waste and recycling storage rooms must be submitted along with Waste Management Plan and Waste and Recycling Storage Room/Area Design Checklist.

(Reason: To keep garbage rooms in a clean and sanitary condition to protect public health.)

48. BULKY WASTE STORAGE (CC)

A store room for storage of bulky household waste such as lounges is to be provided which meets the following conditions:

- i) Bulky waste rooms/areas must be a minimum of 4m² per 10 dwellings.
- ii) Bulky waste (Clean Up) rooms/areas should be located adjacent to waste and recycling storage rooms.
- iii) Bulky waste rooms/areas must contain a sign, minimum size 600mm x 600mm, directing residents the types of waste which can be deposited and the collection process. Sample artwork and signs available from Council.

(Reason: To ensure hard waste is separated and collected efficiently.)

49. ONSITE WASTE COLLECTION

Onsite waste and recycling collection is to be provided, all onsite collection areas must meet the following conditions:

- i) Minimum vehicle access and standing area dimensions:
 - Width 3.6m
 - Height 3.6m
 - Length 14m
 - Path of travel of waste collection vehicle must be identified on plans; ensure

**DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)**

turning circle, height and width requirements are met throughout entire path of travel.

- ii) Maximum grade of ramps and driveway 1:5 (20%) suitable for collection vehicle access.
- iii) Waste collection area must have room for all waste receptacles and the waste collection vehicle.
- iv) Applicant can select the following:

At-grade collection

- A waste collection point is to be provided:
 - behind the front building line,
 - alongside the driveway into the site.
 - Collection vehicles may access the at grade collection point in a forward or backwards direction and exit in a forward direction only (waste vehicles must not reverse along a main road).
- v) Collection vehicles must not impede access to, within or from the site for other users.
- vi) The path of travel for moving bins from the storage area to the identified collection point (if collection is to occur away from the storage area) should be clearly identified.
- vii) The waste collection point shall be no more than 15m from the property boundary at the street.
- viii) There should be convenient and step free access between the waste storage room/area and the collection point.
- ix) The collection area must be designed so that the bin standing area, and the standing area for the vehicle, is level.
- x) The collection area must be designed so that there is sufficient room for the standing and manoeuvring of all waste receptacles as well as standing room for the collection vehicle.
- xi) Any shutter or roller door providing access to the site/waste collection point must be fitted with a master key to Council's requirements to allow access.

(Reason: To ensure an efficient and effective collection system.)

50. WATER SUSTAINABILITY – GENERALLY (CC)

Water collected in the rainwater tank must comprise roof water only and not surface water. Water from the rainwater tank must only be used for following purposes and not for human consumption:

- i) toilet flushing;
- ii) clothes washing;
- iii) garden irrigation;
- iv) car washing and similar outdoor uses;
- v) filling swimming pools, spa pools and ornamental ponds; and
- vi) fire fighting.

(Reason: To promote sustainable water management practice.)

51. WORKS ZONE - APPROVAL BY COUNCIL'S TRAFFIC COMMITTEE (CC)

An application for a 'Works Zone' must be submitted to and approved by the Strathfield Council Traffic Committee prior to the commencement of any site work (including demolition).

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

The suitability of the proposed length and duration of the Works Zone is to be demonstrated in the application for the Works Zone. The application for the Works Zone must be submitted to Council at least six (6) weeks prior to the commencement of work on the site to allow for assessment and tabling of agenda for the Strathfield Council Traffic Committee.

The requirement for a Works Zone may be varied or waived only if it can be demonstrated in the Construction Traffic Management Plan (to the satisfaction of Council) that all construction related activities (including all loading and unloading operations) can and will be undertaken wholly within the site. The written approval of Council must be obtained to provide a Works Zone or to waive the requirement to provide a Works Zone prior to the commencement of any site work.

(Reason: Council requirement.)

CONDITIONS TO BE SATISFIED PRIOR TO THE COMMENCEMENT OF WORKS (CW)

52. APPOINTMENT OF A PRINCIPAL CERTIFYING AUTHORITY (PCA) (CW)

No work shall commence in connection with this Development Consent until:

- i) A construction certificate for the building work has been issued by the consent authority or a Principal Certifying Authority.
- ii) The person having the benefit of the development consent has appointed a principal certifying authority for the building work, and notified the principal certifying authority that the person will carry out the building work as an owner/builder, if that is the case.
- iii) The principal certifying authority has, no later than 2 days before the building work commences:
 - notified the Council of his or her appointment, and
 - notified the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- iv) The person having the benefit of the development consent, if not carrying out the work as an owner-builder, has:
 - appointed a principal contractor for the building work who must be the holder of a contractor licence if any residential building work is involved;
 - notified the principal certifying authority of such appointment; and
 - unless that person is the principal contractor, notified the principal contractor of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- v) The person having the benefit of the development consent has given at least 2 days' notice to the Council of the person's intention to commence the erection of the building.

Note: If the principal certifying authority is the Council, the nomination will be subject to the payment of a fee for the service to cover the cost of undertaking all necessary inspections and the issue of the appropriate certificates.

Under the Environment Planning and Assessment (Quality of Construction) Act, 2003, a sign must be erected in a prominent position on the work site showing the name, address and telephone number of the principal certifying authority; the name of the principal contractor (if any) for the building work and a telephone number at which that person may be contacted outside working hours. That sign must also state that unauthorised entry is prohibited. The sign must not be removed until all work has been completed.

(Reason: Statutory requirement.)

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Lot 20 DP 1101067 (Cont'd)

53. NOTICE OF COMMENCEMENT (CW)

No work shall commence until the following details are submitted to Council:

- i) a Notice of Commencement (form will be attached with issue of a Construction Certificate or available from our website) within two (2) days of the date on which it is proposed to commence works associated with the Development Consent;
- ii) details of the appointment of a Principal Certifying Authority (either Council or another Principal Certifying Authority); and
- iii) details of the name, address and licence details of the Builder.

(Reason: Statutory requirement.)

CONDITIONS TO BE SATISFIED DURING DEMOLITION AND BUILDING WORKS (DW)

54. FILL MATERIAL (DW)

The only waste derived material which may be received at the development site is:

- i) Virgin excavated material (within the meaning of the Protection of the Environment Operations Act 1997), and
- ii) any other waste-derived material the subject of a resource recovery exemption under cl.51A of the Protection of the Environment Operations (Waste) Regulation 2005 that is permitted to be used as fill material.

Any (b)-type material received at the development site must be accompanied by documentation certifying by an appropriately qualified environmental consultant the materials compliance with the exemption conditions; and this documentation must be provided to the Principal Certifying Authority on request.

(Reason: To ensure imported fill is of an acceptable standard for environmental protection purposes.)

55. OBSTRUCTION OF PUBLIC WAY NOT PERMITTED DURING WORKS (DW)

The public way must not be obstructed by any materials, vehicles, refuse, skips or the like, under any circumstances, without the prior approval of Council.

(Reason: To maintain public access and safety.)

56. PUBLIC INFRASTRUCTURE AND SERVICES (DW)

The applicant must comply with the requirements (including financial costs) of any relevant utility provider (e.g. Energy Australia, Sydney Water, Telstra, RMS, Council etc.) in relation to any connections, works, repairs, relocation, replacements and/or adjustments to public infrastructure or services affected by the development.

(Reason: To maintain public infrastructure and/or services.)

57. SURVEY REPORT OF APPROVED LEVELS DURING AND POST CONSTRUCTION (DW)

A Survey Certificate to Australian Height Datum shall be prepared by a Registered Surveyor as follows:

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

- i) At the completion of the first structural floor level prior to the pouring of concrete indicating the level of that floor and the relationship of the building to the boundaries.
- ii) At the completed height of the building, prior to the placement of concrete inform work, or the laying of roofing materials.
- iii) At the completion of the development.

Progress certificates in response to points (1) through to (3) shall be provided to the Principal Certifying Authority at the time of carrying out relevant progress inspections. Under no circumstances is work allowed to proceed until such survey information is submitted to and approved by the Principal Certifying Authority.

(Reason: To ensure compliance with the approved plans.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF AN OCCUPATION CERTIFICATE (OC)

58. ENGINEERING WORKS (CERTIFICATION OF)

Prior to occupation of the premises, a Work As Executed (WAE) Plan of all engineering and/or drainage works is to be submitted to the Principal Certifying Authority. The WAE Plan is to be certified by a suitably Qualified Engineer, with Corporate Membership standing in the Institution of Engineers Australia and registered on the National Professional Engineers Register (NPER) under the appropriate professional category, demonstrating that:

- i) the stormwater drainage system; and/or
- ii) the car parking arrangement and area; and/or
- iii) any related footpath crossing works; and/or
- iv) the proposed basement pump and well system; and/or
- v) the proposed driveway and layback; and/or
- vi) any other civil works

have been constructed in accordance with the approved plans and any relevant Standards and Council policies/specifications.

For major works, such as subdivisions, works within the road reserve (requiring separate S138 approval) and as where specified by Council, a Part 4A Certificate will be required. It is strongly recommended that an Engineer supervise all engineering related works.

Where Council is not the Principal Certifying Authority, an electronic copy of the above documents is to be provided to Council, prior to the issue of any Occupation Certificate.

(Reason: Asset management.)

59. LANDSCAPING - ARBORIST'S FOLLOW UP REPORT OF TREE/S TO BE RETAINED (OC)

As part of the on-going assessment of the tree/s to be retained, the consulting arborist engaged by the applicant is to assess their health and any impacts suffered by them as a result of the proposed approved development, prior to the issue of an Occupation Certificate. Findings are to be compiled in a detailed report and submitted to the Principle Certifying Authority, which documents the following:

- i) methods of excavation or construction used to carry out the works;
- ii) any damage sustained by the tree/s as a result of the works;
- iii) any subsequent remedial works required to be carried out by the consulting arborist

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

- as a result of the damage; and
- iv) any future or on-going remedial work required to be carried out to ensure the long term retention of the tree/s.

(Reason: To ensure the survival of trees to be retained.)

60. OCCUPATION OF BUILDING (OC)

A person must not commence occupation or use (or change of use where an existing building) of the whole or any part of a new building (within the meaning of section 109H (4) of the Act) unless an Interim Occupation Certificate or Final Occupation Certificate has been issued in relation to the building or part.

The Principal Certifying Authority is required to be satisfied, amongst other things, that:

- i) all required inspections (including each applicable mandatory critical stage inspection) have been carried out; and
- ii) any preconditions to the issue of the certificate required by a development consent have been met.

Note: New building includes an altered portion of, or an extension to, an existing building.

(Reason: Statutory requirement.)

61. STORMWATER - CERTIFICATION OF THE CONSTRUCTED DRAINAGE SYSTEM (OC)

The constructed stormwater system shall be certified by a suitably qualified person, in accordance with Council's Stormwater Management Code, prior to the issue of any Occupation Certificate.

(Reason: Adequate stormwater management.)

62. STORMWATER - COVENANT AND RESTRICTION AS TO USER FOR STORMWATER CONTROLLED SYSTEMS (OC)

Prior to the issue of any Occupation Certificate, the applicant shall register a Positive Covenant and a Restriction as to User under section 88E and or section 88B of the Conveyancing Act as appropriate in favour of Council, ensuring the ongoing retention, maintenance and operation of the stormwater facility (on-site detention, pump-out, charged lines, etc).

Where any drainage line or service conduit is to traverse any property other than that which it serves, an appropriate easement will be required. In this case, the applicant shall register an easement no less than 1200mm wide over the proposed drainage line or service concurrently with any subdivision registration.

The wording on the 88E and/or the 88B Instrument is to make reference to the Council file where the Construction plans and the Work As Executed (as built), plans are held. Typical wording can be obtained from Council's Specification for the Management of Stormwater document.

(Reason: To ensure the on-site detention and/or pump system is maintained to an appropriate operational standard.)

CONDITIONS TO BE SATISFIED DURING ONGOING USE OF THE PREMISES (OU)

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

63. BOARDING HOUSES - OPERATIONAL PLAN OF MANAGEMENT (OU)

An Operational Plan of Management is to be formulated and submitted to Council for approval, prior to the issue of any Occupation Certificate.

The Plan of Management must include the minimum criteria as stipulated in Schedule 2 (Standards for Places of Shared Accommodation) of the Local Government (General) Regulation, 2005 under the Local Government Act 1993, the Public Health Act, 1991, Boarding Houses Act 2012 and Boarding Houses Regulation 2013.

The plan shall be dated and the name and signature of the author of the document shall be included. The Plan of Management shall be displayed in prominent locations within the premises and a copy of the Plan shall be provided to all residents of the boarding house. The on-site Caretaker shall be responsible for ensuring that the terms of the Plan are adhered to at all times.

This Operational Plan of Management must incorporate the following:

- i) All the measures to be implemented on the premises in terms of safety & security, amenity and health including but not limited to matters relating to curfews and after-hours access, bringing of visitors on to the site, drug and alcohol policy (including smoking), and use of communal areas etc.
- ii) All the responsibilities of the Caretaker/Manager.
- iii) An Incident Register to be maintained by the Caretaker. This Register must be produced upon demand by any Council officer or NSW Police Officer. The Register must contain a direction that all incidents of a criminal nature are to be reported to the Police immediately.
- iv) Prior to commencement of the boarding house use, a notification letter shall be forwarded to Council, the Police and neighbours providing contact details for the Caretaker/Manager so that any issues regarding the operation of the premises etc can be addressed promptly. Evidence of this letter being forwarded as required in this condition shall be provided to the Principal Certifying Authority, prior to the issue of any Occupation Certificate and commencement of the use.

The Plan of Management referred to in this condition shall be reviewed by Council annually from the date of issue of an Occupation Certificate by the Principal Certifying Authority. The operator of the boarding house shall contact Council annually to carry out this review and the review shall include the Incident Register referred to in point (iii) above. Should amendments be made to the Plan of Management an updated version of same shall be provided to Council and the residents of the boarding house within one (1) month of the annual review being completed.

(Reason: To minimise the impact of the use on surrounding residences.)

64. BOARDING HOUSE - ONGOING USE OF (OU)

Ongoing operation of the approved use for the purpose of a boarding house shall comply with the following:

- i) Use of the premises shall remain consistent with the definition of 'boarding house' as defined under Strathfield Local Environmental Plan 2012 as a building that:
 - is wholly or partly let in lodgings, and
 - provides lodgers with a principal place of residence for 3 months or more, and
 - may have shared facilities, such as a communal living room, bathroom, kitchen or laundry, and

**DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)**

- has rooms, some or all of which may have private kitchen and bathroom facilities, that accommodate one or more lodgers but does not include backpackers" accommodation, a group home, a serviced apartment, seniors housing or hotel or motel accommodation.
- ii) Subdivision of the approved boarding house under strata or community title is prohibited.
- iii) The use and operation of the boarding house shall comply with all applicable legislation/regulation and standards, including:
 - Boarding Houses Act 2012;
 - Boarding Houses Regulation 2013;
 - Local Government Act 1993; and
 - Schedule 2 (Standards for Places of Shared Accommodation) of the Local Government (General) Regulation 2005.
- iv) The maximum number of persons accommodated in a bedroom, or in a cubicle of a dormitory, must not exceed the number determined by allowing a minimum floor area within the bedroom or cubicle in accordance with the relevant provisions under the Public Health Act 1991 for each person.
- v) No speakers or music is permitted outdoors or in the communal areas.
- vi) The Boarding house is required to be registered on a register administered by NSW Fair Trading.

(Reason: To minimise the impact of the boarding house on surrounding residences.)

65. MAINTENANCE AND PROTECTION OF LANDSCAPING AND SOFT LANDSCAPED AREAS (OU)

The applicant and property owner/s are to maintain and enhance the completed landscape works in all private and strata areas, in accordance with the approved landscape design and conditions of development (inclusive of the maintenance and protection of all retained and planted trees).

In this regard the applicant is to provide an annual statement to Council, on or by the 1 July of each calendar year, for the first 5 years after the issue of the occupation certificate, from an AQF Level 5 Landscape Designer or Landscape Architect certifying that;

- All plants (including planted and retained trees) are in a healthy safe condition.
- The turf areas are well maintained.
- The landscaped area is free of all notifiable weeds, pests and diseases as detailed in the NSW Biosecurity Act 2015.
- The landscaped area is free of hazards.
- The mulched areas are well maintained.
- The irrigation systems, sub soil drainage and lighting systems are in a good condition and operating according to both design and manufacturers specifications.
- That all missing plants have been replaced and are establishing well.

This annual statement shall also include reports and information from relevant expert consultants, in support of the statements made and as evidence of proof, in the annual statement.

(Reason: To protect landscape amenity and community environmental values)

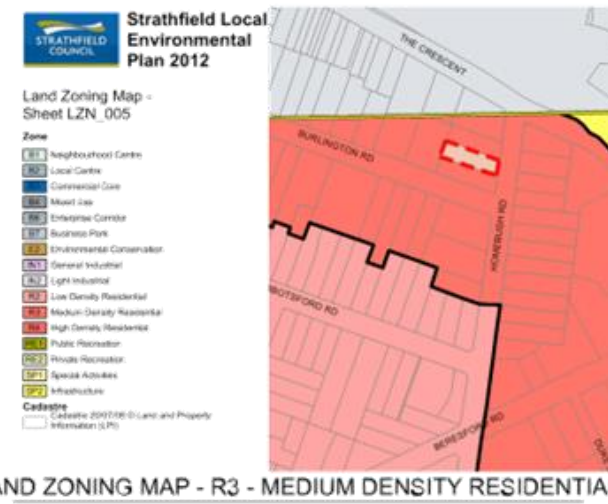
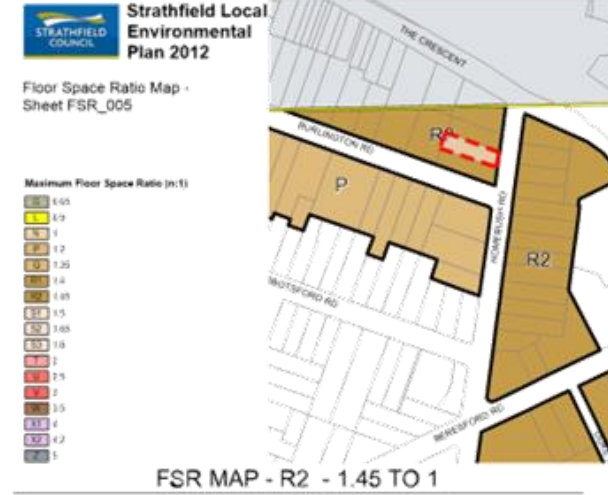
ATTACHMENTS

1. [↓](#) Architectural Plans

DA2018/064 - 3 Homebush Road, Strathfield
Lot 20 DP 1101067 (Cont'd)

3 HOMEBUSH ROAD, STRATHFIELD, NSW 2135 PROPOSED BOARDING HOUSE STRATHFIELD COUNCIL: <https://www.strathfield.nsw.gov.au/>

NOTE: CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE BEFORE COMMENCING WORK. IF PREPARING SHOP DRAWINGS, DO NOT SCALE DRAWING.



Drawing No.	Description	Issue / Revision
DA0001	COVER	
DA0001	DA - HEIGHT STUDIES	
DA1001	SITE LOCATION & ANALYSIS	
DA1002	SITE PLAN	
DA1003	DEMOLITION PLAN	
DA2001	BASEMENT 02	
DA2002	BASEMENT 01	
DA2003	GROUND FLOOR PLAN	
DA2004	LEVEL 01	
DA2005	LEVEL 02	
DA2006	LEVEL 03	
DA2007	ROOF PLAN	
DA2008	GFA DIAGRAM	
DA2009	SOLAR ACCESS DIAGRAMS	
DA2010	SOLAR ACCESS - EXISTING	
DA2011	SOLAR ACCESS PROPOSED	
DA2012	ELEVATION SHADOW AT No 5 HOMEBUSH RD	
DA3001	ELEVATIONS	
DA3002	ELEVATIONS & STREETSCAPE	
DA4001	SECTION A - A & RAMP SECTION	
DA6001	COLOUR SCHEDULE	



Rev.	Description	Date	Author
C	DA AMENDED DRAWINGS	20.08.18	
B	DA AMENDED DRAWING	18.07.18	
A	DA SUBMISSION	13.06.18	

3 HOMEBUSH RD STRATHFIELD BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2135



Permitted with consent
Attached dwellings; Bed and breakfast accommodation; Boarding houses; Building identification signs; Business identification signs; Child care centres; Community facilities; Dual occupancies; Dwelling houses; Environmental protection works; Group homes; Home businesses; Multi dwelling housing; Neighbourhood shops; Places of public worship; Recreation areas; Residential care facilities; Residential flat buildings; Respite day care centres; Roads; Secondary dwellings; Semi-detached dwellings; Seniors housing; Water recycling facilities

AREA: 796.7m²

STRATHFIELD COUNCIL
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AMENDED PLAN

Drawing Title
COVER

Date of Issue: 20/08/2018
Checked: JE
Approved: TJ

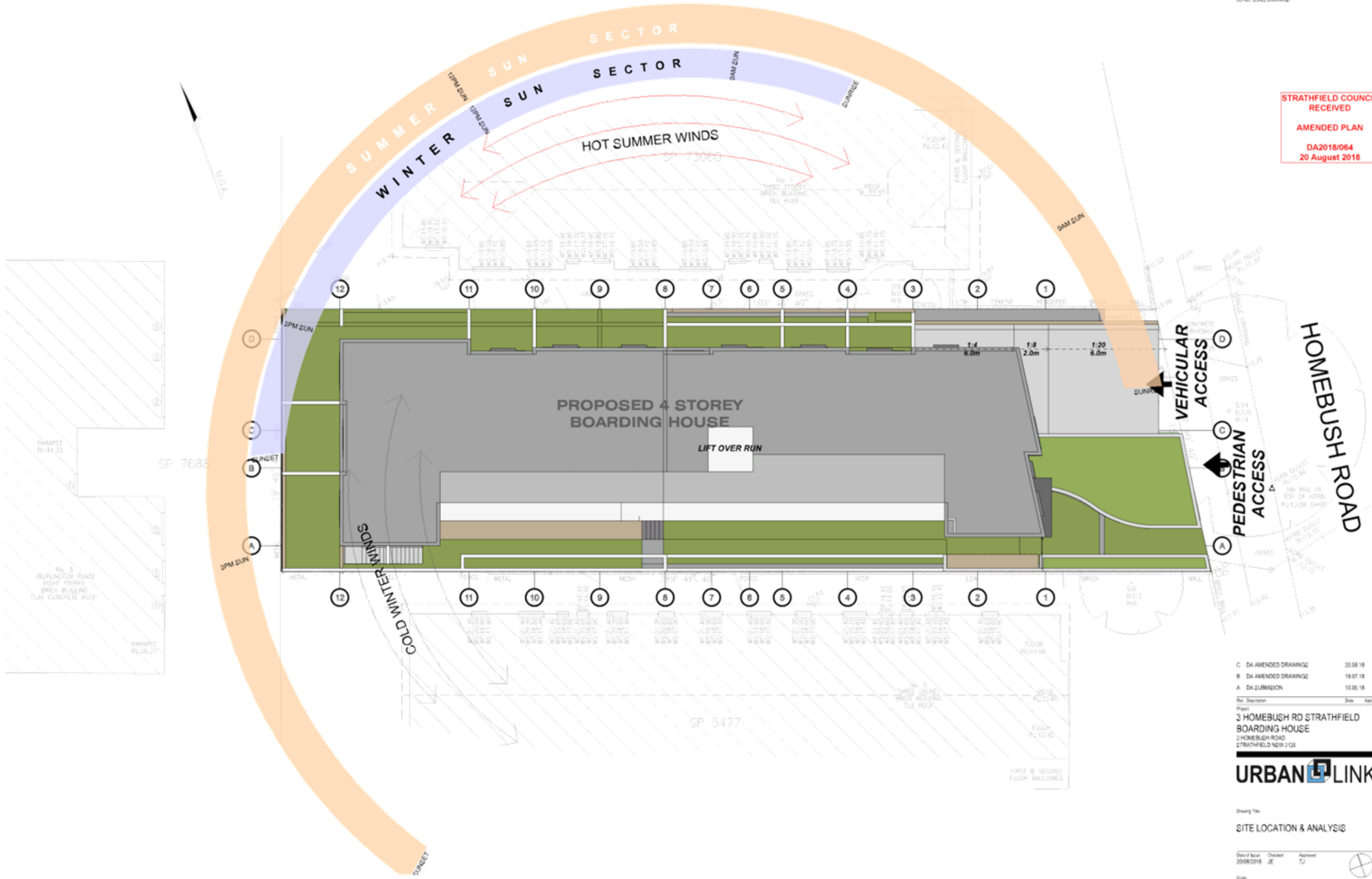
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Project Number: 2017 - 020
Drawing Number: DA0001
Revision: C

STATUS: DEVELOPMENT APPLICATION

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DA2018/064
20 August 2018



C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev. Description Date Assn
 Project:
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2158



Drawing Title:
 SITE LOCATION & ANALYSIS

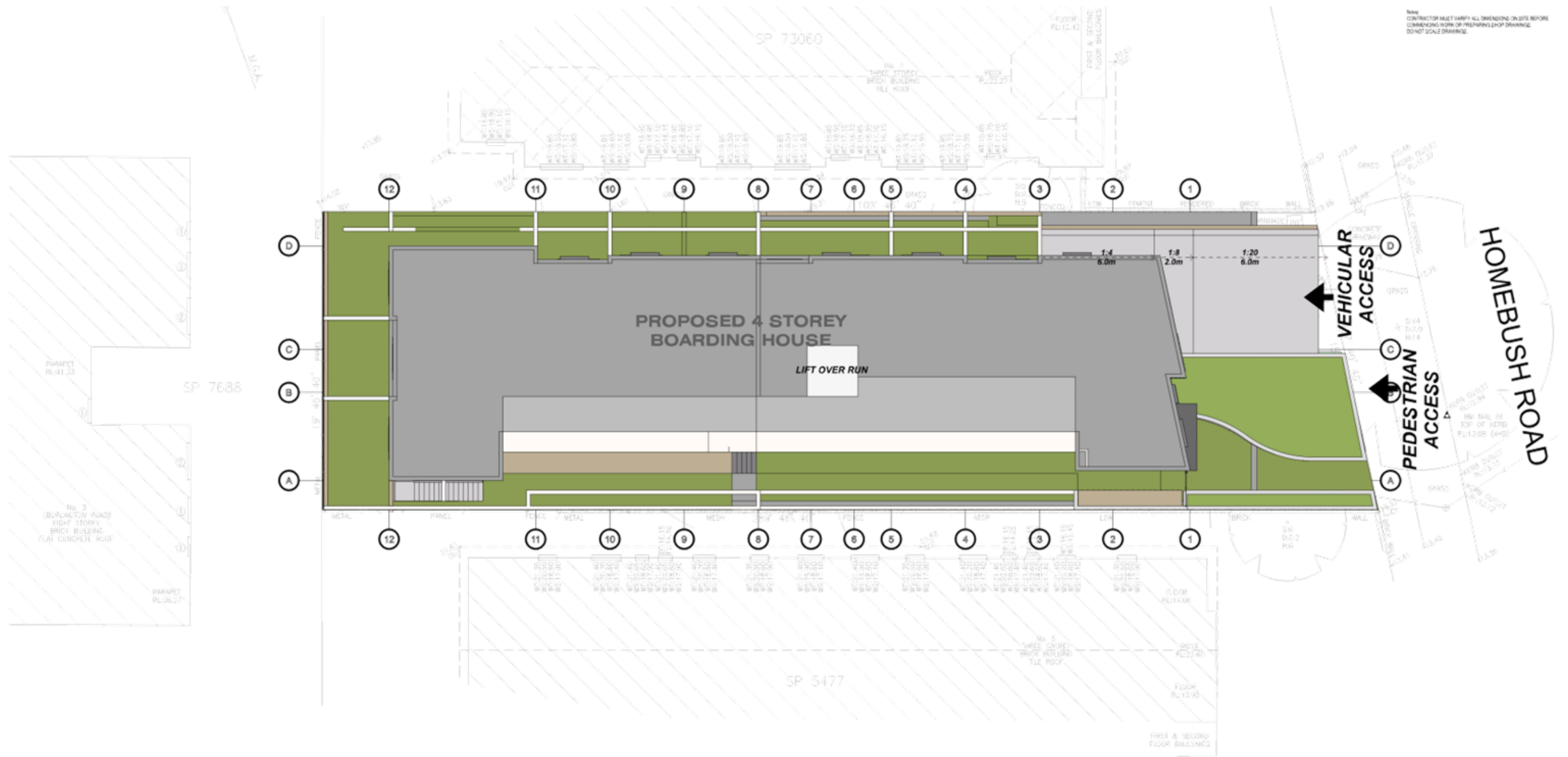
Date of Issue	20/08/2018	Checked	JE	Approved	TJ
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 Project Number: 2017 - 020
 Drawing Number/Revision: DA1001 C
 Date: DEVELOPMENT APPLICATION

1

SITE ANALYSIS

1:100



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1

SITE PLAN

1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev. Description Date Ass'd
Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title
SITE PLAN

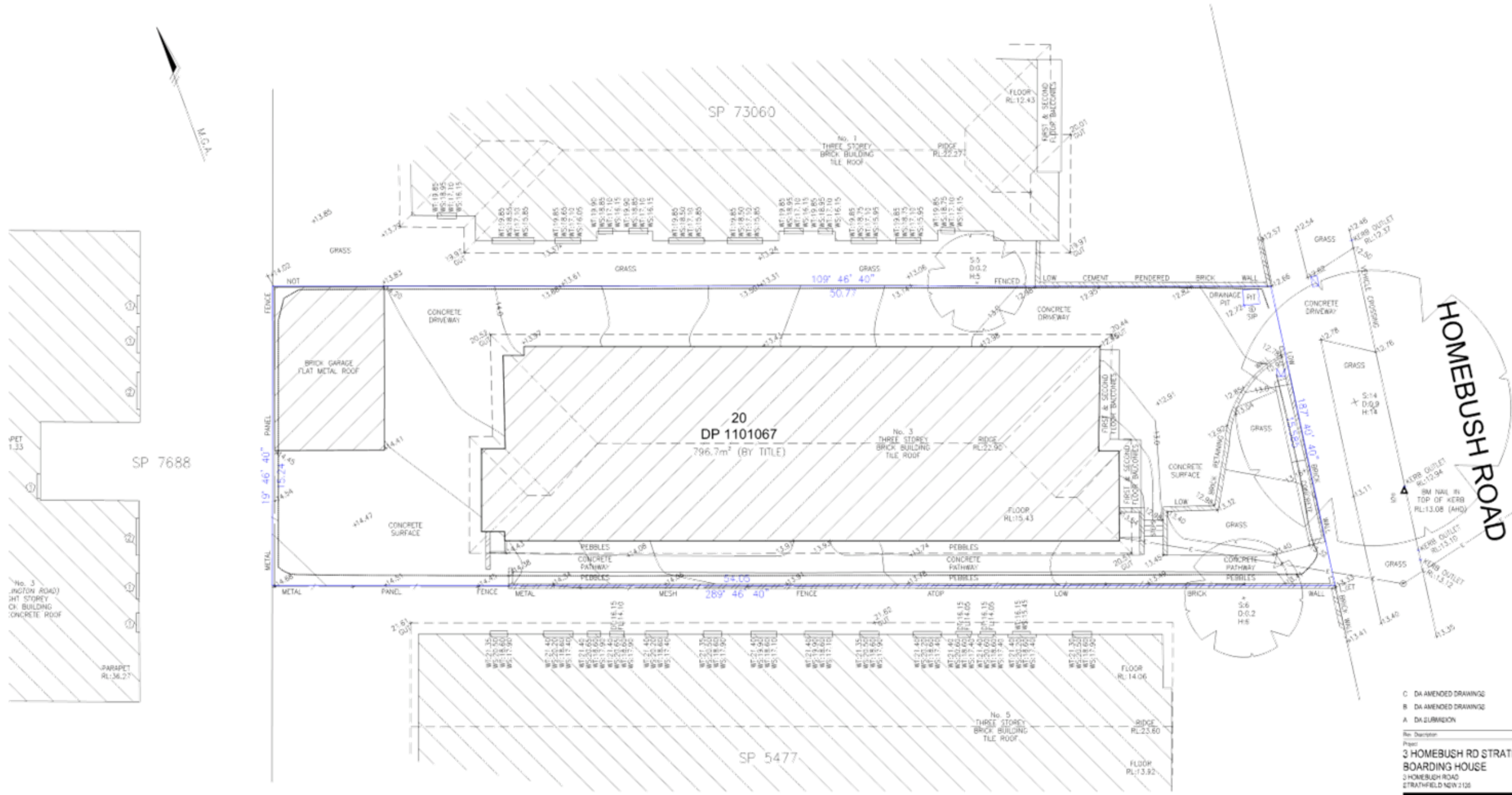
Date of Issue	20/08/2018	Checked	JE	Approved	TJ
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Project Number
2017 - 020
Drawing Number/Revision
DA1002 C
Date:
DEVELOPMENT APPLICATION

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DA2018/064
20 August 2018

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C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWING	19.07.18
A	DA SUBMISSION	10.08.18

Rev. Description Date Ass'd

Project:
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title:
DEMOLITION PLAN

Date of Issue	20/08/2018	Checker	JE	Apprval	TJ
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Scale:
AS SHOWN

Project Number	2017 - 020	Drawing Number	DA1003	Revision	C
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Date:
DEVELOPMENT APPLICATION

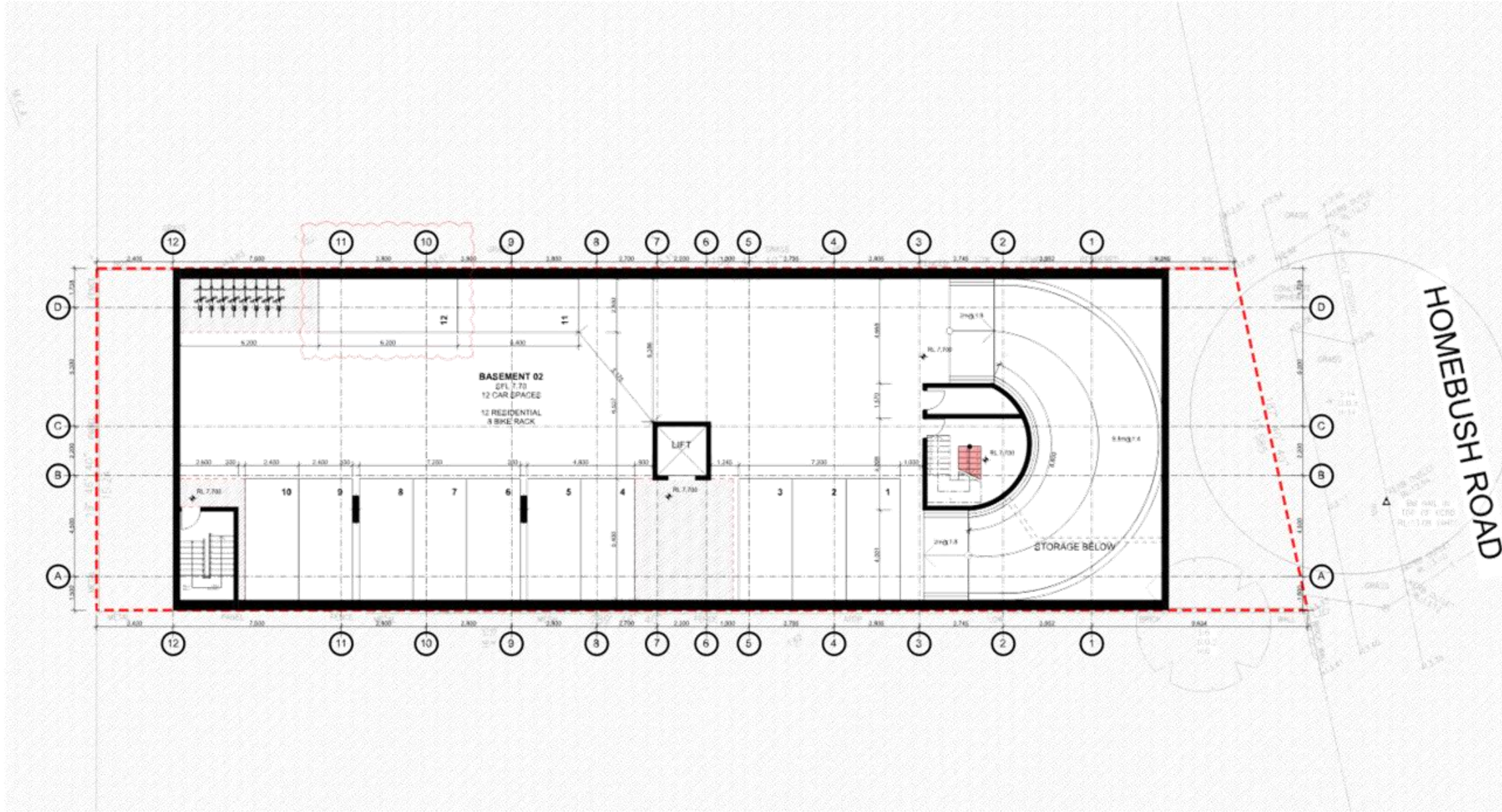
STRATHFIELD COUNCIL
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AMENDED PLAN
DA2018/064
20 August 2018

DEMOLITION PLAN

1:100

1

Note
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BASEMENT 02

1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Project:
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2126



Drawing Title:
BASEMENT 02

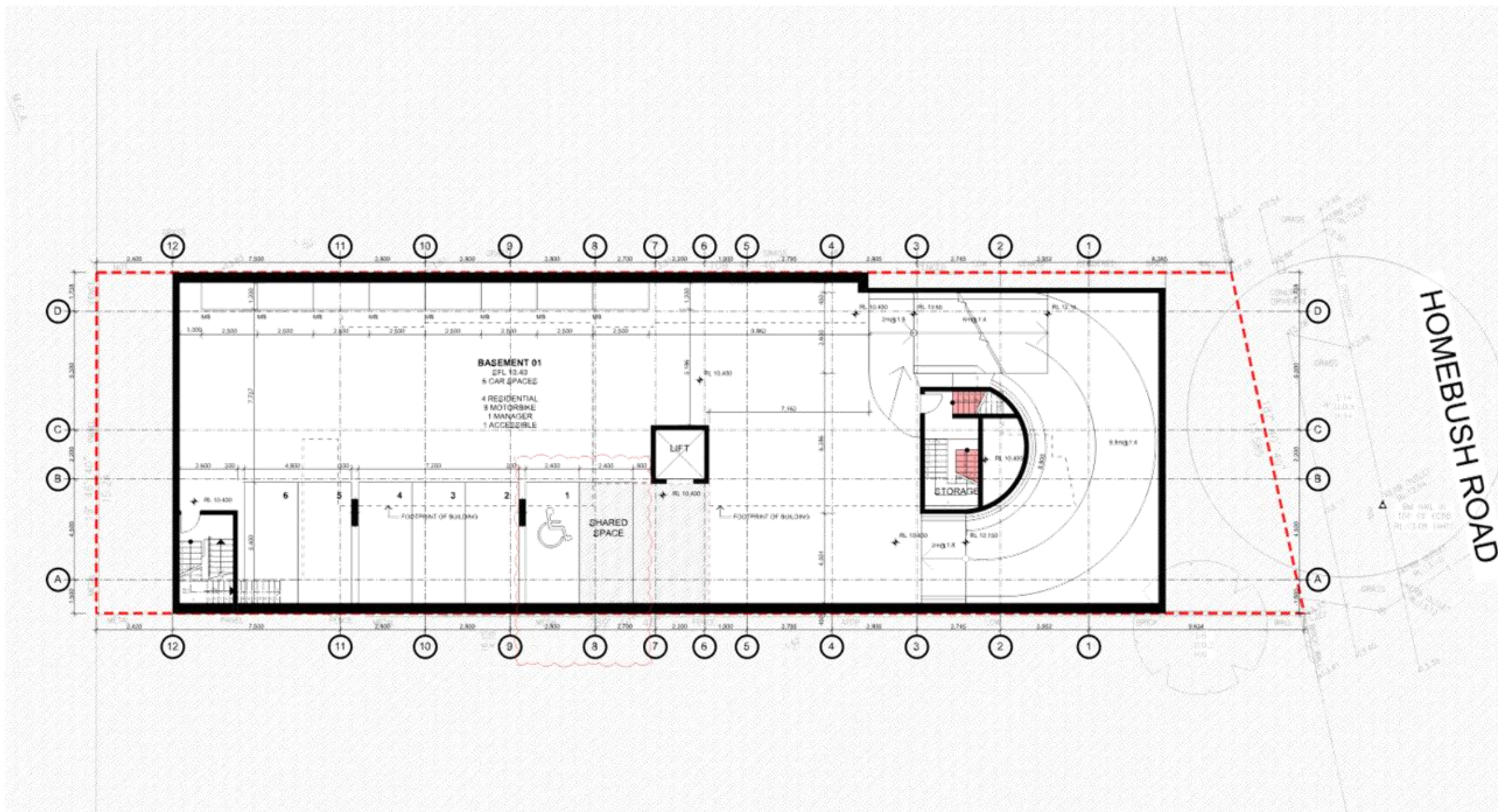
Date of Issue	Checked	Approved
20/08/2018	JE	TJ

Scale:
AS SHOWN

Project Number: 2017 - 020
Drawing Number: DA2001 C
Date: DEVELOPMENT APPLICATION

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DA2018/064
20 August 2018

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BASEMENT 01

1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.05.18

Rev. Description Date Ass'd

3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2126



Drawing Title
BASEMENT 01

Date of Issue 20/08/2018 Checker JE Approver TJ

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Project Number 2017 - 020 Drawing Number Revision DA2002 C

Date DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
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DA2018/064

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GROUND FLOOR PLAN

1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	18.07.18
A	DA SUBMISSION	13.06.18

Rev: Description Date Assn
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD, NSW 2158



Drawing Title
GROUND FLOOR PLAN

Date of Issue	20/08/2018	Checker	JE	Assessor	TJ
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2017 - 020

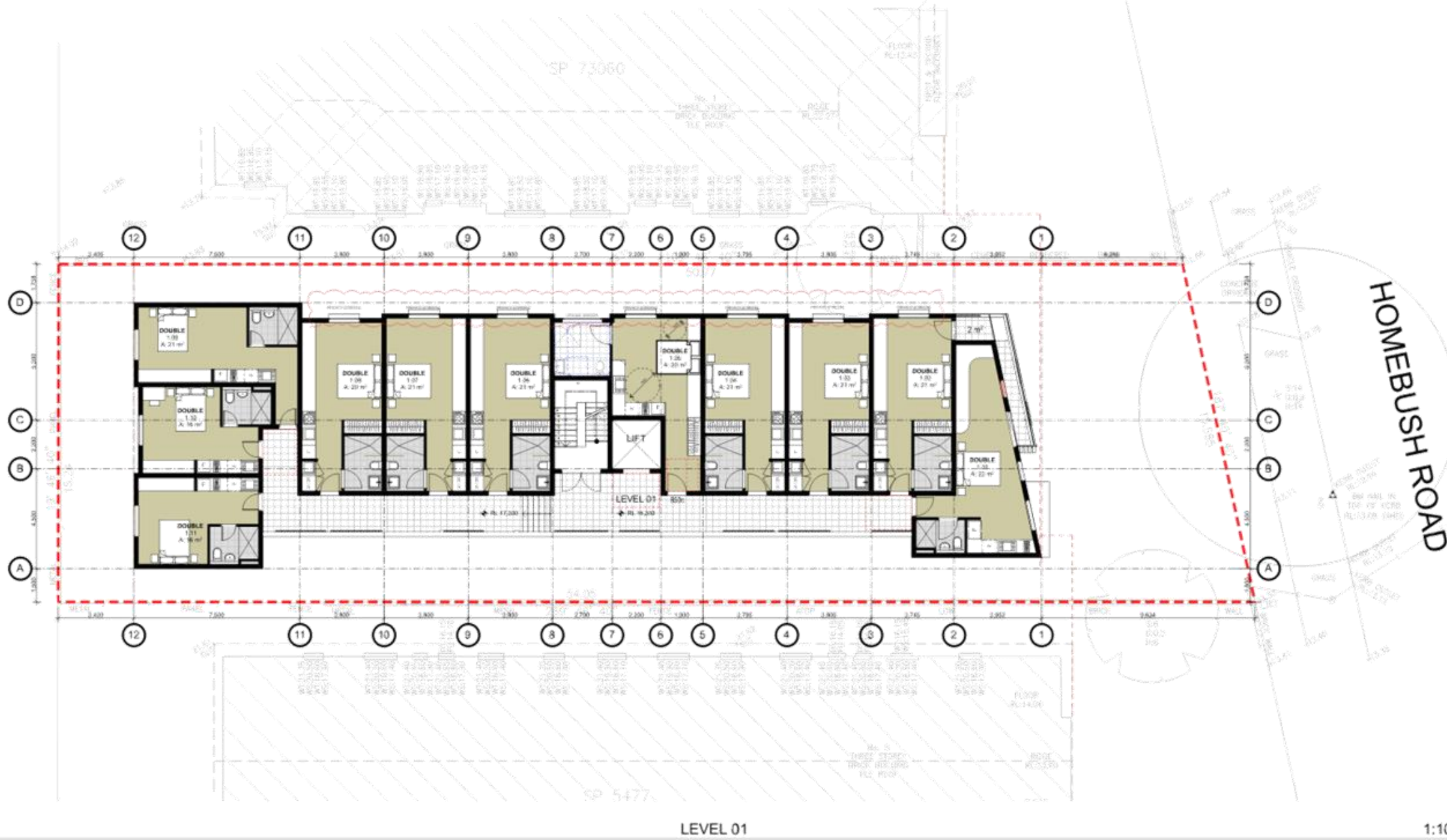
Drawing Number
DA2003

Revision
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Date
DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
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AMENDED PLAN
DA2018/064
20 August 2018

Note
CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE BEFORE
COMMENCING WORK OR PREPARING SHOP DRAWINGS
DO NOT SCALE DRAWING



LEVEL 01

1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev: Description Date Assn
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2128



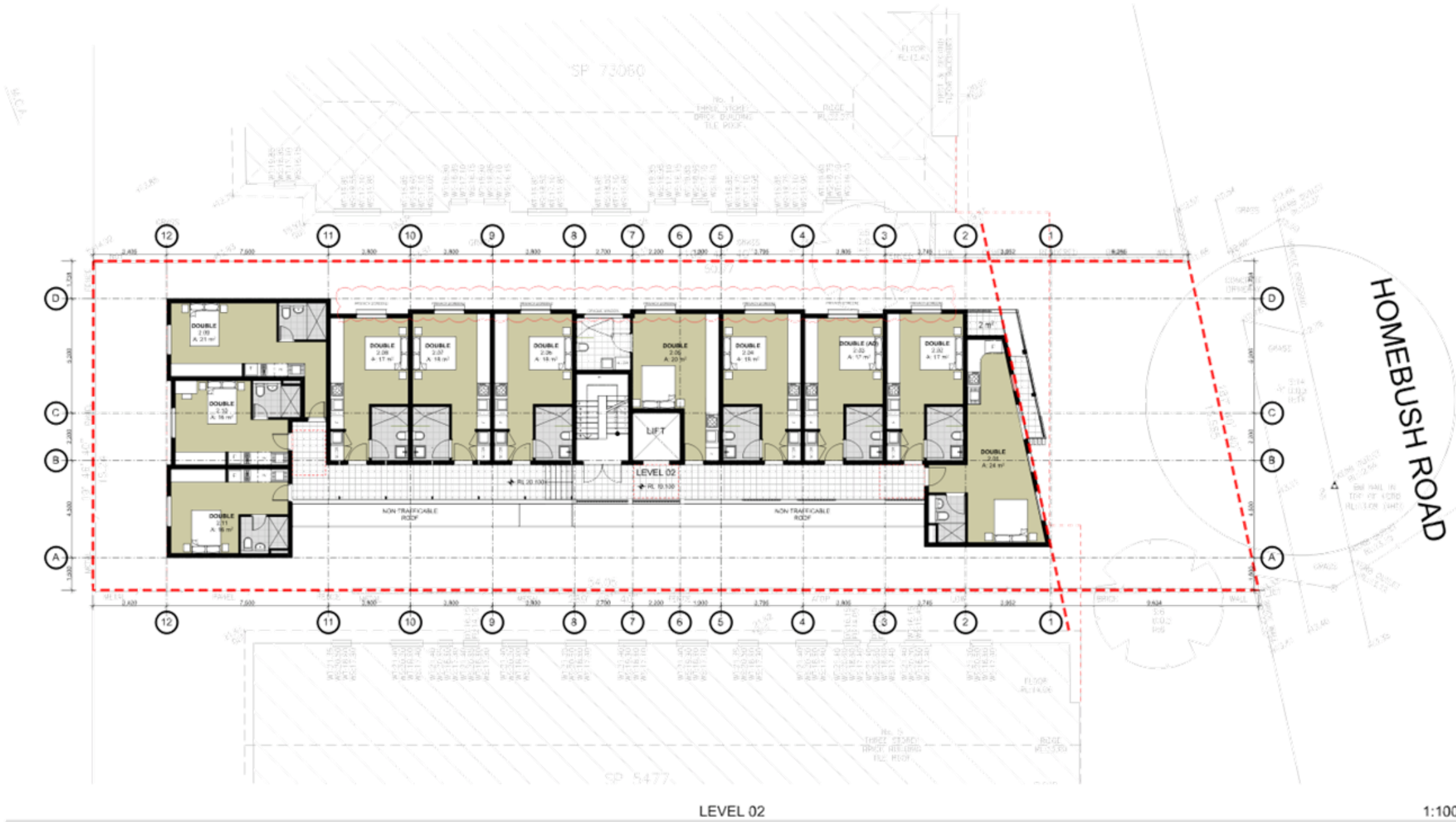
Drawing Title
 LEVEL 01

Date of Issue	20/08/2018	Checker	JE	Apprval	TJ
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Scale
 AS SHOWN
 Project Number
 2017 - 020
 Drawing Number
 DA2004 C
 Date
 DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
 RECEIVED
 AMENDED PLAN
 DA2018/064
 20 August 2018

Note:
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COMMENCING WORK OR PREPARING SHOP DRAWINGS.
DO NOT SCALE DRAWING.



LEVEL 02

1:100

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	10.06.18

Project:
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



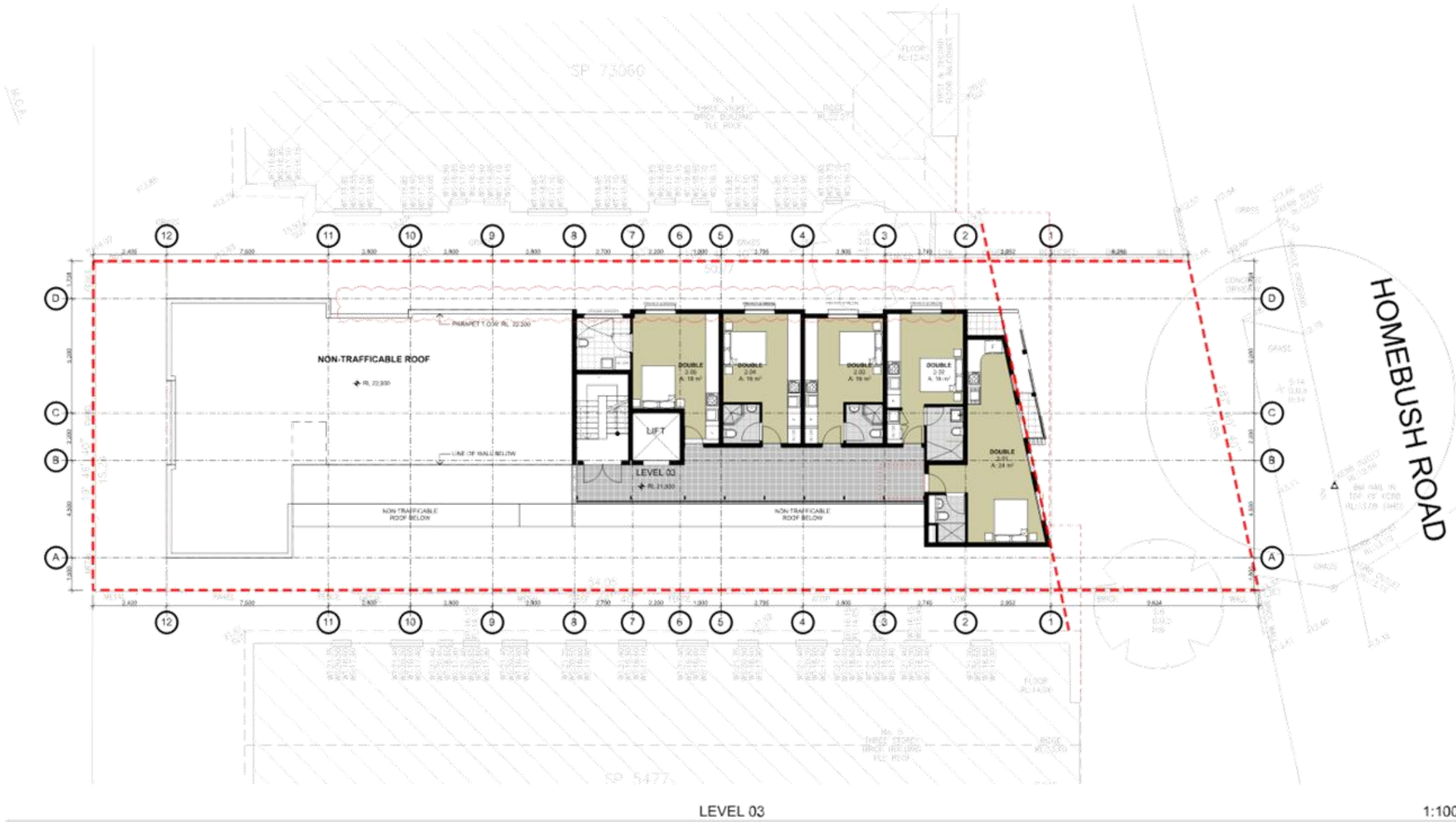
Drawing Title:
LEVEL 02

Date of Issue	20/08/2018	Checked	JE	Approved	TJ
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Scale:
AS SHOWN

Project Number: 2017 - 020
Drawing Number/Revision: DA2005 - C
Date: DEVELOPMENT APPLICATION

Note
CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE BEFORE
COMMENCING WORK OR PREPARING SHOP DRAWINGS
DO NOT SCALE DRAWING



C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev: Description Date Assn
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2158



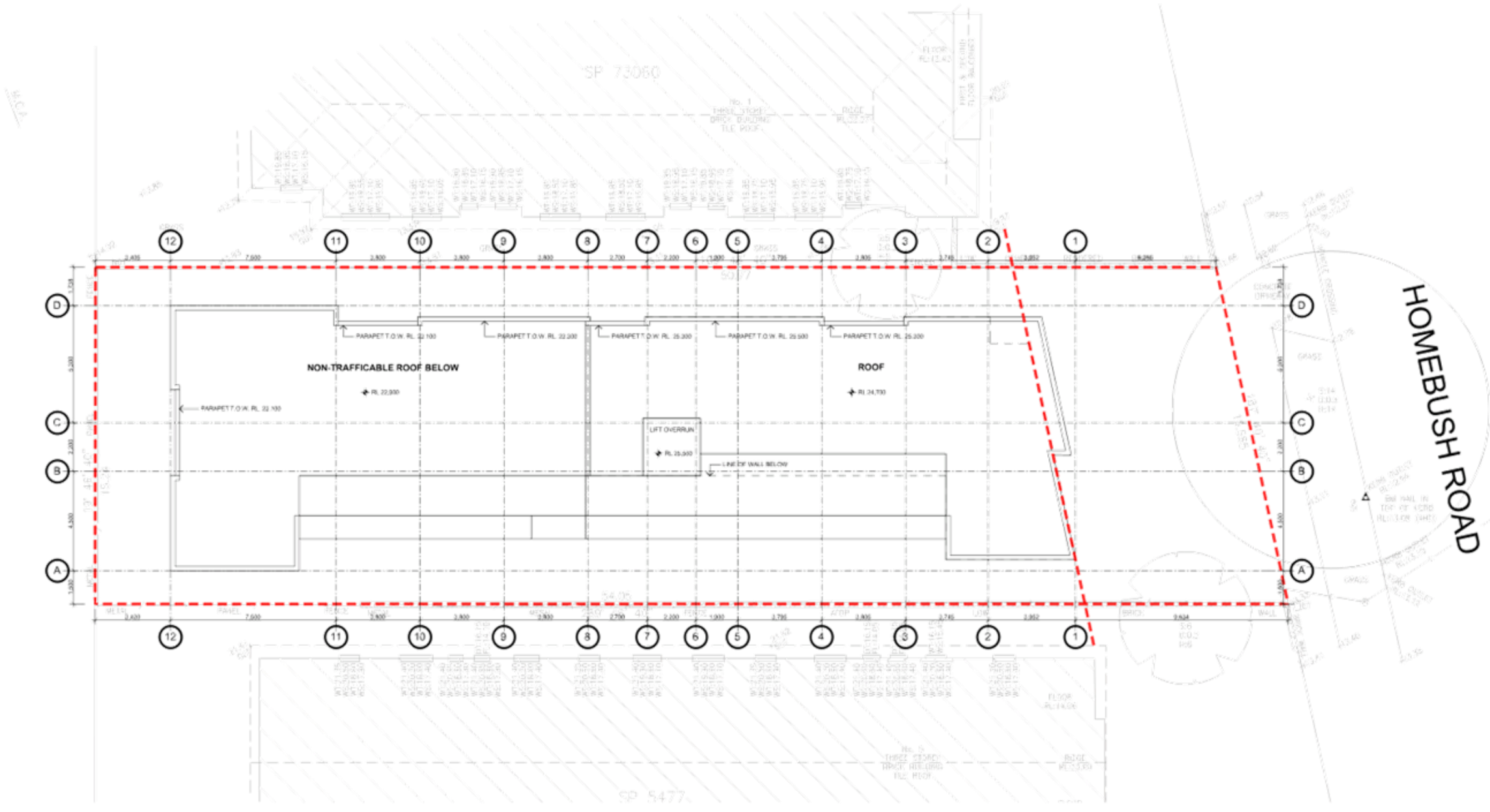
Drawing Title
 LEVEL 03

Date of Issue	Checker	Assessor
20/08/2018	JE	TJ

Scale
 AS SHOWN
 Project Number
 2017 - 020
 Drawing Number/Revision
 DA2005 - C
 Date
 DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
 RECEIVED
 AMENDED PLAN
 DA2018/064
 20 August 2018

NOTE
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COMMENCING WORK OR PREPARING SHOP DRAWINGS.
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ROOF PLAN

1:100

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	10.06.18

Rev Description Date Assn

Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title
ROOF PLAN

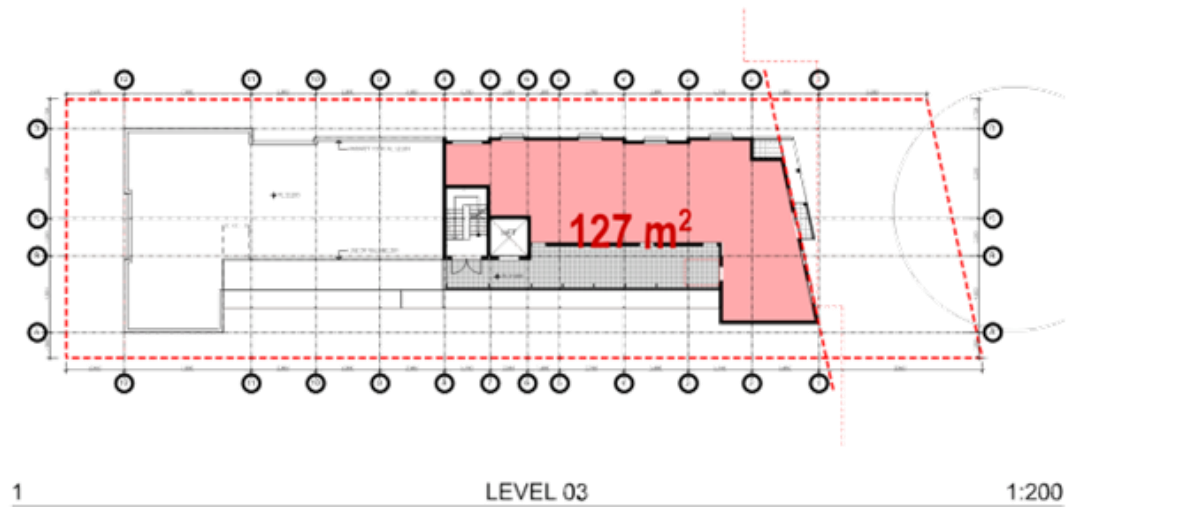
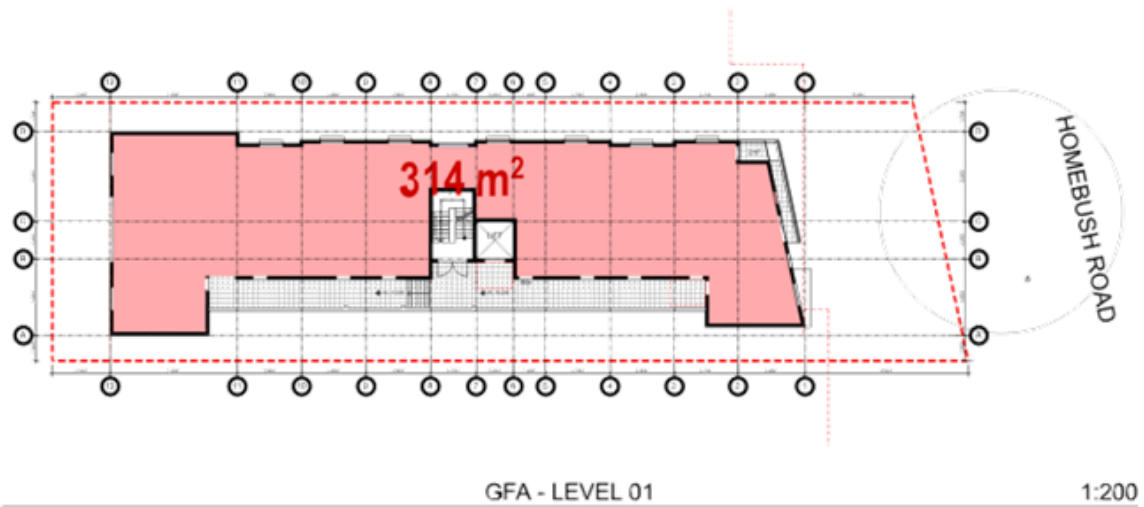
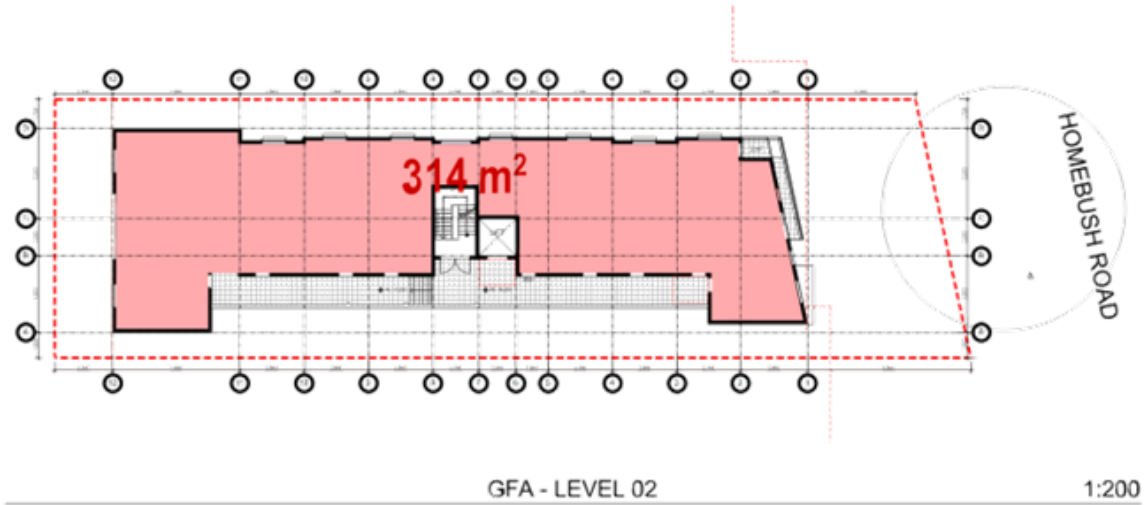
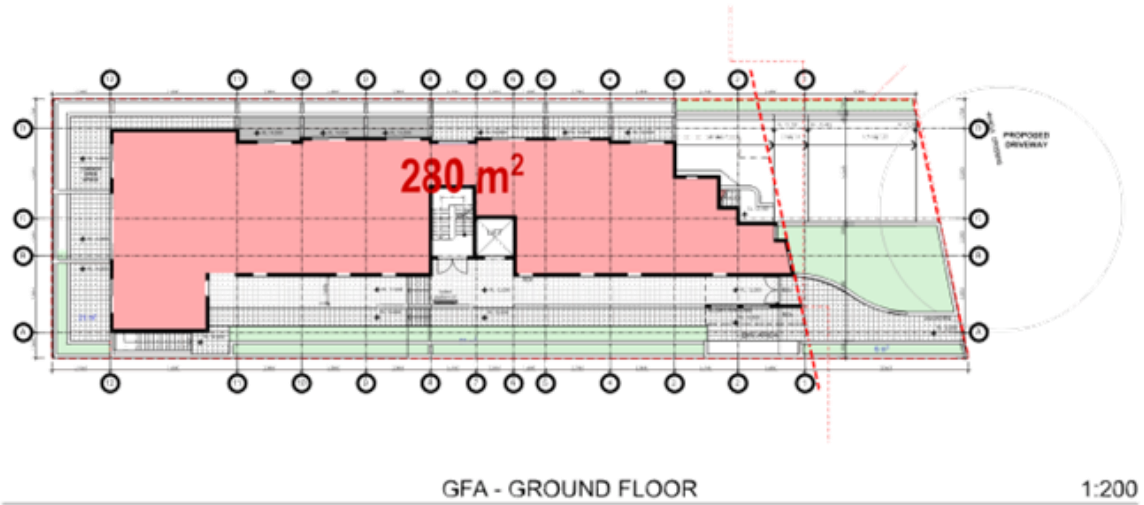
Date of Issue	20/08/2018	Checked	JE	Approved	TJ
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Scale
AS SHOWN

Project Number	2017 - 020	Drawing Number	Roofplan
Date	20/08/2018	Revision	DA2005 C

DEVELOPMENT APPLICATION

NOTE
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DO NOT SCALE DRAWINGS.



CALCULATIONS MATRIX					
LEVEL	ROOM No	No OF LODGERS	INTERNAL GFA (EXCL KIT & BATH)	INTERNAL GFA (INCL KIT & BATH)	ACCESSIBLE
			m²	m²	
GROUND	G 01	2	19.00	25.00	NO
	G 02	2	21.00	27.00	NO
	G 03	2	21.00	28.00	NO
	G 04	2	20.00	29.00	ACCESSIBLE
	G 05	2	21.00	28.00	NO
	G 06	2	21.00	28.00	NO
	G 07	2	20.00	27.00	NO
	G 08	2	20.00	27.00	NO
	G 09	2	18.00	22.00	NO
LEVEL 1	1 01	2	22.00	29.00	NO
	1 02	2	21.00	27.00	NO
	1 03	2	21.00	27.00	NO
	1 04	2	21.00	28.00	NO
	1 05	2	20.00	29.00	ACCESSIBLE
	1 06	2	21.00	28.00	NO
	1 07	2	21.00	28.00	NO
	1 08	2	20.00	27.00	NO
	1 09	2	20.00	27.00	NO
	1 10	2	18.00	22.00	NO
	1 11	2	18.00	21.00	NO
LEVEL 2	2 01	2	25.00	31.00	NO
	2 02	2	17.00	24.00	NO
	2 03	2	17.00	24.00	NO
	2 04	2	18.00	24.00	NO
	2 05	2	20.00	28.00	NO
	2 06	2	18.00	24.00	NO
	2 07	2	18.00	24.00	NO
	2 08	2	17.00	24.00	NO
	2 09	2	20.00	27.00	NO
	2 10	2	18.00	22.00	NO
2 11	2	18.00	21.00	NO	
LEVEL 3	3 01	2	25.00	31.00	NO
	3 02	2	17.00	24.00	NO
	3 03	2	17.00	24.00	NO
	3 04	2	18.00	24.00	NO
	3 05	2	20.00	28.00	NO
TOTAL	36	72			

TOTAL SITE AREA			
		796.7m²	
CONTROL	MAX ALLOWABLE	PROPOSED	COMPLIES
FLOOR SPACE RATIO (FSR)	1.45 + 0.5 - 1.95:1	1.29:1	YES
GROSS FLOOR AREA (GFA)	1,553.565m²	1035 m²	YES
BUILDING HEIGHT	14m	>14m	YES
CONTROL	MIN. REQUIRED	PROPOSED	COMPLIES
COMMON OPEN SPACE	20.00m2	21m2	YES
LANDSCAPE		10.9% = 87m2	
CARPARKING	18 SPACES	18 SPACES	YES
ADAPTABLE UNITS	2 Units	G 04, 1 05	YES

GFA CALCULATION			
ROOMS			
LEVEL	AREA	ROOMS	BEDS
G	280 m²	9	18
1	314 m²	11	22
2	314 m²	11	22
3	127 m²	5	10
TOTAL	1035 m²	36	72

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C DA AMENDED DRAWINGS 20.08.18
B DA AMENDED DRAWINGS 19.07.18
A DA SUBMISSION 13.08.18

Project:
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



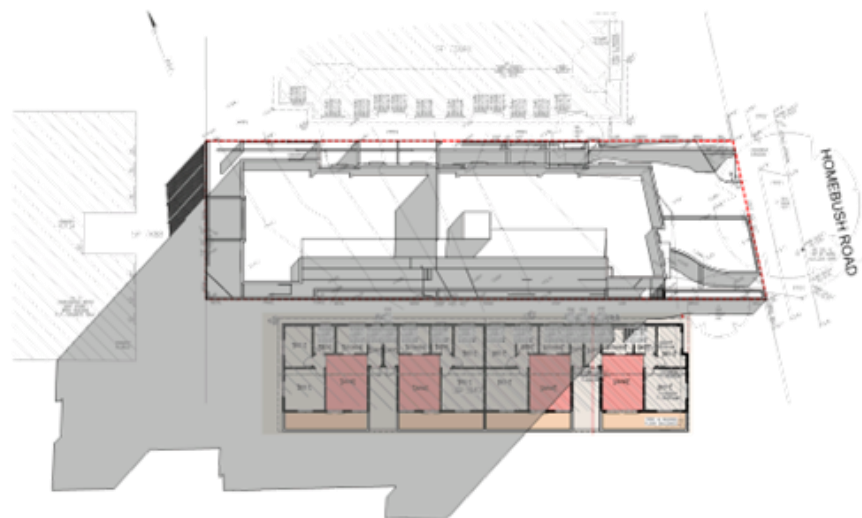
Drawing Title:
GFA DIAGRAMS

Date of Issue: 20/08/2018
Checked: JE
Approved: TJ

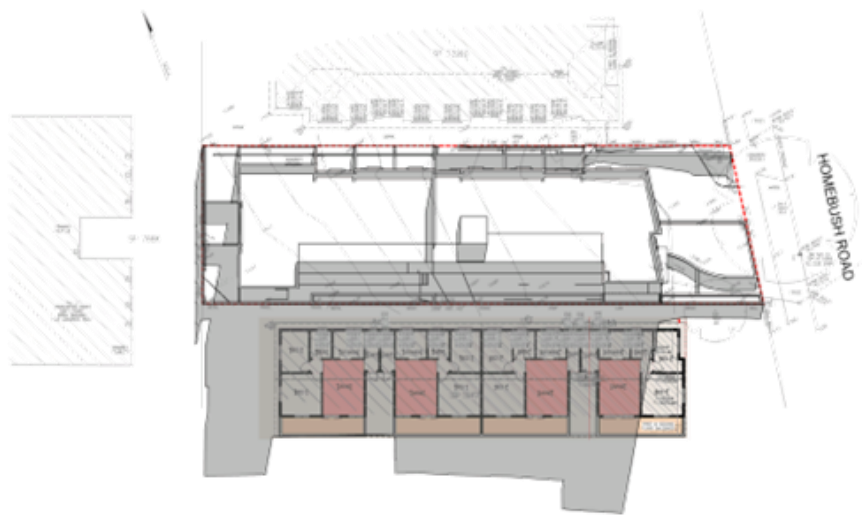
Scale:
AS SHOWN

Project Number: 2017 - 020
Drawing Number: DA2901 C
Date:
DEVELOPMENT APPLICATION

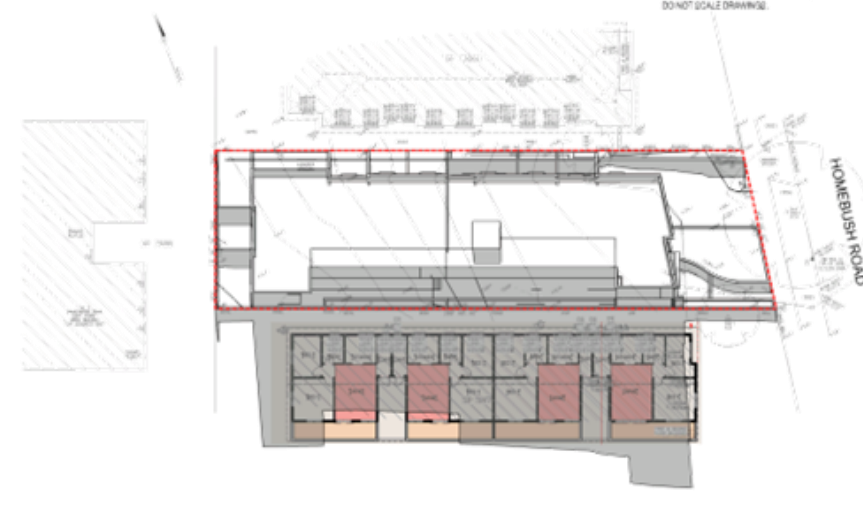
Note
CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE BEFORE
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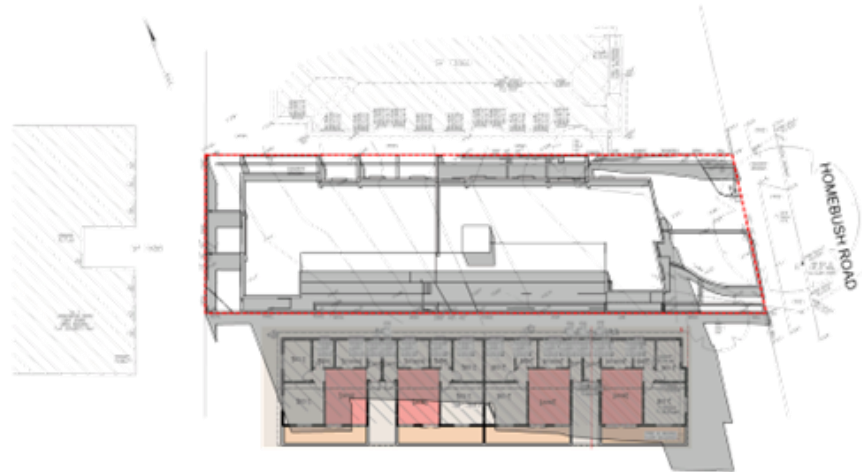
JUNE 21ST 9AM



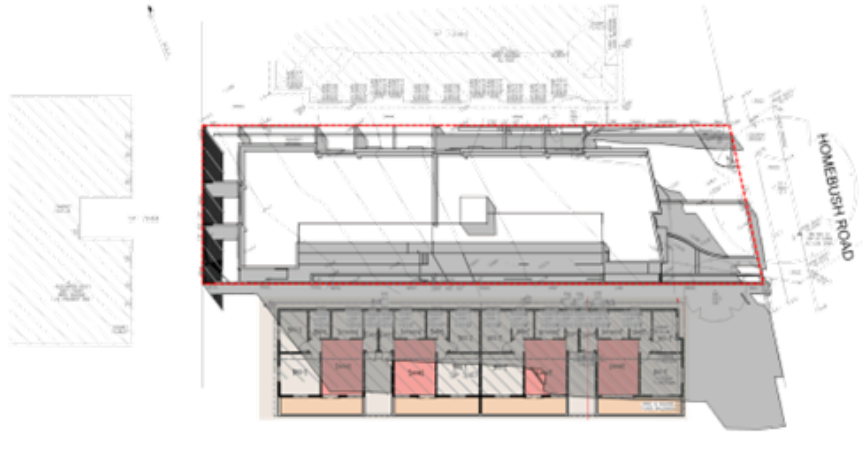
JUNE 21ST 10AM



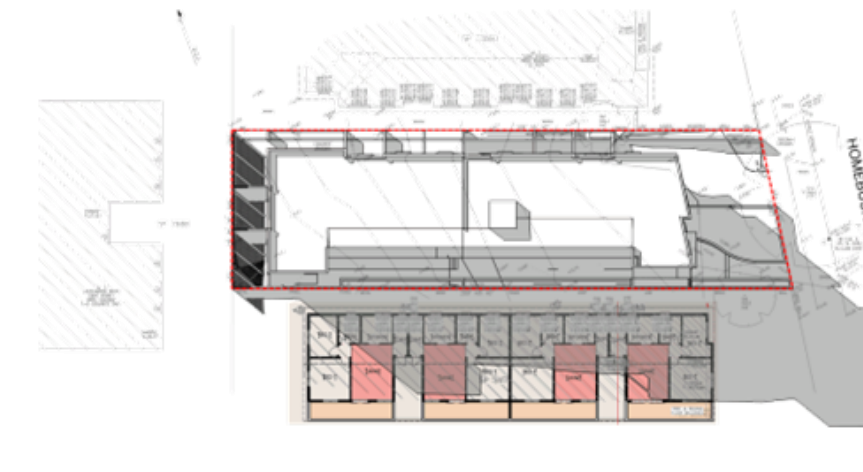
JUNE 21ST 11AM



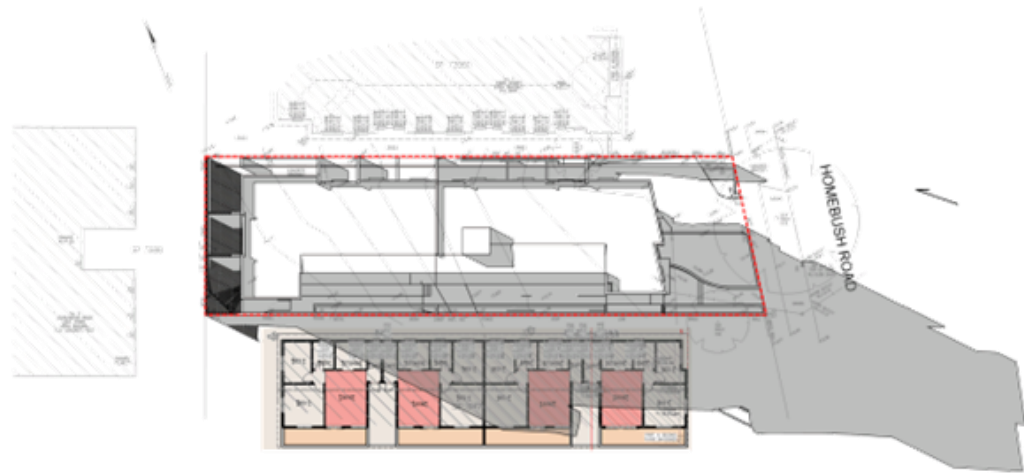
JUNE 21ST 12PM



JUNE 21ST 01PM



JUNE 21ST 02PM



JUNE 21ST 02PM

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

LEGEND

- PROPOSED OUTLINE OF SHADOW DIAGRAM
- EXISTING LOCATION OF BALCONIES
- EXISTING LOCATIONS OF LIVING AREAS

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title
SOLAR ACCESS DIAGRAMS

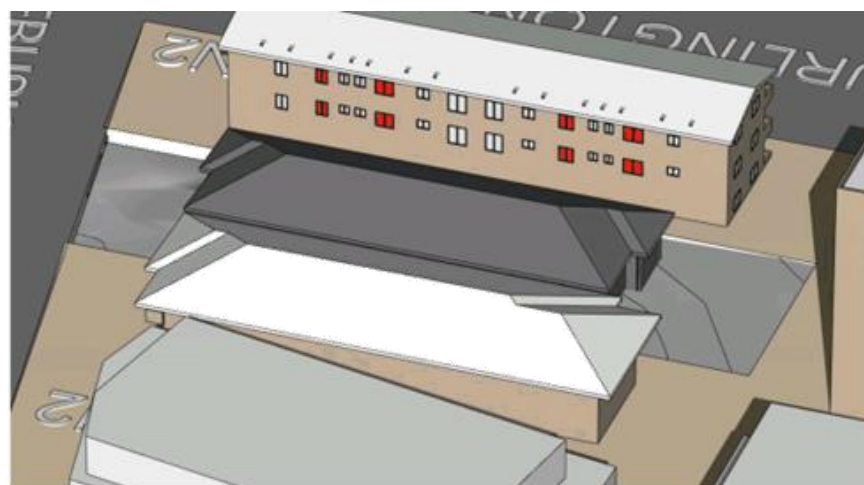
Date of Issue	Checker	Apprval
20/08/2018	JE	TJ

Scale
AS SHOWN

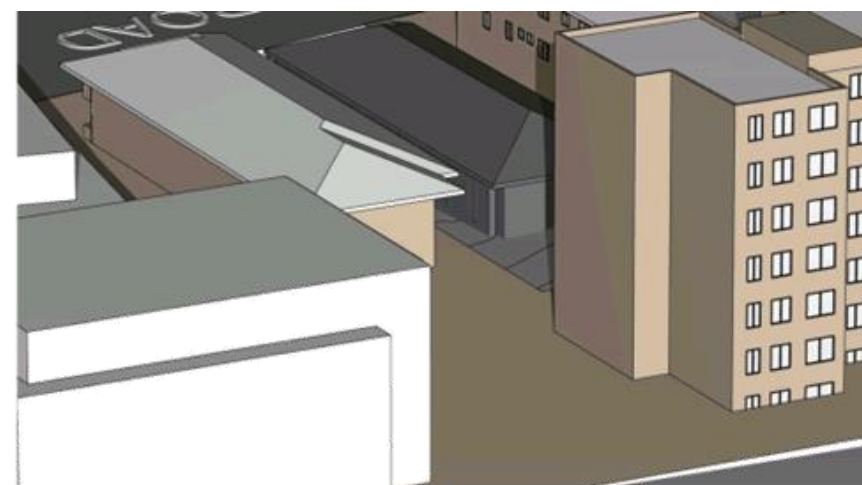
Project Number
2017 - 020
Drawing Number/Revision
DA2902 - C
Date:
DEVELOPMENT APPLICATION



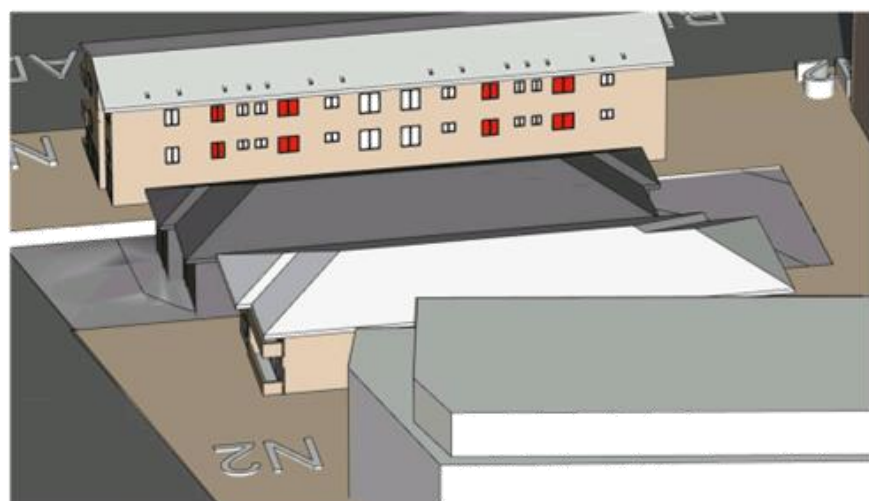
1 VIEW FROM SUN 9 AM - JUNE 21st



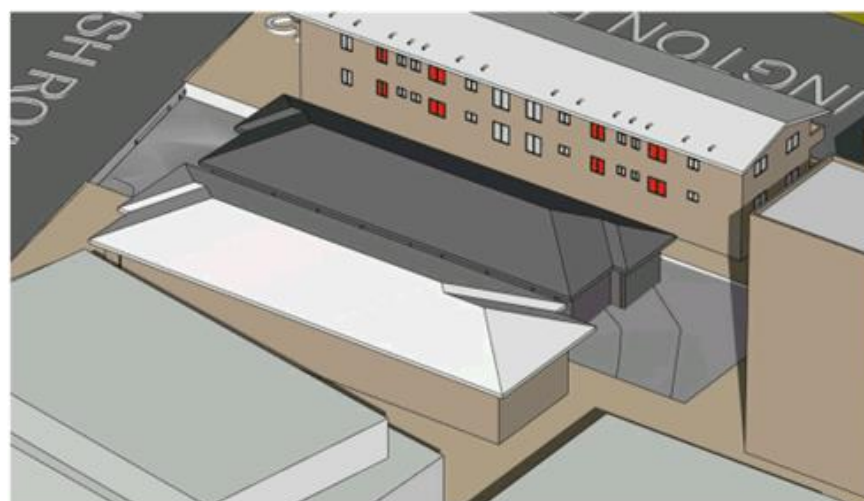
1 VIEW FROM SUN 12 PM - JUNE 21st



1 VIEW FROM SUN 3 PM - JUNE 21st



1 VIEW FROM SUN 10 AM - JUNE 21st



1 VIEW FROM SUN 1 PM - JUNE 21st



1 VIEW FROM SUN 11 AM - JUNE 21st



1 VIEW FROM SUN 2 PM - JUNE 21st

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.05.18

Preparer: _____ Date: _____
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2128

URBAN LINK

Drawing Title:
SOLAR ACCESS - EXISTING

Date of Issue	Checker	Approved
20/08/2018	JE	TJ

Scale:
 AS SHOWN

Project Number	Drawing Number	Revision
2017 - 020	DA2903	C

DATE: DEVELOPMENT APPLICATION



1 VIEW FROM SUN 9 AM - JUNE 21st



1 VIEW FROM SUN 12 PM - JUNE 21st



1 VIEW FROM SUN 3 PM - JUNE 21st



1 VIEW FROM SUN 10 AM - JUNE 21st



1 VIEW FROM SUN 1 PM - JUNE 21st



1 VIEW FROM SUN 11 AM - JUNE 21st



1 VIEW FROM SUN 2 PM - JUNE 21st

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.05.18

Rev: Description Date: Asst:

3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2158



Drawing Title
SOLAR ACCESS PROPOSED

Date of Issue	Checker	Approved	
20/08/2018	JE	TJ	

Scale
AS SHOWN

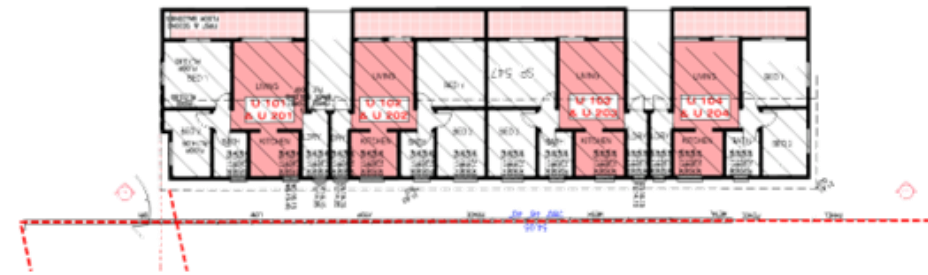
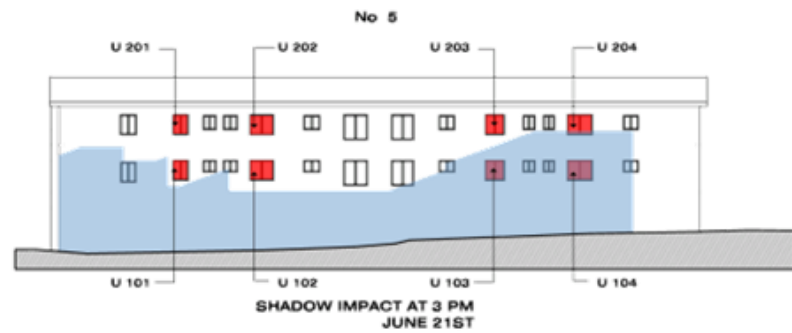
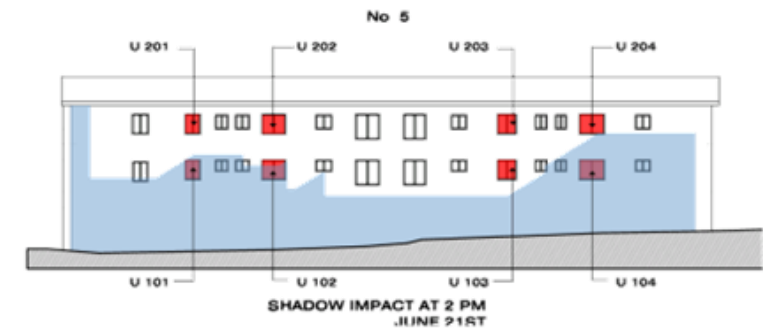
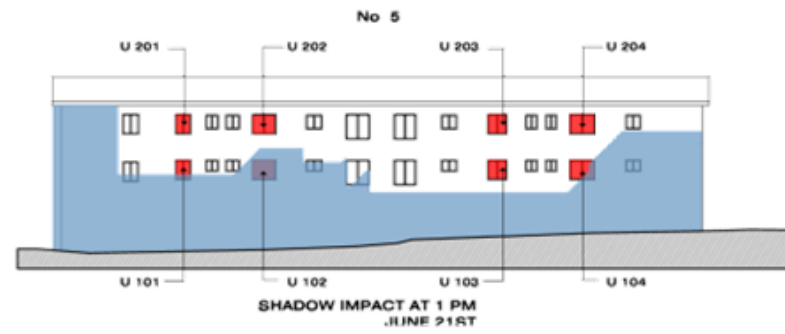
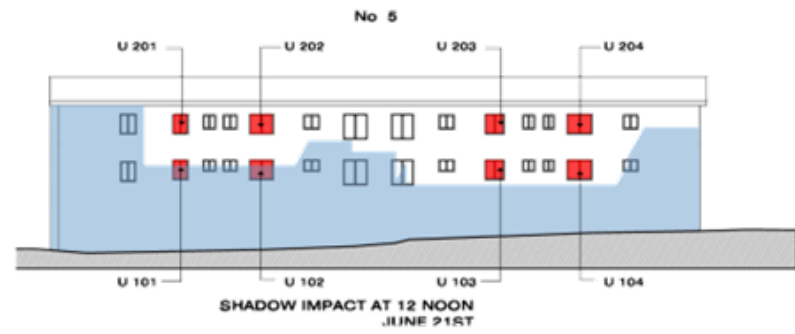
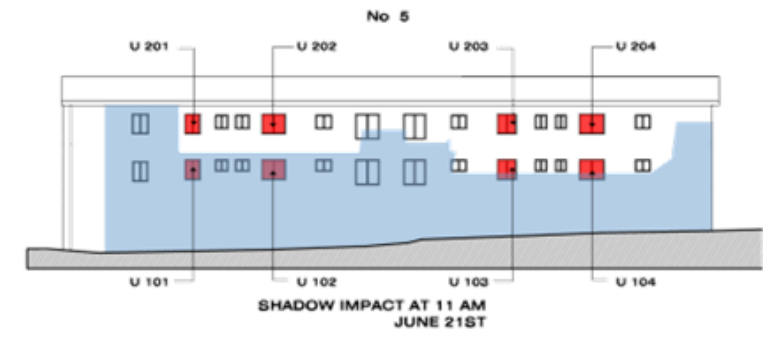
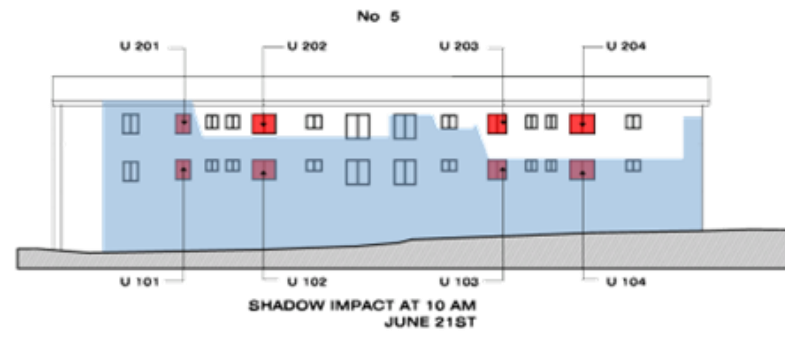
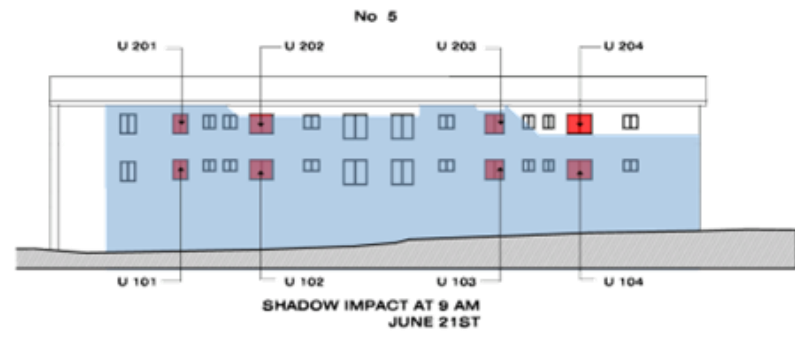
Project Number
2017 - 020

Drawing Number/Revision
DA2904 C

Date:
DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

NOTE
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STRATHFIELD COUNCIL
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DA2018/064
20 August 2018



S O L A R A C C E S S							
U N I T N o	9-10AM	10-11AM	11-12AM	12-01PM	01-02 PM	02-03PM	TOTAL
UNIT 101			0.5	1	0.5	0.5	2.5
UNIT 102			0.5		0.5	1	2
UNIT 103		0.5	1	1	0.5		3
UNIT 104			0.5	1	0.5		2
UNIT 201		0.5	1	1	1	1	4.5
UNIT 202	1	1	1	1	1	1	6
UNIT 203	0.5	1	1	1	1	1	5.5
UNIT 204	1	1	1	1	1	1	6

C DA AMENDED DRAWINGS	20.08.18
B DA AMENDED DRAWINGS	19.07.18
A DA SUBMISSION	13.06.18

Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Subject
ELEVATION SHADOW AT No 5
HOMEBUSH RD

Date of Issue
20/08/2018

Checked
JE

Approved
TJ

Scale
AS SHOWN

Project Number
2017 - 020

Drawing Number/Revision
DA2905 C

Date
DEVELOPMENT APPLICATION

NOTE
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STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Project:
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128

URBAN LINK

Drawing Title:
ELEVATIONS

Date of Issue	Checker	Approved
20/08/2018	JE	TJ

Scale:
AS SHOWN

Project Number: 2017 - 020
Drawing Number/Revision: DA3001 C
Date: DEVELOPMENT APPLICATION

NOTE
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1 East Elevation 1:100



1 West Elevation 1:100

STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018



1 Streetscape 1:100

C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Prepared by: [Signature]
3 HOME BUSH RD STRATHFIELD
BOARDING HOUSE
3 HOME BUSH ROAD
STRATHFIELD NSW 2158

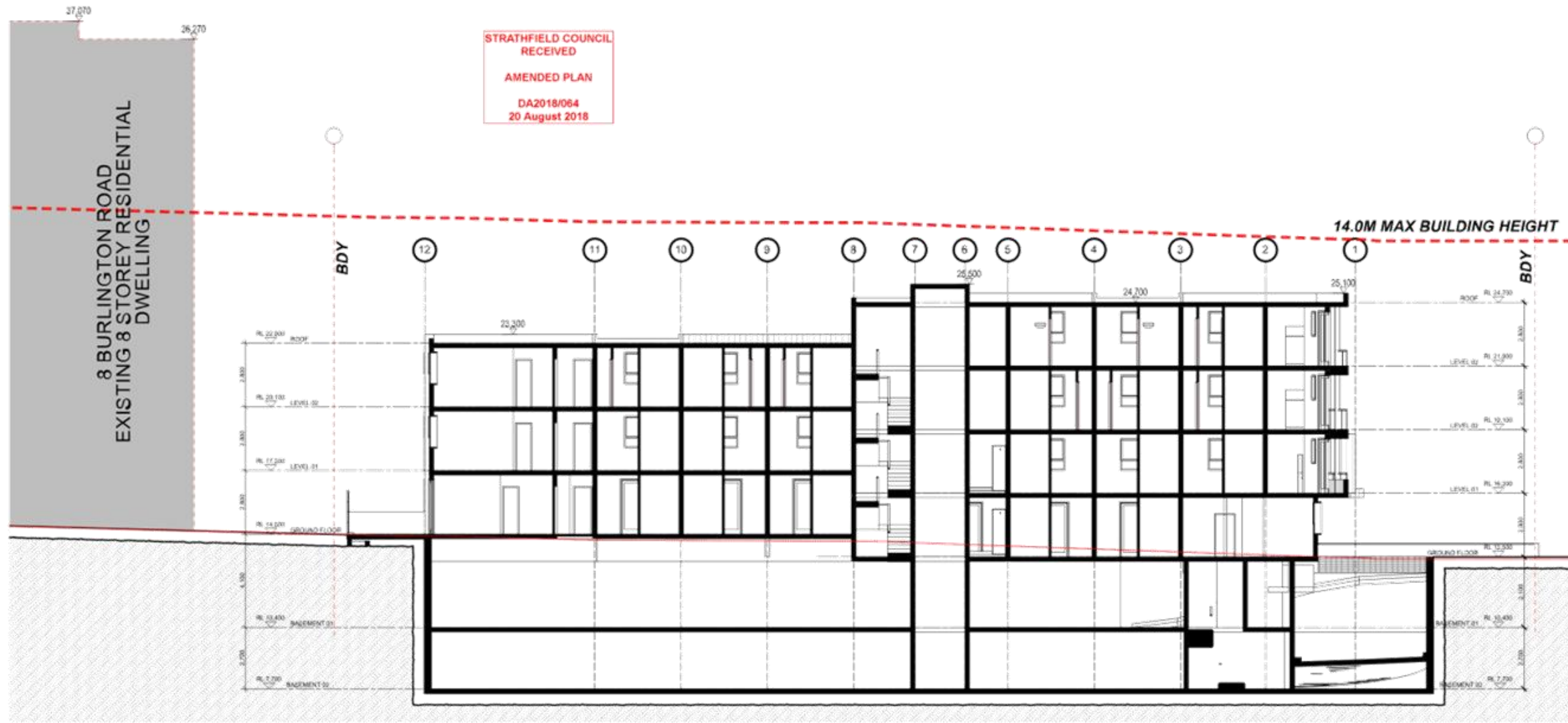


Drawing Title
ELEVATIONS & STREETScape

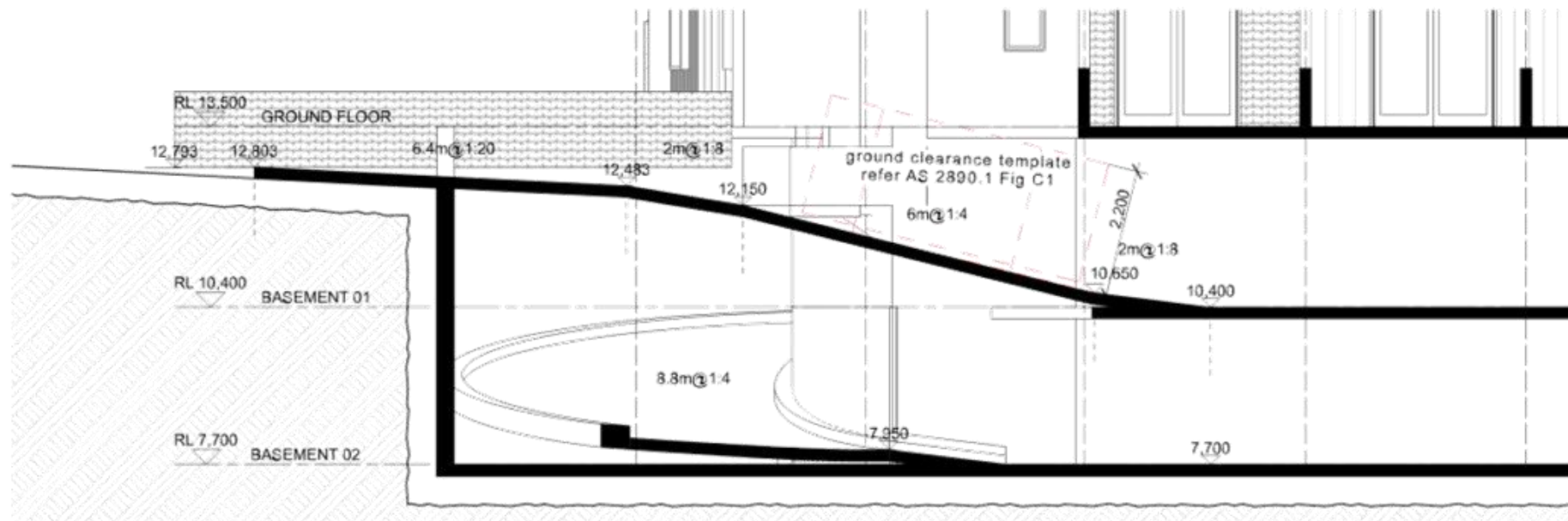
Date of Issue	20/08/2018	Checker	JE	Apprval	TJ
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Scale
AS SHOWN

Project Number
2017 - 020
Drawing Number
DA3002 C
Date
DEVELOPMENT APPLICATION



1 SECTION A - A 1:100



1 RAMP SECTION 1:50

NOTE
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C	DA AMENDED DRAWINGS	20.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev. Description Date Assn

Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title
SECTION A - A & RAMP SECTION

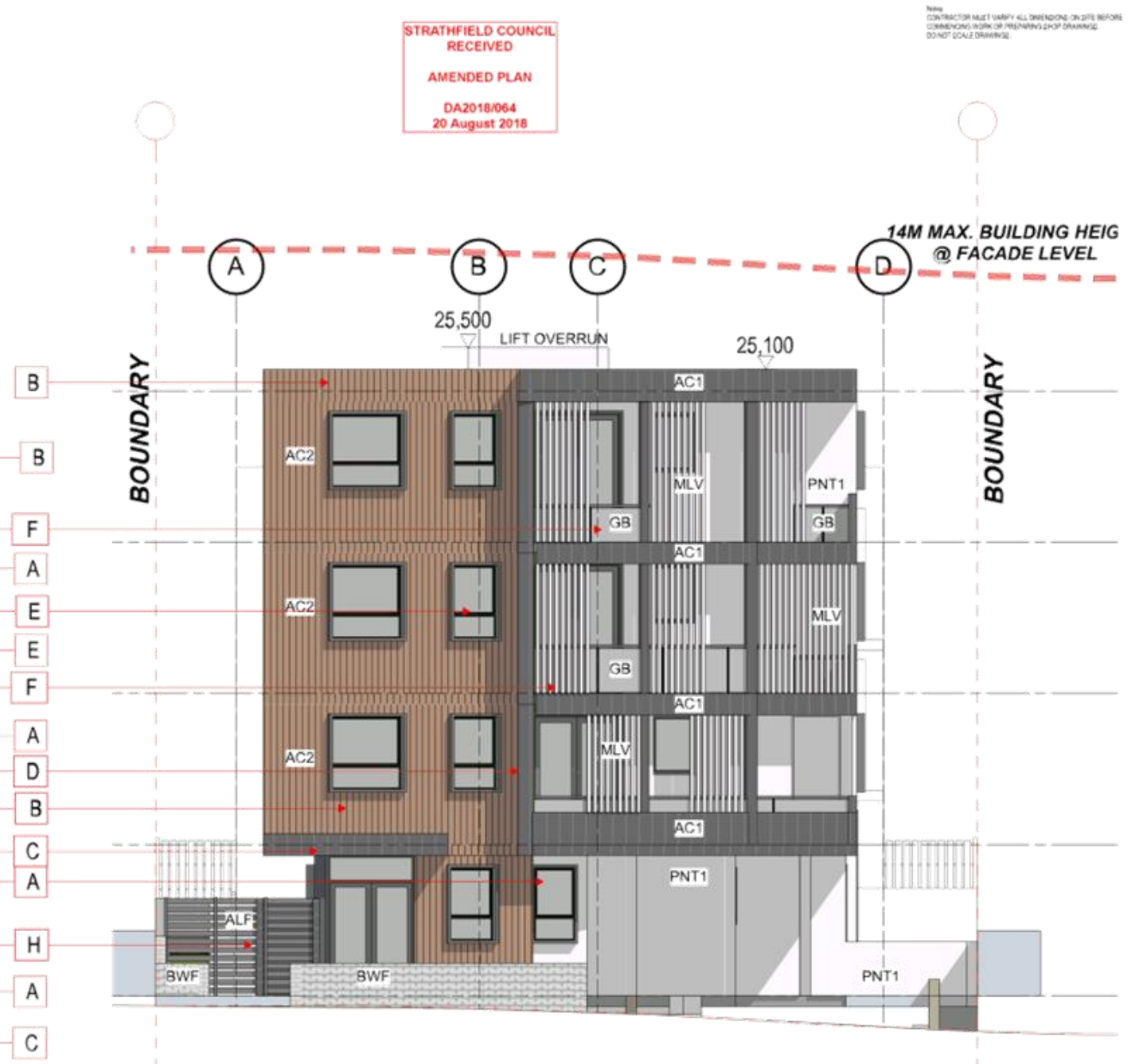
Date of Issue	Checker	Apprnt
20/08/2018	JE	TJ

Scale
AS SHOWN

Project Number
2017 - 020

Drawing Number
DA4001 C

Date
DEVELOPMENT APPLICATION



C	DA AMENDED DRAWINGS	22.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.05.18

Project:
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2126



Drawing Title:
 COLOUR SCHEDULE

Date of Issue	20/08/2018	Checker	JE	Approved	TJ
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Scale:
 AS SHOWN
 Project Number:
 2017 - 020
 Drawing Number/Revision:
 DA6301 C
 Status:
 DEVELOPMENT APPLICATION

A	PNT1 PAINT FINISH TAUBMANS - BRILLIANT WHITE OR SIMILAR	B	ALC2 ALUMINIUM CLADDING DECOWOOD - CASUARINA OR SIMILAR PRODUCT	C	BWF BRICK PGH - MOON DUST OR SIMILAR	D	PNT2 PAINT FINISH DULUX POWDERCOAT - MALAY GREY (MATT FINISH) OR SIMILAR	E	ALUMINIUM CLADDING DULUX POWDERCOAT - MALAY GREY (MATT FINISH) OR SIMILAR	F	MLV ALUMINIUM LOUVERS DECOWOOD - CASUARINA OR SIMILAR PRODUCT	G	GB GLASS BALUSTRADE FRAME COLOUR D OR SIMILAR	H	ALF ALUMINIUM FENCE & LOUVERS FRAMING: COLOUR D LOUVERS: COLOUR D OR SIMILAR
						4									
						TO BE USED FOR GLAZING FRAMING & BALUSTRADES		ALUMINIUM FRAMED WINDOW & DOORS FRAME COLOUR D OR SIMILAR							

FINISHES SCHEDULE

Note:
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STRATHFIELD COUNCIL
RECEIVED
AMENDED PLAN
DA2018/064
20 August 2018

C	DA AMENDED DRAWINGS	22.08.18
B	DA AMENDED DRAWINGS	19.07.18
A	DA SUBMISSION	13.06.18

Rev. Description Date Assn
Project
3 HOMEBUSH RD STRATHFIELD
BOARDING HOUSE
3 HOMEBUSH ROAD
STRATHFIELD NSW 2128



Drawing Title
PERSPECTIVE

Date of Issue	Checker	Approved	
20/08/2018	JE	TJ	

Scale
AS SHOWN

Project Number
2017 - 020

Drawing Number/Revision
DA6302 C

Date
DEVELOPMENT APPLICATION

NOTE:
CONTRACTOR MUST VERIFY ALL DIMENSIONS ON SITE BEFORE
COMMENCING WORK. IF PREPARING SHOP DRAWINGS,
DO NOT SCALE DRAWING.



C	DA AMENDED DRAWINGS	22.08.18
B	DA AMENDED DRAWINGS	18.07.18
A	DA SUBMISSION	13.05.18

Rev: Description Date: Asst
 Project:
 3 HOMEBUSH RD STRATHFIELD
 BOARDING HOUSE
 3 HOMEBUSH ROAD
 STRATHFIELD NSW 2128

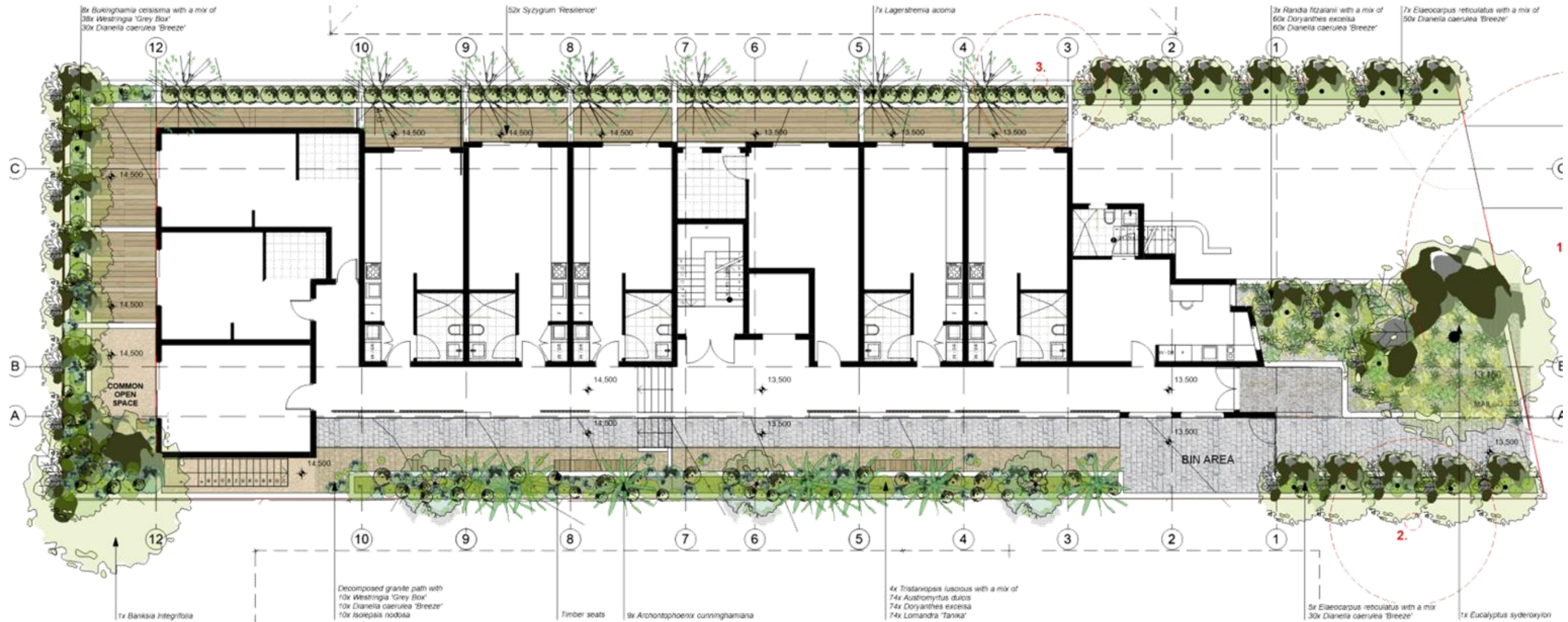


Drawing Title:
 PHOTOMONTAGE

Date of Issue: 20/08/2018
 Checked: JE
 Approved: TJ

Scale:
 AS SHOWN
 Project Number: 2017 - 020
 Drawing Number: DA6303
 Revision: C
 Date: DEVELOPMENT APPLICATION

STRATHFIELD COUNCIL
 RECEIVED
 AMENDED PLAN



TREES TO BE RETAINED OR REMOVED		PLANT SCHEDULE - GROUND FLOOR				
SPECIES		COMMON NAME	MATURE SIZE	QTY	POT SIZE	NATIVE
Please refer to the 'Arborist Report' for 3 Homebush Road Strathfield prepared by Redgum Horticultural - 2018, REF: 3916, for all information relating to removal or retention of existing trees. Existing trees to be retained and removed are numbered from T1 to T3 and correspond to the tree numbering within the Arborist's Report.						
T1						
T2						
T3						
Tree 1, 2, 3 to be removed						
STRATHFIELD COUNCIL RECEIVED (AMENDED PLAN) DA2018/064 19 July 2018						
SPECIES		COMMON NAME	MATURE SIZE	QTY	POT SIZE	NATIVE
TREES						
Archontophoenix cunninghamiana	Bangalow Palm	10-15m	9	5-9m trunk	YES	
Banksia integrifolia	Coast Banksia	7-8m	1	100L	YES	
Elaeocarpus reticulatus	blueberry ash	10-15m	12	5-9m trunk	YES	
Lagerstromia acoma	Crepe Myrtle	3m	7	100L		
Randia fitzalanii	Yellow Mangosteen	3m	3	100L	YES	
Tristanopsis 'Luscious'	Water Gum	7-8m	4	100L	YES	
Eucalyptus syderoxylon	Eucalyptus		1	100L	YES	
Banksia integrifolia	Ivory curl	8 m	8	100L	YES	
SHRUBS/GRASSES/CLIMBERS						
Austromyrtus dulcis	Midgen Berry	1m	74	5L	YES	
Blechnum 'Silver Lady'	Silver Lady Fern	1 m	30	5L	YES	
Dianella caerulea 'Breeze'	Blue Flax Lily	0.5 m	180	140mm pot	YES	
Doryanthes excelsa	Gymea Lily	2m	134	5L	YES	
Isotria medeoloides	Knodding Club Rush	0.5 m	10	140mm pot	YES	
Lomandra 'Tanika'	Tanika	0.5 m	134	140mm pot	YES	
Syzygium 'Resilience'	Lilly Pilly	3m	52	45L	YES	
Westringia 'Grey Box'	Coastal Rosemary	1m	38	45L	YES	

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ISSUE	DESCRIPTION	DATE
A	Draft for comments	27.04.18
B	FOR DA SUBMISSION	17.07.18

PROJECT	3 HOMEBUSH RD STRATHFIELD NSW 2135
CLIENT	

DRAWING TITLE				
LANDSCAPE PLAN				
SCALE @ A2	DRAWN	PROJECT NO.	DWG NO.	SHEET
1:100	MW	1823	LS01	1 of 1

ISSUE **B**

NOTES
REFER TO DETAIL SURVEY FOR SITE IDENTIFICATION, BOUNDARY INFORMATION & LEVELS
ALL LEVELS AND DIMENSIONS TO BE CONFIRMED ON SITE. IF ANY DISCREPANCIES OCCUR PLEASE NOTIFY THE LANDSCAPE ARCHITECT

TO: Strathfield Local Planning Panel Meeting - 4 October 2018
REPORT: SLPP – Report No. 4
SUBJECT: DA2018/101 - 13 & 15 DUNLOP STREET, STRATHFIELD SOUTH
LOT 1 & 2 DP 863557
DA NO. DA2018/101

SUMMARY

Proposal: Use of the existing premises for the purpose of manufacturing cured in place pipe lining and the storage of associated equipment and materials.

Applicant: Abergeldie Complex Infrastructure

Owner: R.A. Craig

Date of lodgement: 16 July 2018

Notification period: 24 July 2018 to 10 August 2018

Submissions received: Four (4) written submissions were received.

Assessment officer: ND

Estimated cost of works: Nil

Zoning: IN1 General Industrial - SLEP 2012

Heritage: N/A

Flood affected: Yes

Is a Clause 4.6 variation proposed? No

RECOMMENDATION OF OFFICER: **APPROVAL**

EXECUTIVE SUMMARY

- 1.0 The application seeks Council approval for the use of an existing premises for the manufacturing of a cured in place pipe (CIPP) liner and the storage of associated equipment and materials.
- 2.0 The application was notified in accordance with Part L of the SCDCP 2005 from 24 July 2018 to 10 August 2018. Four (4) submissions were received raising concerns regarding traffic and noise impacts generated by trucks and storage and use of chemicals.
- 3.0 The proposed use is compatible with the IN1 General Industrial zoning of the site. The scale of the development is suitable whereby traffic and environmental impacts to the surrounding streetscape are minimal. To ensure the residential amenity of the wider streetscape is maintained conditions of consent have been recommended to restrict the number of daily truck movements and require the installation of an acoustic barrier (as recommended in the submitted acoustic impact assessment report).
- 4.0 Overall, the proposal is acceptable on its merits and is recommended for approval, subject to the recommended conditions of consent.

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)

BACKGROUND

- 4 June 1997 DA97/45 approved for the use of the warehouse for materials, plant and equipment used in civil construction and to carry out internal alterations to provide an office.
- 21 December 2017 Notice of Proposed Order pursuant to Section 121H of the *Environmental Planning & Assessment Act 1979* was issued to the owner as it was identified that the site was operating outside the confines of its development consent DA97/45 being for the use of the premises for storage purposes only and does not include the manufacturing of cured in place pipe lining.
- 16 July 2018 The current application was lodged.
- 24 July to
10 August 2018 The plans and accompanying documents were publicly notified. Four (4) written submissions were received raising concerns regarding truck noise and the use of toxic chemicals.
- 9 August 2018 A deferral letter was sent to the Applicant, requesting additional information in relation to the ongoing operations of the proposed development, internal layout of the building, parking and delivery arrangements and plan of management.
- 22 August 2018 Additional information including a Community Management Plan, Project Management Plan, Hazardous Chemical Management Plan and amended internal layout of the premises were submitted.
- 28 August 2018 A deferral letter was sent to the Applicant requesting additional information in regards to the Noise Impact Assessment Report, Air Quality Report and Environmental Management Plan.
- 10 September 2018 A meeting was held at Council to discuss matters raised in the deferral letter dated 28 August 2018.
- 20 September 2018 Amended Noise Impact Assessment Report and Air Quality Report was submitted.

DESCRIPTION OF THE SITE AND LOCALITY

The site is located on the western side of the cul-de-sac of Dunlop Street and is currently occupied by a warehouse/industrial building. Dunlop Street contains industrial and commercial premises including a concrete batching plant, printing business and engineering-related premises. The site adjoins industrial uses to the north and south and adjoins Dean Reserve to the west. The wider locality to the north and east comprises of low density residential housing (illustrated in Figure 1). As Dunlop Street is a cul-de-sac, all heavy vehicles must pass through the residential street Dean Street to access the wider road network.

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
 Lot 1 & 2 DP 863557 (Cont'd)

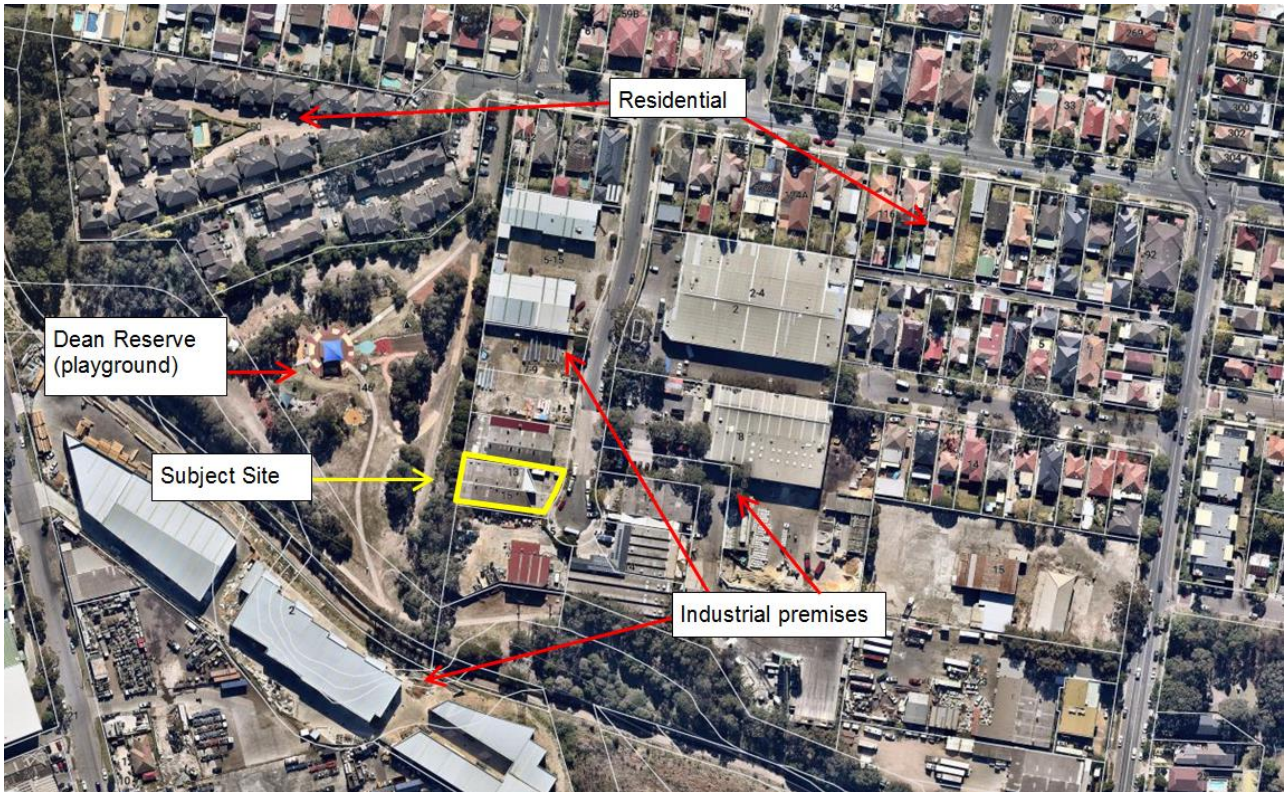


Figure 1: Aerial view of the subject site and surrounding industrial and residential properties. The subject site is outlined in yellow.



Figure 2: View of the subject site from Dunlop Street.

PROPERTY BURDENS AND CONSTRAINTS

There are no easements or burdens on the land which could affect, or be affected by, the proposed development.

DESCRIPTION OF THE PROPOSED DEVELOPMENT

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)

The application seeks Council approval for the use of the existing premises for the purpose of manufacturing cured in place pipe liner and the storage of associated equipment and materials.

The specific elements of the proposal are:

Use:

- Manufacturing of a CIPP liner; and
- Storage of equipment, materials and chemicals associated with the CIPP liner process.

Hours of Operation:

- Monday to Friday: 7am to 5pm
- Saturday: 8am to 5pm
- Due to the nature of the manufacturing process requiring completion once started, the use will occasionally necessitate operational hours extending to 2am.

Staff:

- During the wet out of a cured in place pipe liner, seven (7) staff members will be present with an additional three (3) staff members supervising the process during audits.

Deliveries:

- Deliveries will be via flat bed truck and semi trailers.

REFERRALS

INTERNAL REFERRALS

Waste Comments

Council's Waste Officer offered no objections to the proposal, subject to the imposition of recommended conditions of consent.

Traffic Comments

Council's Traffic Engineer offered no objections to the proposal, subject to the imposition of a special condition limiting the number of truck movements to and from the site.

Environmental Health Comments

Council's Environmental Health Officer offered no objections to the proposal, subject to the imposition of the recommended conditions of consent.

EXTERNAL REFERRALS

Environment Protection Authority (EPA) Comments

The EPA commented as follows:

"The proposed use does not constitute Scheduled Activities under Schedule 1 of the Protection of the Environment (Operations) Act, 1997 (POEO). The EPA does not consider that the proposal will require an Environmental Protection Licence (EPL) under the POEO Act."

SECTION 4.15 CONSIDERATIONS – EP&A Act, 1979

In determining a development application, the consent authority is to take into consideration the following matters of consideration contained within Section 4.15 of the Environmental Planning and Assessment Act, 1979 as relevant to the development application:

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)

4.15(1)(a) the provisions of:

(i) any environmental planning instrument

State Environmental Planning Policy No. 33 – Hazardous and Offensive Development (SEPP 33)

SEPP 33 applies to any proposals which fall under the definition of ‘potentially hazardous industry’ or ‘potentially offensive industry’. Clause 13 requires the consent authority to consider guidelines published by the Department of Planning relating to hazardous or offensive development, consult relevant public authorities and in the case of development that falls under the category of potentially hazardous industry, a preliminary hazard analysis is to be prepared and submitted.

In accordance with SEPP 33, the proposed development was assessed against “Hazardous and Offensive Development Application Guidelines Applying SEPP 33” and Hazardous Industry Planning Advisory Papers (HIPAP) published by the NSW Department of Planning and Environment. As the proposal exceeds the thresholds in the risk screening tests, it is classified as a potentially hazardous industry. Subsequently, a Preliminary Hazard Analysis (PHA) was prepared to determine the risk to people, property and the environment at the proposed location.

The PHA concluded that the proposal, with the implementation of controls identified in the risk matrix, falls in the lowest category of the Risk Identification Matrix. In addition to the PHA, the development application was accompanied by a WorkCover NSW Acknowledgement of Notification of Hazardous Chemical letter, Emergency Response Management Plan, Occupational Chemical Exposure Assessment and Hazardous Chemical Risk Assessment. Council’s Environmental Health officer has raised no objections to the proposal subject to the recommended conditions of consent.

A special condition of consent has been recommended for an independent Hazard Audit to be conducted 12 months after operations commence (and every three years thereafter) to verify the safety systems and that the facility is operating in accordance with its hazards-related conditions of consent. This is to include the implementation of the recommendations contained in the Air Quality and Noise Impact Assessment Reports.

Based on the information submitted by the applicant, the proposal is of low risk in terms of storage of hazardous materials and chemicals and the manufacturing of cured in place pipe liners. Overall, the objectives outlined within SEPP 33 are considered to be satisfied.

State Environmental Planning Policy No 55 – Remediation of Land

SEPP 55 applies to the land and pursuant to Section 4.15 is a relevant consideration. The proposal does not involve excavation or construction works.

The objectives outlined within SEPP55 are considered to be satisfied.

STRATHFIELD LOCAL ENVIRONMENTAL PLAN (SLEP) 2012

An assessment of the proposal against the general aims of SLEP 2012 is included below:

Cl. 1.2(2) Aims	Complies
(a) <i>To achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield</i>	Yes

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Lot 1 & 2 DP 863557 (Cont'd)

(b)	To promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development	Yes
(c)	To promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community	Yes
(d)	To provide opportunities for economic growth that will enhance the local community	Yes
(e)	To promote future development that integrated land use and transport planning, encourages public transport use, and reduced the traffic and environmental impacts of private vehicle use	Yes
(h)	To minimise risk to the community by identifying land subject to flooding and restricting incompatible development	Yes

Comments: The proposal is consistent with the general aims of SLEP 2012, in that it proposes a compatible industrial use in industrial zoned land providing employment opportunities for the community. An Acoustic Impact Assessment Report accompanied the application recommending the installation of an acoustic barrier along the northern boundary of the site to ensure residential amenity to the wider streetscape is maintained.

Permissibility

The subject site is zoned IN1 General Industrial under the *Strathfield Local Environmental Plan 2012*. The proposed use of manufacturing the wet out of a cured in place pipe lining and storage of associated materials falls under the definition of general industry as follows:

“general industry means a building or place (other than a heavy industry or light industry) that is used to carry out an industrial activity”

General industry is permissible within the IN1 zone with consent.

Zone Objectives

An assessment of the proposal against the objectives of the IN1 Zone is included below:

Objectives	Complies
➤ To provide a wide range of industrial and warehouse land uses.	Yes
➤ To encourage employment opportunities.	Yes
➤ To minimise any adverse effect of industry on other land uses.	Yes
➤ To support and protect industrial land for industrial uses.	Yes
➤ To minimise fragmentation of valuable industrial land, and provide large sites for integrate and large floorplate activities.	Yes

Comments: The proposal is consistent with the objectives of the IN1 zone as it will provide an industrial use which is compatible with the existing industrial sites within the streetscape and encourage employment opportunities. Operations are primarily undertaken within the existing building and suitably separated from residential properties to maintain an acceptable level of residential acoustic amenity.

Part 4: Principal development standards

An assessment of the proposal against the relevant provisions contained within Part 4 of the SLEP 2012 is provided below.

Height of building

Cl.	Standard	Controls	Proposed	Complies
4.3	Height of building	12m	Unchanged	Yes

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Lot 1 & 2 DP 863557 (Cont'd)

	Objectives	Complies
(a)	<i>To ensure that development is of a height that is generally compatible with or which improves the appearance of the existing area</i>	Yes
(b)	<i>To encourage a consolidation pattern that leads to the optimum sustainable capacity height for the area</i>	Yes
(c)	<i>To achieve a diversity of small and large development options.</i>	Yes

Comments:

The propose use is to be located within the existing building on the site.

Part 5: Miscellaneous Provisions

Not applicable.

Part 6: Local Provisions

The relevant provisions contained within Part 6 of the SLEP 2012 are addressed below as part of this assessment:

6.1 Acid sulfate soils

The subject site is identified as having Class 4 soils. As the proposal does not seek approval for any excavation, the application was not required to be accompanied by an Acid Sulfate Management Plan and has satisfied the requirements of Clause 6.1 of the *SLEP 2012*.

6.2 Earthworks

No earthworks are proposed with the development application.

6.3 Flood planning

The subject site is identified as being affected in the 1 in 100 year flood event by the Cooks River. As the proposal does not seek consent for building works and the flooding is contained to the front setback, no further assessment is required.

6.4 Essential services

Clause 6.4 of the SLEP 2012 requires consideration to be given to the adequacy of essential services available to the subject site. The proposed development is located within a well serviced area and features existing water and electricity connection and access to Council's stormwater drainage system. As such, the subject site is considered to be adequately serviced for the purposes of the proposed development.

4.15 (1)(a)(ii) any draft environmental planning instruments

There are no applicable draft planning instruments that are or have been placed on public exhibition, to consider as part of this assessment.

4.151)(a)(iii) any development control plan

STRATHFIELD CONSOLIDATED DEVELOPMENT CONTROL PLAN (SCDCP) 2005

The following is an assessment of the proposal's compliance with the relevant provisions contained within SCDCP 2005.

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Lot 1 & 2 DP 863557 (Cont'd)

PART D – INDUSTRIAL DEVELOPMENT

An assessment of the proposal against the objectives and development controls contained within Part D of SCDCP 2005 is included below:

1.2: Objectives of Part D

1.2	Objectives	Satisfactory
A.	To improve the quality of industrial development within the Strathfield Municipality	Yes
B.	To ensure the orderly development of industrial sites to minimise their environmental impact while maximising their functional potential	Yes
C.	To ensure development is consistent with the principles of Ecologically Sustainable Development	Yes
G.	To ensure that development will not unreasonably impact upon the amenity of any residential area in the vicinity;	Yes
H.	To ensure that traffic generated by industrial development does not adversely impact upon local or regional traffic movements	Yes
I.	To ensure that each development has adequate on-site parking and manoeuvring areas for vehicles	Yes
J.	To encourage employee amenity within industrial developments.	Yes

Comments:

The proposal seeks to utilise the existing building for the storage of materials and manufacturing of CIPP liners. The storage and manufacturing are pre-dominantly undertaken within the existing building thereby minimising noise emissions and odours generated by the CIPP lining process. Nonetheless, to maintain the residential amenity of surrounding residential properties, conditions of consent have been recommended for the premises to comply with the NSW Environmental Protection Authority's Environmental Noise Control Manual and Noise Policy for Industry 2017 and the Protection of the Environment Operations Act 1997.

The existing on-site parking provisions including five (5) car parking spaces and two (2) loading bays are adequate for the maximum number of staff proposed. To ensure the scale of operations at the premises remain compatible with the residential context of the wider locality, a condition of consent has been recommended for the number of truck movements (to and from the site) to be limited to (14) per day.

2.1: Site Analysis and Design Principles

2.1	Objectives	Satisfactory
A.	To ensure layout and building design makes best use of the existing characteristics, opportunities and constraints of the site and surrounds to result in a high quality development sensitive to the environment	Yes
B.	To ensure all activities associated with the development do not adversely impact on the environment.	Yes

2.1	Design Principles	Complies
	The development is compatible with the predominant height, bulk, scale and character of existing development in the vicinity	Yes

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Lot 1 & 2 DP 863557 (Cont'd)

Comments: The application does not propose any building works that will alter the existing height and floor space ratio.

2.4: Development Adjoining Residential Zones

2.4 Objectives		Satisfactory
A.	To ensure industrial development does not unreasonably impact or intrude upon any adjoining residential area(s).	Yes
2.4 Guidelines		Complies
4	Goods, plant equipment and other materials are to be stored within the proposed industrial building or suitably screened from residential development.	Yes
5	Noise associated with the premises including plant and equipment will be subject to the NSW Environmental Protection Authority's Environmental Noise Control Manual and Industrial Noise Policy 2000 and the Protection of the Environment Operations Act 1997.	Yes
6	Noise generated from fixed sources or motor vehicles associated with the proposed industrial development will be effectively insulated or otherwise minimised.	Yes
7	The operating noise level of plant and equipment shall generally not exceed 5dB(A) above the background noise level when measured at the boundaries of the premises between the hours of 7.00am and 10.00pm.	Yes
8	If operating noise levels of plant and equipment are proposed outside the hours of 7.00pm and 10.00pm, the applicant may be subject to a merit based assessment which may need to be supported by an Acoustical Engineers' report.	Yes
9	The development shall not otherwise cause nuisance to residents, by way of hours of operation, traffic movement, parking, headlight glare, security lighting and the like.	Yes

Comments:

The subject site is located in Dunlop Street (a cul-de-sac), with the nearest residential property approximately 100m from the premises. Accordingly, an Acoustic and Air Quality Impact Assessment Report accompanied the Development Application. During the assessment process, Council's Environmental Health Officer requested additional information including an Occupational Chemical Exposure Assessment. The additional information satisfied these concerns with Council's Environmental Health Officer raising no objection to the proposal subject to the recommendations of the Acoustic and Air Quality Impact Assessment Reports.

The Noise Impact Assessment recommends the installation of a 4m high acoustic barrier along the northern boundary of the site to reduce the noise levels to nearby residential properties. The Air Quality Impact Assessment recommends that field odour surveys are undertaken to confirm that odours emitted during the wet-out process are not a nuisance (as defined under the *Protection of the Environment Operations Act 1997*). In the event that the odours are deemed a nuisance, a mechanical ventilation system is to be installed. A condition of consent shall be imposed for all recommendations contained in the Acoustic and Air Quality Impact Assessment Reports to be adopted, implemented and adhered to.

In accordance with Clause 2.4, a condition of consent has been recommended for the noise and air pollution emissions associated with the premises to comply with Environmental Pollution

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 Lot 1 & 2 DP 863557 (Cont'd)

Authority's Noise Policy for Industry 2017 and the *Protection of the Environment Operations Act 1997 (NSW)*.

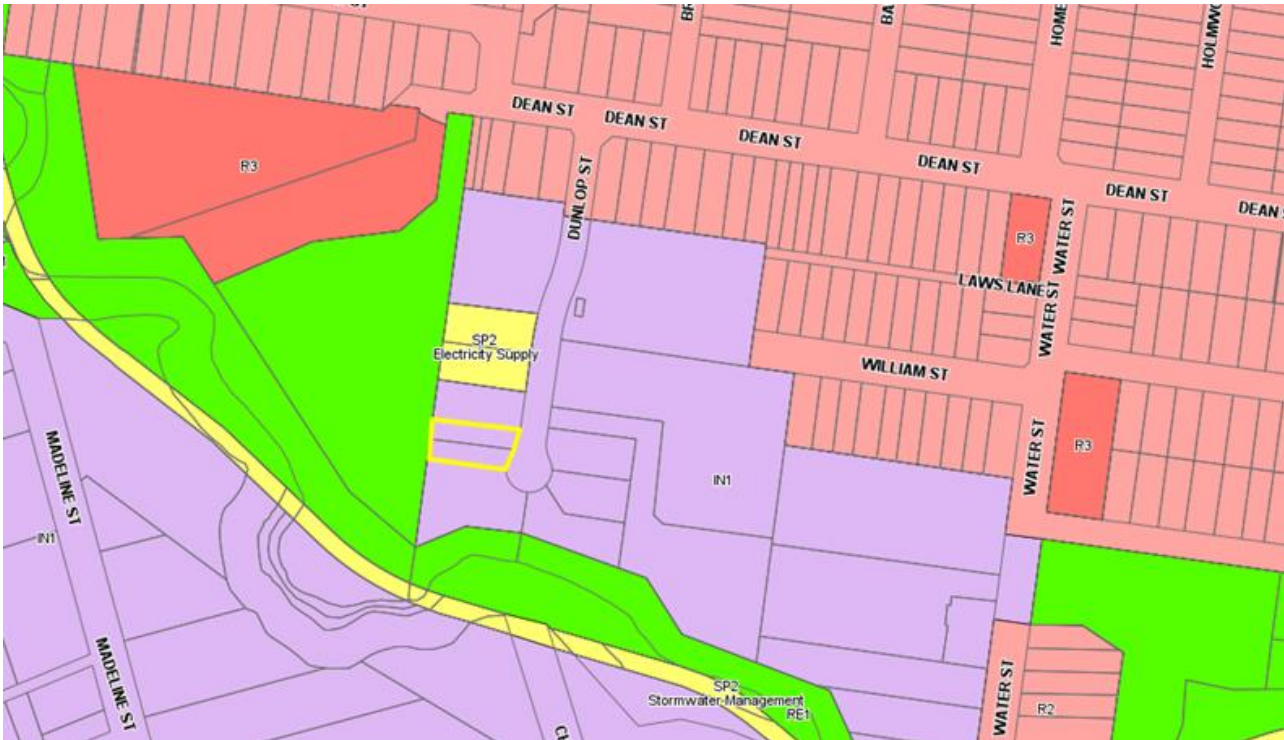


Figure 3: SLEP 2012 land use zoning map of the subject site and surrounding streetscape (subject site is outlined in yellow).
 IN1 (purple) = General Industrial R2 (light red) = Low Density Residential R3 (red) = Medium Density Residential

2.9: Parking, Access and Manoeuvring

2.9.1 Objectives		Satisfactory	
A.	To ensure sufficient car parking spaces are provided on-site for employees and visitors;	Yes	
B.	To ensure the effective design of car parking areas;	Yes	
2.9 Guidelines and Requirements		Complies	
.1.	Parking		
	1	The design of off-street parking areas meet the requirements of Australian Standard (AS) 2890.1-1993 – Off-street car parking, AS2890.2-1989 – Commercial vehicles and Strathfield Part I - Provision of Off-Street Parking Facilities.	Yes
	2	Provision of spaces:	
	(i)	Industry: 1 space per 50m2 GFA where any office component is under 20%. If the office component is greater than 20% that additional area will be assessed at a rate of 1 space per 40m2 GFA.	No, refer to discussion below.
	(iii)	Delivery and service vehicles associated with a development: 1 space per 800m2 GFA up to 8,000m2 GFA plus 1 space per 1,000m2 GFA thereafter.	Yes

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	3	<i>Car parking areas located in the front setback for easy access.</i>	Yes
	4	<i>Loading/unloading and parking areas are separated so as not to cause conflict.</i>	No, refer to discussion below.

Comments:

Under Section 2.9.1, industry premises must provide on-site car parking at a rate of one (1) space per 50m² GFA and one (1) delivery and service vehicle space per 800m², resulting in a (14) car space and 1 delivery space minimum requirement. The existing on-site parking arrangement provides five (5) on-site car parking spaces and two (2) delivery bays with direct access to the building via roller doors. The proposal does not propose any building works or alterations to the existing on-site parking provisions, with an average of seven (7) employees and an additional three (3) during management inspections. Whilst there is a shortfall in the number of on-site parking spaces, there is sufficient on-street parking available along Dunlop Street and Dean Street to accommodate the shortfall. In this instance, the variation to the parking controls is considered acceptable.

2.9.2	Objectives	Satisfactory
A.	<i>To ensure that provision is made for safe vehicular ingress and egress having regard to the nature of vehicles likely to patronise the site;</i>	Yes
B.	<i>To ensure satisfactory on-site manoeuvring for vehicles, including the loading/unloading of goods;</i>	Yes
C.	<i>To minimise potential for congestion or hazard on adjoining roads at points of ingress/egress;</i>	Yes
D.	<i>To ensure that traffic generated by industrial development does not adversely affect local or regional traffic movements</i>	Yes
E.	<i>To ensure that any traffic generated by the development will not impact unreasonably upon the amenity of any residential areas in the vicinity</i>	Yes

2.9	Guidelines and Requirements	Complies
	<i>Site Design</i>	
.4.	1 <i>All vehicles are to enter and leave the site in a forward direction</i>	No, refer to discussion below.
	2 <i>Driveways and manoeuvring areas are designed so that all vehicles entering and leaving the site can do so with minimum interference to traffic on adjoining roads.</i>	Yes
	4 <i>All servicing, including waste collection, is carried out wholly within the site with suitable collection points at convenient locations.</i>	Yes
.5.	<i>Unloading and Loading</i>	
	1 <i>All loading and unloading shall take place within the curtilage of the site.</i>	Yes

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
 Lot 1 & 2 DP 863557 (Cont'd)

Comments:

The proposal does not seek to alter the existing on-site parking and delivery arrangements which do not enable heavy rigid vehicles to enter and leave the site in a forward direction. However, the cul-de-sac configuration of the street enables the truck to maneuver a three-point turn safely without impeding on the flow of the traffic in the street (as shown below in Figure 4). As a cul-de-sac street, the only direct vehicular access for trucks (due to weight limits) to the wider road network via Dean Street, which is a R2 Low Density Residential zoned area. To minimise the noise and traffic impacts generated by the trucks associated with the development, a condition of consent has been recommended for the maximum number of truck movements to and from the site in any one day is limited to (14).

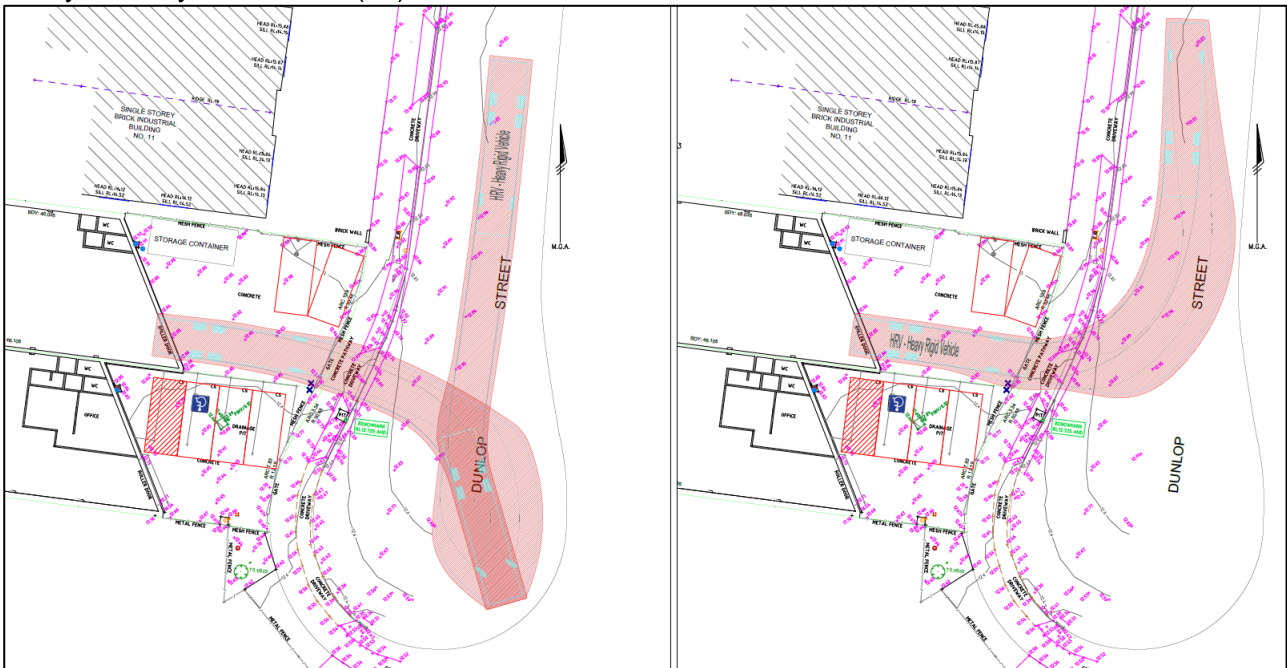


Figure 4: Swept paths of a heavy rigid vehicle.

2.14: Air Noise and Water Pollution

2.14 Objectives		Satisfactory
A.	To ensure industrial developments do not create a pollution problem by the discharge of an unacceptable level of air, noise and/or water emissions.	Yes
2.14 Guidelines and Requirements		Complies
General		
.1.	1 The emission of any air impurities including offensive odours, the discharge of any waste into any waters or the emission of noise associated with any development shall not contravene the Protection of the Environment Operations Act 1997.	Yes
Noise		
.2.	3 The proposed building(s) are designed to inhibit the transmission of noise. Note: Council may require an acoustic report from a suitably qualified acoustic consultant where a proposed development may create	Yes

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Lot 1 & 2 DP 863557 (Cont'd)

	<i>excessive noise.</i>		
4	<i>The use of the premises including plant and equipment will be subject to strict compliance with the NSW Environmental Protection Authority's Environmental Noise Control Manual and the Industrial Noise Policy 2000.</i>	Yes	
<i>Water</i>			
5	<i>Details of the types, volumes and method of storage of any chemicals to be used on site shall be submitted with any development application.</i>	Yes	
.3.	6	<i>Only clean water shall be discharged to the stormwater system.</i>	Yes
	7	<i>Any discharge to Sydney Water's sewer will need their approval and may involve a Trade Waste Agreement.</i>	Yes
	9	<i>Any wastewater that is generated in this manner is considered as trade waste and may need pre treatment prior to its discharge to the sewer.</i>	Yes

Comments:As discussed above in Section 2.4, the proposal was accompanied by an acoustic and air quality impact assessment report. Council's Environmental Health Officer raises no objections to the development, subject to the implementations of the recommendations contained in the acoustic and air quality reports.

The Development Application was accompanied by an Environmental Management Plan with additional information and amended plans submitted during the assessment process regarding the on-site storage details of the various chemicals and their utilisation during the CIPP lining process. All chemicals are to be stored in designated storage containers in accordance with Australian Standards and Safe Work NSW requirements. A condition of consent has been recommended to this effect. Spill kits will be located throughout the premises.

PART H - WASTE MANAGEMENT (SCDCP 2005)

A separate Waste Management Plan did not accompany the Development Application, with waste storage and disposal arrangements detailed in the Environmental Management Plan. Spent drums and general waste are to be stored in a designated area within the building, collected and transported by a private contractor. To ensure all waste generated by the premises is stored and disposed of in an appropriate manner conditions of consent are recommended pertaining to the removal of trackable waste (as defined by the NSW Environment Protection Authority) in accordance with the Protection of the Environment Operations Waste Regulation 2005. Prior to the re-commencement of operations an updated and separate Waste Management Plan shall be submitted to Council.

4.15 (1)(a)(iia) any planning agreement or draft planning agreement

Not applicable.

4.15 (1)(a)(iv) matters prescribed by the regulations

No applicable.

(i) any coastal zone management plan

The NSW Government projects sea levels to rise by 40cm in 2050 and by 90cm in 2100 above the relative mean sea level in 1990. These planning benchmarks are to be considered in the

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)

assessment of development applications through the applicable coastal zone management plan or alternatively the provisions of the *NSW Coastal Planning Guideline: Adapting to Sea Level Rise*.

Although Council is not subject to a coastal zone management plan, the sea level rise planning benchmarks have also been established in order to assess the likely increase in the frequency, duration and height of flooding and as a consequence likely property and infrastructure damage on affected and potentially affected land. Council is therefore required to consider the impact of sea level rise and resultant flooding from Powell's Creek and Cook's River which are tributaries of Sydney Harbour (Parramatta River) and Botany Bay respectively.

The subject site is identified as being affected in the 1 in 100 year flood event by the Cooks River. As the proposal does not seek consent for the building works and the flooding is contained to the front setback, no further assessment is required.

4.15(1)(b) *the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality*

Hours of Operation

The proposal seeks to manufacture CIPP liners Monday to Friday 7am to 5pm and Saturday 8am to 5pm. These hours of operation are consistent with other industrial premises along Dunlop Street. However, there will be some exception given the nature of the business. Once the manufacturing process commences for the wetting out of a liner, the process must be completed otherwise the product will not be of an acceptable quality. Consequently in some instances operations are required to continue until 2am for larger liners. With the installation of the acoustic barrier (as recommended in the submitted Acoustic Impact Assessment Report) and the nature of the equipment being located internally within the building footprint, the likely impacts on surrounding residents in continuing the operation until 2am will be somewhat minimised. Nonetheless, to further maintain residential amenity, a condition of consent has been recommended for the extension of hours until 2am once a fortnight, only where written approval has been obtained by Council.

4.15 (1)(c) *the suitability of the site for the development*

The proposed development is considered to be suitable for the site in that the operations are of a small scale and are unlikely to result in adverse traffic, noise or air quality impacts. Specifically, the number of trucks, noise and odour emissions associated with the premises are considered reasonable and minimised through the recommended conditions of consent and implementation of the submitted acoustic and air quality impact assessment reports.

4.15 (1)(d) *any submissions made in accordance with this Act or the regulations*

The application was notified in accordance with Part L of the SCDCP 2005 from 24 July 2018 to 10 August 2018, with four (4) submissions received, raising the following concerns:

1. Truck Noise

Concern is raised regarding the noise generated by the number of trucks along Dunlop Street and Dean Street. Dean Street is primarily a residential area, if this application is approved it will only worsen the situation. Dunlop Street is a light industrial area and there should be no heavy vehicles.

Assessing officer's comments: The site is located within Dunlop Street and zoned IN1 General Industrial, whereby the proposed development is permissible with Council consent. The proposed use is of a small scale with operations and deliveries determined by a demand basis. A condition of consent has been recommended to restrict the number of daily truck movements to and from the

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site to (14) to minimise traffic and associated truck noise. Additionally, all deliveries are confined to the operational hours of the premises.

2. Storage and Use of Chemicals

Concern is raised regarding the toxic chemical liquids stored and used to produce the cured pipe lining at the premises. There have been occasions where the drums have been left open outside at the front of the building.

Assessing officer's comments: During the assessment process a meeting was held with the Applicant to discuss the investigations which were carried out by Council identifying the storage of spent resin drums outside the premises. The internal layout of the premises has been amended to facilitate a designated spent drum and waste storage area. Additionally, a special condition of consent has been recommended stating that no drums are to be left unattended outside of the building. All materials and chemicals are to be stored and disposed of in accordance with Australian Standards, Protection of Environment and Operations Act 1997 and the NSW Work Cover Authority.

3. Increased Traffic

There is already a large amount of traffic along Dean Street as a result of the industrial vehicles in Dunlop Street. The approval of this application will further exacerbate the problem.

Assessing officer's comments: The number of trucks associated with the proposed use is significantly smaller than the surrounding businesses operating in Dunlop Street. The scale of operations is compatible with the size of the premises, with a condition of consent recommended to restrict the number of truck movements to and from the site to 14 per day Monday to Saturday.

4.15 (1)(e) the public interest

The public interest is served through the detailed assessment of this development application under the relevant local planning controls and legislation and consideration of any submissions received relating to it by Council. The proposed development is not considered to be contrary to the public interest.

SECTION 7.11 CONTRIBUTIONS

Section 7.11 of the EP&A Act 1979 relates to the collection of monetary contributions from applicants for use in developing key local infrastructure. The Act reads as follows:

- “(1) If a consent authority is satisfied that development for which development consent is sought will or is likely to require the provision of or increase the demand for public amenities and public services within the area, the consent authority may grant the development consent subject to a condition requiring:*
- (a) the dedication of land free of cost, or*
 - (b) the payment of a monetary contribution,*
 - or both.*
- (2) A condition referred to in subsection (1) may be imposed only to require a reasonable dedication or contribution for the provision, extension or augmentation of the public amenities and public services concerned.”*

STRATHFIELD DIRECT SECTION 7.11 CONTRIBUTIONS PLAN

Section 7.11 Contributions are not applicable to the proposed development in accordance with the Strathfield Direct Development Contributions Plan 2010-2030 as no building works are proposed.

CONCLUSION

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The application has been assessed having regard to Section 4.15 of the Environmental Planning and Assessment Act 1979, the Strathfield Local Environmental Plan 2012 and the Strathfield Development Control Plan 2005 and is considered to be satisfactory for approval, subject to the recommended conditions of consent.

The proposed industrial use is of a small scale that achieves the objectives of the IN1 General Industrial zone and is unlikely to result in adverse noise, traffic and odour impacts to the surrounding streetscape.

In approving this development it will be necessary for the applicant to surrender the existing consent at the site DA97/45 (use of a warehouse for materials, plant and equipment as used in civil construction and to carry out internal alterations to provide an office) to avoid conflicting land use consents.

RECOMMENDATION

That Development Application No. 2018/101 for the use of the existing premises for the purpose of manufacturing cured in place pipe lining and the storage of associated equipment and materials at 13 & 15 Dunlop Street, Strathfield South be **APPROVED**, subject to the following conditions:

SPECIAL CONDITIONS (SC)**1. SURRENDER OF CONSENT (CC)**

The Applicant shall surrender Development Consent No. DA97/45 at 15 Dunlop Street for the use of a warehouse for materials, plant and equipment as used in civil construction and to carry out internal alterations to provide an office in accordance with the Environmental Planning and Assessment Regulation 2000, prior to release of the Construction Certificate for the acoustic barrier.

(Reason: To ensure certainty as to the consent applying to the land.)

2. HAZARD AUDIT (SC)

Twelve (12) months after the commencement of operations of the development and every three (3) years thereafter, a comprehensive Hazard Audit of the development consistent with the Department of Planning's Hazardous Industry Planning Advisory Paper No. 5 'Hazard Audit Guidelines' is to be carried out. The audit shall be carried out by a qualified person or team, independent of the development.

A copy of the Hazard Audit report shall be forwarded to Council.

(Reason: To ensure the development is carried out in a safe manner.)

3. STORAGE OF CHEMICAL DRUMS (SC)

No chemical drums including resin are to be stored outside of the premises unattended, unless stored in an enclosed refrigeration container.

(Reason: To ensure the appropriate storage of chemicals.)

4. HOURS OF OPERATION - INDUSTRIAL PREMISES (SC)

The hours of operation of the approved industrial premises must not exceed the following, without the prior approval of Council:

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Days	Approved hours of operation
Mon-Friday	7:00am to 4:30pm
Saturday	8:00am to 4:30pm
Sunday and public holidays	Closed

Notwithstanding the above, the premises is permitted to be used for the manufacturing process associated with the cured in placement pipe lining (no deliveries permitted) until 2am subject to written approval from Council being obtained. Approval from Council must be sought at least five (5) days prior to any such operations and shall not be applied for more than once every fortnight Monday to Friday.

This condition does not apply in respect of refrigeration equipment, unless they create a nuisance or were not part of the original consent.

(Reason: Ensure industry operates between approved hours.)

5. NOISE RESTRICTIONS – OUT OF HOURS (SC)

The operational noise from site operations during out of hours works pre-approved by Council in accordance with Condition No. 3 shall comply with the following noise limits:

Location	Period	Maximum Noise Level L _{Aeq, 15min} dBA
Residential suburban	Evening (6pm- 10pm)	40
Residential suburban	Night (10pm to 7am)	38

6. NOISE LEVEL (SC)

The use of the premises shall not give rise to:

- a) The transmission of unacceptable vibration to any surrounding occupancy; and
- b) A noise level exceeding the background (L90) noise level by more than 5dB(A) when measured from an adjoining premise. The noise level shall be assessed as an LA eq 15min reading and adjusted in accordance with current EPA Guidelines for tonality, frequency weighting, impulsive characteristics, fluctuation and intermittency or any subsequent guidelines.

7. TRUCK MOVEMENTS (SC)

The total maximum number of truck movements to and from the site on any day shall not exceed fourteen (14). No double barrel trucks shall be used.

(Reason: To maintain residential Amenity)

GENERAL CONDITIONS (GC)

8. APPROVED PLANS AND REFERENCE DOCUMENTATION (GC)

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The development, except where modified by a condition(s) of consent, is to be carried out in accordance with the following plans and reference documentation:

Plans affixed with Council's 'Development Consent' stamp relating to Development Consent No.2018/101:

Drawing No.	Title/Description	Prepared by	Issue / Revision & Date	Date received by Council
-	Site Plan	Abergeldie Complex Infrastructure	5.1	20 September 2018

Reference Documentation affixed with Council's 'Development Consent' stamp relating to Development Consent No.2018/101:

Title / Description	Prepared by	Issue/Revision & Date	Date received by Council
Air Quality Impact Assessment	Wilkinson Murray	Version B dated 18 September 2018	20 September 2018
Emergency Response Management Plan	Abergeldie Complex Infrastructure	Revision dated 18 September 2018	20 September 2018
Environmental Management Plan	Abergeldie Complex Infrastructure	Revision dated 18 September 2018	20 September 2018
Noise Impact Assessment	Wilkinson Murray	Version B dated 18 September 2018	20 September 2018
Traffic & Parking Statement	PDC Consultants	Dated 6 July 2018	16 July 2018
Statement of Environmental Effects	Nexus	Dated 11 July 2018	16 July 2018

In the event of any inconsistency, the conditions of this consent shall prevail.

(Reason: To ensure that the form of the development undertaken is in accordance with the determination of Council.)

9. CONSTRUCTION HOURS (GC)

No construction or any other work related activities shall be carried out on the site outside the hours of 7.00 am to 5.00 pm Mondays to Fridays and 8am to 1pm Saturdays.

No building activities are to be carried out at any time on a Sunday or public holiday.

Where the development involves the use of jackhammers/rock breakers and the like or other heavy machinery, such equipment may only be used between the hours of 7.00 am - 5.00 pm Monday to Friday only.

(Reason: To maintain amenity to adjoining land owners.)

10. CONSTRUCTION WITHIN BOUNDARIES (GC)

The development including but not limited to footings, walls, roof barges and guttering must be constructed wholly within the boundary of the premises. No portion of the proposed structure shall encroach onto the adjoining properties. Gates must be installed so they do

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not open onto any footpath or adjoining land.

(Reason: Approved works are to be contained wholly within the subject site.)

11. **LIGHTING (GC)**

Any lighting of the premises shall be installed and maintained in accordance with Australian Standard AS 4282-1997: Control of the Obtrusive Effects of Outdoor Lighting so as to avoid annoyance to the occupants of adjoining premises or glare to motorists on nearby roads.

No flashing, moving or intermittent lighting, visible from any public place may be installed on the premises or external signage associated with the development, without the prior approval of Council.

(Reason: To protect the amenity of surrounding development and protect public safety.)

12. **PRINCIPAL CERTIFYING AUTHORITY (PCA) IDENTIFICATION SIGN (GC)**

Prior to commencement of any work, signage must be erected in a prominent position on the work site identifying:

- i) the Principal Certifying Authority (PCA) by showing the name, address and telephone number of the PCA;
- ii) the Principal Contractor by showing the Principal Contractor's name, address and telephone number (outside of work hours) for that person; and
- iii) the sign must state that unauthorised entry to the work site is prohibited.

Any such sign is to be maintained while the work is being carried out, but must be removed when the work has been completed.

This clause does not apply to building work, subdivision work or demolition work that is carried out inside an existing building that does not affect the external walls of the building.

(Reason: Statutory requirement.)

13. **SITE MANAGEMENT (DURING DEMOLITION AND CONSTRUCTION WORKS)**

All of the following are to be satisfied/complied with during demolition, construction and any other site works:

- i) All demolition is to be carried out in accordance with Australian Standard AS 2601-2001.
- ii) Demolition must be carried out by a registered demolition contractor.
- iii) A single entrance is permitted to service the site for demolition and construction. Protection pads are to be installed to the kerb, gutter and nature strip where trucks and vehicles enter the site.
- iv) No blasting is to be carried out at any time during construction of the building.
- v) Care must be taken during demolition/ excavation/ building/ construction to prevent any damage to adjoining buildings.
- vi) Adjoining owner property rights and the need for owner's permission must be observed at all times, including the entering onto land for the purpose of undertaking works.
- vii) Any demolition and excess construction materials are to be recycled wherever practicable.
- viii) The disposal of construction and demolition waste must be in accordance with the

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- requirements of the Protection of the Environment Operations Act 1997.
- ix) All waste on the site is to be stored, handled and disposed of in such a manner as to not create air pollution (including odour), offensive noise or pollution of land and/or water as defined by the Protection of the Environment Operations Act 1997. All excavated material should be removed from the site in an approved manner and be disposed of lawfully to a tip or other authorised disposal area.
 - x) All waste must be contained entirely within the site.
 - xi) A site plan including temporary waste storage locations, construction material storage areas and vehicular access to and from the site must be included with application.
 - xii) Section 143 of the Protection of the Environment Operations Act 1997 requires waste to be transported to a place which can lawfully accept it. All non-recyclable demolition materials are to be disposed of at an approved waste disposal depot in accordance with legislation.
 - xiii) Details as to the method and location of disposal of demolition materials (weight dockets, receipts, etc.) should be kept on site as evidence of approved methods of disposal or recycling and must be presented to Council and/or Principal Certifying Authority upon request.
 - xiv) All materials on site or being delivered to the site are to generally be contained within the site. The requirements of the Protection of the Environment Operations Act 1997 must be complied with when placing/stockpiling loose material, disposing of concrete waste, or other activities likely to pollute drains or water courses.
 - xv) Any materials stored on site must be stored out of view or in such a manner so as not to cause unsightliness when viewed from nearby lands or roadways.
 - xvi) Public footways, include nature strips and roadways adjacent to the site must be maintained and cleared of obstructions during construction. No building materials, waste containers or skips may be stored on the road reserve or footpath without prior separate approval from Council, including payment of relevant fees.
 - xvii) Building operations such as brick-cutting, washing tools or paint brushes, and mixing mortar not be performed on the roadway or public footway or any other locations which could lead to the discharge of materials into the stormwater drainage system.
 - xviii) All site waters during excavation and construction must be contained on site in an approved manner to avoid pollutants entering into waterways or Council's stormwater drainage system.
 - xix) Any work must not prohibit or divert any natural overland flow of water.
 - xx) Toilet facilities for employees must be provided in accordance with WorkCover NSW.

(Reason: To ensure that demolition, building and any other site works are undertaken in accordance with relevant legislation and policy and in a manner which will be non-disruptive to the local area.)

14. WASTE – TRACKABLE (GC)

Removal of trackable waste (as defined by the NSW Environment Protection Authority) from the site must comply with the Protection of the Environment Operations (Waste) Regulation 2005 for the transportation, treatment and disposal of waste materials. Waste materials must not be disposed on land without permission of the land owner and must with the provisions of the Protection of the Environment and Operations Act 1997.

(Reason: To ensure compliance with legislation.)

15. HAZARDOUS GOODS AND WASTE (GC)

Dangerous and hazardous goods shall be stored in accordance with NSW WorkCover requirements and AS1940-2004 - The Storage and Handling of Flammable and Combustible Liquids.

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Hazardous waste must be contained, managed and disposed of in a responsible manner in accordance with the Protection of Environment and Operations Act 1997, NSW Environment Protection Authority (EPA) and the New South Wales WorkCover Authority.

Material Safety Data Sheets of all hazardous substances used, such as bleaches and nail products must be kept on site and readily available to all staff. Staff must be trained how to safely use and store these chemicals prior to the commencement of work.

Details demonstrating compliance with the requirements of this condition are to be submitted to the satisfaction of the Principal Certifying Authority prior to the issue of any Construction Certificate.

(Reason: Health and safety.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE (CC)

16. BUILDING CODE OF AUSTRALIA - COMPLIANCE WITH (CC)

All architectural drawings, specifications and related documentation shall comply with the Building Code of Australia (BCA). All work must be carried out in accordance with the requirements of the Building Code of Australia (BCA).

In the case of residential building work for which the Home Building Act 1989 requires there to be a contract of insurance in force in accordance with Part 6 of that Act, such a contract of insurance is to be in force before any building work authorised to be carried out by the consent commences.

Details demonstrating compliance with this condition are to be submitted to the Principle Certifying Authority, prior to issue of the Construction Certificate.

(Reason: This is a 'prescribed' condition under clause 98(1) of the Environmental Planning and Assessment Regulation 2000.)

17. COMMENCEMENT OF WORKS (NO WORKS UNTIL A CC IS OBTAINED)

Building work, demolition or excavation must not be carried out until a Construction Certificate has been issued by either Strathfield Council or a Principal Certifying Authority.

Demolition of any part of a building triggers 'commencement of erection of building' pursuant of section 4.19 of the EP&A Act 1979. Accordingly, demolition works must not commence until a Construction Certificate has been issued, a Principal Certifying Authority has been appointed and a Notice of Commencement has been issued.

(Reason: To ensure compliance with statutory provisions.)

18. CONSTRUCTION AND ENVIRONMENTAL MANAGEMENT PLAN (CC)

The applicant must prepare and submit a Construction and Environmental Management Plan (CEMP) to the Principal Certifying Authority, including:

- i) Detailed information on any approvals required from other authorities prior to or during construction.
- ii) Traffic management, including details of:
 - ingress and egress of vehicles to the site;

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- management of loading and unloading of materials;
 - the location of heavy vehicle parking off-site; and
 - designated routes for vehicles to the site.
- iii) The proposed areas within the site to be used for a builder's site office and amenities, the storage of excavated material, construction materials and waste containers during the construction period.
- iv) Erosion and sediment control, detailing measures and procedures consistent with the requirements of Council's guidelines for managing stormwater, including:
- the collection and treatment of stormwater and wastewater generated on site prior to discharge; and
 - procedures to prevent run-off of solid material and waste from the site.
- v) Waste management, including:
- details of the types and estimated volumes of waste materials that will be generated;
 - procedures for maximising reuse and recycling of construction materials; and
 - details of the off-site disposal or recycling facilities for construction waste.
- vi) Dust control, outlining measures to minimise the generation and off-site transmission of dust and fine particles, such as watering or damp cloth fences.
- vii) A soil and water management plan, which includes:
- measures to minimise the area of soils exposed at any one time and conserve top soil;
 - identification and protection of proposed stockpile locations;
 - preservation of existing vegetation and revegetation;
 - measures to prevent soil, sand, sediments leaving the site in an uncontrolled manner;
 - measures to control surface water flows through the site in a manner that diverts clean run-off around disturbed areas, minimises slope gradient and flow distance within disturbed areas, ensures surface run-off occurs at non-erodible velocities, and ensures disturbed areas are promptly rehabilitated;
 - details of sediment and erosion control measures in place before work commences;
 - measures to ensure materials are not tracked onto the road by vehicles entering or leaving the site; and
 - details of drainage to protect and drain the site during works.
- viii) Asbestos management procedures:
- Anyone who removes, repairs or disturbs bonded or a friable asbestos material must hold a current removal licence from Workcover NSW holding either a Friable (Class A) or a Non- Friable (Class B) Asbestos Removal License which ever applies and a current WorkCover Demolition License where works involve demolition. To find a licensed asbestos removalist please see www.workcover.nsw.gov.au
 - Removal of asbestos by a person who does not hold a Class A or Class B asbestos removal license is permitted if the asbestos being removed is 10m² or less of non-friable asbestos (approximately the size of a small bathroom). Friable asbestos materials must only be removed by a person who holds a current Class A asbestos license.
 - Before starting work, a work site-specific permit approving each asbestos project must be obtained from WorkCover NSW. A permit will not be granted without a current WorkCover licence. All removal, repair or disturbance of or to asbestos material must comply with the following:
 - The Work Health and Safety Act 2011;
 - The Work Health and Safety Regulation 2011;
 - How to Safety Remove Asbestos Code of Practice – WorkCover 2011; and
 - Safe Work Australia Code of Practice for the Management and Control of

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Asbestos in the Workplace.

- Following completion of asbestos removal works undertaken by a licensed asbestos removalist re-occupation of a workplace must not occur until an independent and suitably licensed asbestos removalist undertakes a clearance inspection and issues a clearance certificate.
- The developer or demolition contractor must notify adjoining residents at least two (2) working days (i.e. Monday to Friday exclusive of public holidays) prior to the commencement of asbestos removal works. Notification is to include, at a minimum:
 - the date and time when asbestos removal works will commence;
 - the name, address and business hours contact telephone number of the demolisher, contractor and/or developer;
 - the full name and license number of the asbestos removalist/s; and
 - the telephone number of WorkCover's Hotline 13 10 50
 - warning signs informing all people nearby that asbestos removal work is taking place in the area. Signs should be placed at all of the main entry points to the asbestos removal work area where asbestos is present. These signs should be weatherproof, constructed of light-weight material and adequately secured so they remain in prominent locations. The signs should be in accordance with AS 1319-1994 Safety signs for the occupational environment for size, illumination, location and maintenance; and
 - appropriate barricades installed as appropriate to prevent public access and prevent the escape of asbestos fibres. Barricades must be installed prior to the commencement of asbestos removal works and remain in place until works are completed.

(Reason: Safety, amenity and protection of public infrastructure and the environment.)

19. **COUNCIL PERMITS – FOR ALL ACTIVITIES ON COUNCIL LAND (CC)**

Works Permit

(as per Section 68 of the Local Government Act 1993 and Section 138 and 139 of the Roads Act 1993)

A Works Permit is required for construction of a vehicular crossing (driveway), new stormwater down pipe connection to kerb and gutter, new footpath and/or stormwater connection. A Works Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Standing Plant Permit

This permit must be applied for where it is intended to park a concrete pump, crane or other plant on the roadway or footpath. A Standing Plant Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Skip Bin Permit

This permit must be applied for if you intend to place a skip bin on the roadway or footpath. A Skip Bin Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Temporary Full or Part Road Closure Permit

This permit must be applied for if you require a full or a part road closure to take place to

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assist in your construction works. Please use the Works Permit Application Form, which is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Hoarding/Fencing Permit

This permit must be applied for if you intend to erect a Class A (fence type) or Class B (overhead type) hoarding/fencing along the street frontage(s). A Hoarding Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Work Zone Permit

This permit must be applied for if you require permanent parking along the kerbside at the front of the site during construction works. A Work Zone Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Ground Anchoring Permit

This permit must be applied for, for the installation of ground anchors under Council's footway/road reserve. It does not cover ground anchors under private properties. A separate approval is required to be obtained from Roads and Maritime Services (RMS) if it is proposed to install ground anchors under a State or Classified Regional Road (please refer to the end of this application form for more information).

(Reason: Council requirement.)

20. WASTE MANAGEMENT PLAN (CC)

A Waste Management Plan (WMP) is to be provided in accordance with Part H of Strathfield Consolidated Development Control Plan 2005. All requirements of the approved Waste Management Plan must be implemented during demolition, construction and on-going use of the premises.

The WMP must identify the types of waste that will be generated and all proposals to reuse, recycle or dispose of the waste. The WMP is to be submitted and approved to the Principal Certifying Authority, prior to issue of the Construction Certificate. A copy of the Waste Management Plan shall be sent to Council.

(Reason: To ensure appropriate management of waste.)

21. FIRE SAFETY SCHEDULE (CC)

A Fire Safety Schedule specifying the fire safety measures (both current and proposed) which should be implemented in the building premises must be submitted with the Construction Certificate application, in accordance with Part 9 of Clause 168 of the Environmental Planning and Assessment Regulation 2000.

Note: A Construction Certificate cannot be issued until a Fire Safety Schedule is received.

(Reason: Compliance with the Environmental Planning and Assessment Act 1979.)

22. SPECIALISED WASTE EQUIPMENT (CC)

Details of any specialised waste disposal equipment to be used in the development i.e. compactors (carousel and linear), bin tugs, chutes, crushers, bunding, oil water separators

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(coalescing plate separators), etc. to be provided to Council for approval.

(Reason: To ensure compliance with legislation.)

CONDITIONS TO BE SATISFIED PRIOR TO THE COMMENCEMENT OF WORKS (CW)

23. APPOINTMENT OF A PRINCIPAL CERTIFYING AUTHORITY (PCA) (CW)

No work shall commence in connection with this Development Consent until:

- i) A construction certificate for the building work has been issued by the consent authority or a Principal Certifying Authority.
- ii) The person having the benefit of the development consent has appointed a principal certifying authority for the building work, and notified the principal certifying authority that the person will carry out the building work as an owner/builder, if that is the case.
- iii) The principal certifying authority has, no later than 2 days before the building work commences:
 - notified the Council of his or her appointment, and
 - notified the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- iv) The person having the benefit of the development consent, if not carrying out the work as an owner-builder, has:
 - appointed a principal contractor for the building work who must be the holder of a contractor licence if any residential building work is involved;
 - notified the principal certifying authority of such appointment; and
 - unless that person is the principal contractor, notified the principal contractor of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- v) The person having the benefit of the development consent has given at least 2 days' notice to the Council of the person's intention to commence the erection of the building.

Note: If the principal certifying authority is the Council, the nomination will be subject to the payment of a fee for the service to cover the cost of undertaking all necessary inspections and the issue of the appropriate certificates.

Under the Environment Planning and Assessment (Quality of Construction) Act, 2003, a sign must be erected in a prominent position on the work site showing the name, address and telephone number of the principal certifying authority; the name of the principal contractor (if any) for the building work and a telephone number at which that person may be contacted outside working hours. That sign must also state that unauthorised entry is prohibited. The sign must not be removed until all work has been completed.

(Reason: Statutory requirement.)

24. NOTICE OF COMMENCEMENT (CW)

No work shall commence until the following details are submitted to Council:

- i) a Notice of Commencement (form will be attached with issue of a Construction Certificate or available from our website) within two (2) days of the date on which it is proposed to commence works associated with the Development Consent;
- ii) details of the appointment of a Principal Certifying Authority (either Council or another Principal Certifying Authority); and

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- iii) details of the name, address and licence details of the Builder.

(Reason: Statutory requirement.)

CONDITIONS TO BE SATISFIED DURING DEMOLITION AND BUILDING WORKS (DW)

25. OBSTRUCTION OF PUBLIC WAY NOT PERMITTED DURING WORKS (DW)

The public way must not be obstructed by any materials, vehicles, refuse, skips or the like, under any circumstances, without the prior approval of Council.

(Reason: To maintain public access and safety.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF AN OCCUPATION CERTIFICATE (OC)

26. FIRE SAFETY CERTIFICATION (OC)

A fire safety certificate shall be obtained in accordance with Part 9, Division 4 of the Environmental Planning and Assessment Regulation 2000, prior to the issue of any Occupation Certificate.

A fire safety certificate is a certificate issued by the owner of a building to the effect that each essential fire safety measure specified in the current fire safety schedule for the part of the building to which the certificate relates:

- i) has been assessed by a properly qualified person; and
- ii) was found, when it was assessed, to be capable of performing to at least the standard required by the current fire safety schedule for the building for which the certificate is issued.

An interim fire safety certificate must be provided before an interim occupation certificate can be used for a building under Clause 153(2) of the Environmental Planning & Assessment Regulation 2000.

A final fire safety certificate must be provided before an interim occupation certificate can be used for a building under Clause 153(1) of the Environmental Planning & Assessment Regulation 2000.

A copy of the fire safety certificate and fire safety schedule shall be:

- i) submitted to Strathfield Council;
- ii) submitted to the Commissioner of the New South Wales Fire Brigade; and
- iii) prominently displayed in the building.

(Reason: Fire safety and statutory requirement.)

27. OCCUPATION OF BUILDING (OC)

A person must not commence occupation or use (or change of use where an existing building) of the whole or any part of a new building (within the meaning of section 109H (4) of the Act) unless an Interim Occupation Certificate or Final Occupation Certificate has been issued in relation to the building or part.

The Principal Certifying Authority is required to be satisfied, amongst other things, that:

**DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)**

- i) all required inspections (including each applicable mandatory critical stage inspection) have been carried out; and
- ii) any preconditions to the issue of the certificate required by a development consent have been met.

Note: New building includes an altered portion of, or an extension to, an existing building.

(Reason: Statutory requirement.)

28. VENTILATION SYSTEMS – MECHANICAL (OC)

The mechanical ventilation system is to comply with the following:

- i) The Building Code of Australia;
- ii) Protection of the Environment Operations Act 1997; and
- iii) Australian Standard AS1668-1991.

In addition, odour control measures, such as activated carbon or catalytic oxidisers, must be used to treat ventilation gases prior to discharge. The method of odour control must be designed by a suitably qualified mechanical ventilation engineer.

At the completion of the installation of the mechanical exhaust ventilation system, a certificate from a practising mechanical engineer shall be submitted to the Principal Certifying Authority, prior to issue of the Occupation Certificate demonstrating compliance with the above.

(Reason: To ensure the mechanical exhaust ventilation system complies with the relevant requirements/standards.)

29. CHEMICAL STORAGE REQUIREMENTS (OU)

All chemicals, paints and other liquids shall be stored in approved receptacles in an area that is bunded or has a spill containment system that will minimise the risk of pollution from liquid spills and leaks. Where applicable the construction of bunds must comply with the requirements of:

- i) Australian Standard AS 1940D1993: The storage and Handling of Flammable and Combustible Liquids;
- ii) Australian Standard AS 4452B1997: The storage and Handling of Toxic Substances; and
- iii) The Dangerous goods Act 1975.

(Reason: Environmental protection.)

30. DELIVERIES (OU)

All deliveries must not occur before 7:00am or after 8:00pm weekdays and before 8:00am or after 4:30pm weekends and public holidays, to avoid noise disruption to the surrounding area.

(Reason: To control noise impacts.)

31. FIRE SAFETY ANNUAL STATEMENT (OU)

Pursuant to Part 9, Division 5 of the Environmental Planning and Assessment Regulation (as amended) the owner of the building shall provide to Council an Annual Fire Safety

**DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)**

Statement from an appropriately qualified person certifying the essential fire safety measures in the building. The Annual Fire Safety Statement shall be submitted within 12 months of the issue of the fire safety certificate, and then on an annual basis.

A copy of the Fire Safety Statement obtained and Fire Safety Schedule shall also be:

- i) Forwarded to the Commissioner of the New South Wales Fire Brigade; and
- ii) Prominently displayed in the building.

(Reason: Fire safety.)

32. LOADING AND UNLOADING - NO OBSTRUCTION OF PUBLIC ROAD OR FOOTWAY (OU)

All loading and unloading operations including fork lift trucks or other similar loading, lifting and/or carrying appliances used in conjunction with the premises, shall be carried out wholly within the boundaries of the site, at all times. There shall be no obstruction of any public roadway or footway at any time, without the prior

(Reason: Public safety.)

33. NOISE AND AIR - COMPLAINTS

In the event of Council receiving complaints regarding air pollution or odour from the premises, the person(s) in control of the premises shall at their own cost arrange for an environmental investigation to be carried out (by a suitably qualified person) and submit a report to Council specifying the proposed methods for the control of odour or noise emanating from the premises. Such measures are to be installed at no cost to Council and may require further development consent from Council.

(Reason: To ensure amenity is maintained.)

34. NOISE - COMPLIANCE WITH ACOUSTIC ASSESSMENT REPORT (OU)

All recommendations contained in the approved Acoustic Impact Assessment Report prepared by Wilkinson Murray shall be adopted, implemented, and adhered to.

The Principal Certifying Authority (PCA) shall obtain a certificate from an appropriately qualified acoustic consultant, certifying that the acoustic mitigation measures outlined in the above stated report have been suitably incorporated into the development and that relevant noise criteria have been satisfied prior to the issue of any Occupation Certificate.

Any changes made to the proposal that would alter the outcome will require a further assessment and a copy of this further report shall be provided to the PCA for approval and all recommendations of the report shall be adopted, implemented, and adhered to.

(Reason: Noise control and amenity.)

35. AIR QUALITY – COMPLIANCE WITH AIR QUALITY ASSESSMENT REPORT (OU)

All recommendations contained in the approved Air Quality Impact Assessment Report prepared by Wilkinson Murray shall be adopted, implemented, and adhered to.

The Principal Certifying Authority (PCA) shall obtain a certificate statement from an appropriately qualified environmental consultant, stating that the recommendations outlined

**DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)**

in the above stated report have been completed at that relevant odour criteria have been satisfied prior to the issue of any Occupation Certificate.

Any changes made to the proposal that would alter the acoustic assessment will require a further acoustic assessment and a copy of this further report shall be provided to Council for approval prior to the commencement of works and any recommendations adopted, implemented, and adhered to.

(Reason: Noise control and amenity.)

36. NOISE - REQUIREMENTS FOR INDUSTRIAL PREMISES (OU)

The use of the premises shall comply with the requirements of the NSW Environment Protection Authority Industrial Noise Policy 2000 and shall not give rise to the transmission of offensive noise as defined in the Protection of the Environment Operation Act 1997 (NSW).

(Reason: Noise control and amenity.)

37. POLLUTION - COMPLIANCE WITH PEOA 1997 GENERALLY (OU)

The activities carried out on site shall not constitute a nuisance in relation to noise, air or water pollution as specified under the Protection of the Environment Operations Act 1997.

(Reason: Environmental protection.)

38. WASTE AND RECYCLING COLLECTION (COMMERCIAL AND INDUSTRIAL) (OU)

- i) The collection of commercial and industrial waste and recycling must only occur between 6.00am and 8.00pm weekdays and 9.00am and 5.00pm on weekends and public holidays, to avoid noise disruption to the surrounding area.
- ii) Commercial and industrial garbage and recycling must be collected on site unless expressly agreed to in OC.
- iii) Where consent is given for commercial and industrial garbage and recycling to be placed kerbside for collection bins must not be placed on the kerbside more than one hour before the scheduled collection time. Bins and containers are to be removed from the kerbside within one (1) hour of collection and returned to the designated garbage storage area(s).
- iv) The garbage and recyclable storage area and bins must be adequate to contain the volume and type of garbage and recyclable matter of the food premises.
- v) All garbage and recyclable matter must be enclosed in the waste bins with lids completely closed at all times.
- vi) Recycling options must be provided for all commercially recyclable waste products including but not limited to paper, cardboard, oil, food waste, plastics, metals, chemicals etc.

(Reason: To regulate noise and garbage collection arrangements.)

39. CONTROL OF LITTER (OU)

The occupant or person in control of the premises must take all practicable steps to ensure that the area of public footpath or public area adjacent to the premises is maintained in a clean and tidy condition.

A litter management plan must be included with all applications, litter management plan must include the below:

DA2018/101 - 13 & 15 Dunlop Street, Strathfield South
Lot 1 & 2 DP 863557 (Cont'd)

- i) All steps being taken to prevent, reduce and collect any litter produced by the site
- ii) Measures such as cleaner to conduct litter collection within a 50m radius to be included.

(Reason: To manage litter throughout the LGA.)

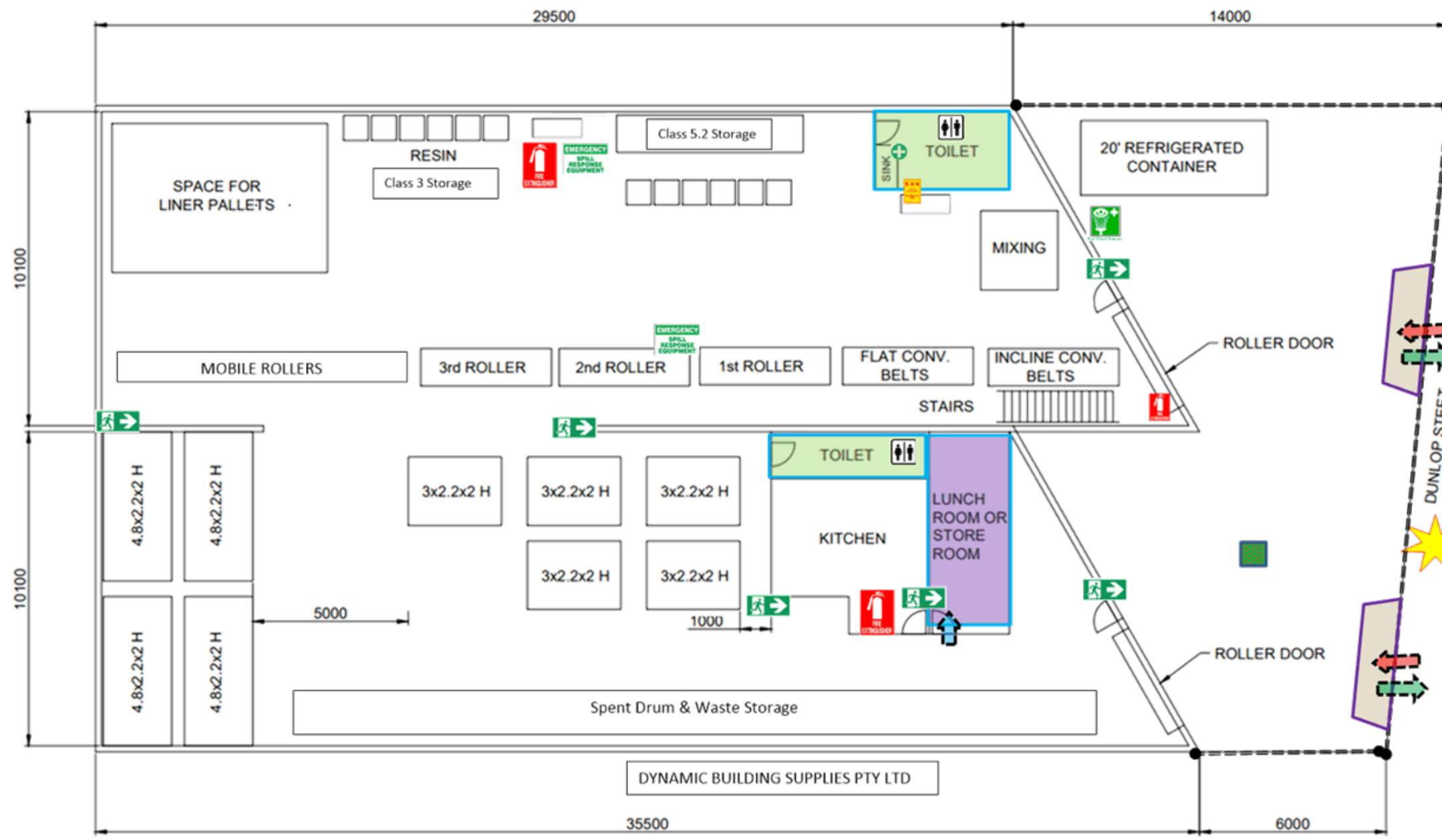
40. **INDUSTRIAL WASTE MATERIALS (OU)**

- i) The applicant shall ensure that industrial waste materials must not be discharged onto the site, or onto neighbouring land or into any road, drain, pipeline or watercourse.
- ii) Storage tanks, fuelling areas, product mixing, filling and preparation areas, and the like and wash bays provided for cleaning and maintenance of vehicles, machinery, equipment and the like, must be bunded and all waste water collected and discharged to the sewer in accordance with the requirements of Sydney Water and the EPA.
- iii) Documentary evidence of compliance with the Sydney Water's requirements must be submitted to the Principal Certifying Authority, prior to use or occupation of the building/additions. Vehicles, machinery, equipment or the like, must not be washed or steam cleaned or the like, except in areas provided especially for the purpose in accordance with this condition.
- iv) Where the approval of Sydney Water, in accordance with the requirements of this condition, is not forthcoming, some other method of treatment and removal of wastes must be installed and/or implemented to the satisfaction the Principal Certifying Authority.
- v) Any overflow or spillage of fuel, oil, solid or liquid chemicals, or the like, must be dealt with in accordance with the requirements for treatment of waste materials as set out in (i) and (ii) of this condition.

(Reason: Manage industrial waste in the LGA.)

ATTACHMENTS

- 1. [↓](#) Architectural Plans
- 2. [↓](#) Statement of Environmental Effects



AMENITIES

- Lunch Room
- Toilet

First Aid Kit, Air Horn and Defibrillator located in site office. Spill Kit located in container.

LEGEND – BOUNDARIES AND AREAS

- Site Parking Area
- North

CONTROLS AND DEVICES

- Drain Protection
- Emergency Ass. Point
- Stabilised Entry/Exit
- Fence

Icons: Toilets, First Aid Kit, SDS Location, Fire Extinguisher, AED, Air Horn, Spill Kit, Sunscreen, Auto External Defibrillator, Chemical Storage Area, Eye Wash Station.

ALL DIMENSIONS TO BE SITE CONFIRMED

WORKING HOURS
 Monday to Friday – 7am to 5pm
 Saturday – 7am to 1pm
 No work on Sunday's or Public Holidays

NOISE
 High noise activities to be limited where possible and nearby residential noise receptors will be marked on this plan.

SITE INDUCTION
 All personnel are required to undergo basic environmental training contained within the site-specific induction.

PEDESTRIAN / TRAFFIC CONTROL

- All traffic control will be deployed as per the approved TMP for the site.
- All pedestrian traffic will be moved to clearly marked routes with approved way-finding signage.

BOUNDARY / EXCLUSION ZONES

- All areas marked as exclusion/Boundary zones (incl. tree protection zones) shall be clearly marked on the above plans.

TRAFFIC DEVICES/CONTROLS.

Vehicles in Route Vehicles Out Route

Emergency Exit Route

PROJECT START / FINISH:

THIS LAYOUT SHALL BE DISPLAYED IN A PROMINENT POSITION ON SITE FOR VIEWING BY ALL PERSONNEL

STRATHFIELD COUNCIL RECEIVED

AMENDED PLAN

DA2018/101
20 September 2018

<p>ABERGELDIE COMPLEX INFRASTRUCTURE</p> <p>WHS020-F05 Issue Date: Oct/2016</p>	<p>15 DUNLOP STREET CIPP WETOUT FACILITY</p>	<p>Address: 15 Dunlop Street, Strathfield South NSW 2136</p>	<p>Environment, Safety and Quality Managers</p>		<p>Emergency Site Contacts</p>	
		<p>DATE: 18/09/2018</p> <p>Revision #5.1</p>	<p>Mark McGrath</p>	<p>EXT 2191</p>	<p>Matthew Boyle</p> <p>Peter Merlo</p>	<p>0407 709 908</p> <p>0407 210 967</p>

STATEMENT OF ENVIRONMENTAL EFFECTS

LIGHT INDUSTRY

Nos.13-15 DUNLOP STREET

STRATHFIELD SOUTH



**STRATHFIELD COUNCIL
RECEIVED**

**DA2018/101
16 July 2018**



STATEMENT OF ENVIRONMENTAL EFFECTS

LIGHT INDUSTRY

Nos.13-15 DUNLOP STREET

STRATHFIELD SOUTH

11 July 2018

Prepared by:
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Attachment 1:	Site Survey
Attachment 2:	Internal Site Survey
Attachment 3:	PDC Traffic and Parking Statement
Attachment 4:	Spill Management Procedure

1. INTRODUCTION

Nexus Environmental Planning Pty Ltd has been requested by Abergeldie Watertech Pty Ltd (**the Applicant**) to prepare a Statement of Environmental Effects (**SEE**) to accompany a Development Application to Strathfield Council (**the Council**) for the operation of a Light Industry at Nos.13-15 Dunlop Street, South Strathfield (**the Site**). The location of the Site is shown on **Figure 1**.



Figure 1: Site location map with the Site outlined in red. © Google Maps

2. THE SITE

The Site has an area of 978.3m² and is legally defined as:

Lots 1 & 2, DP 863557
Nos.13-15 Dunlop Street
STRATHFIELD SOUTH

An extract from an aerial photograph of the Site is at **Figure 2**. A cadastral map is at **Figure 3**.

The site has a single street frontage of 20 metres. The northern and southern boundaries of the Site border industrial developments having lengths of 50 metres and 45 metres respectively. The western boundary borders a public park, Dean Reserve, having a length of 20 metres.



Figure 2: Extract from an aerial photograph with the Site highlighted in yellow. © NearMap

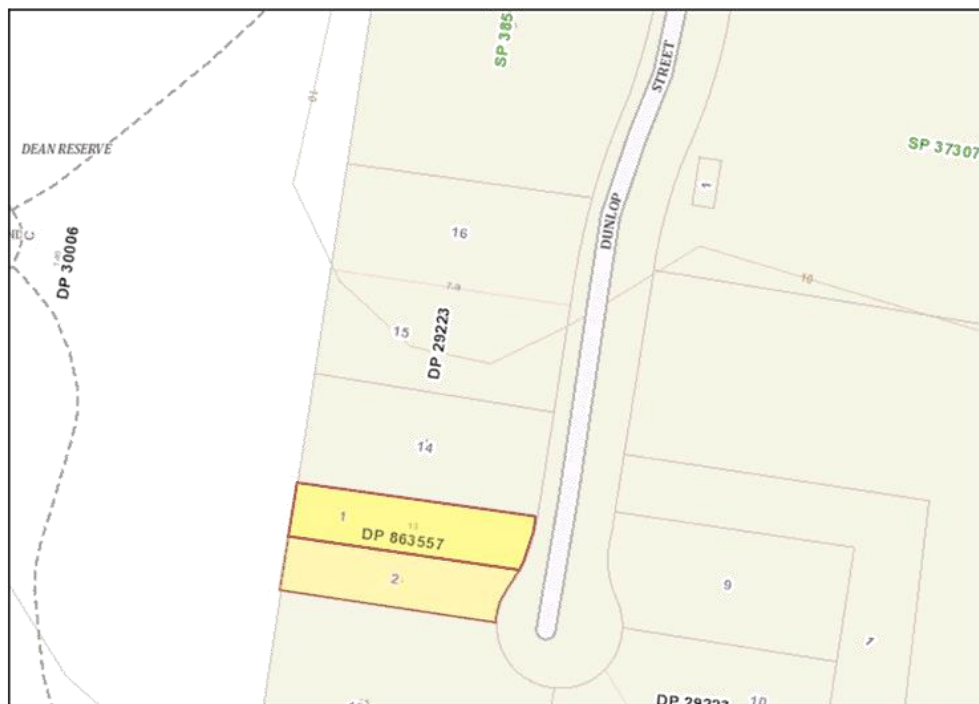


Figure 3: Cadastral Map with the Site highlighted in yellow. © SIX Maps

A survey of the Site has been prepared, a reduced copy of which is at **Figure 4**. A copy of the survey is at **Attachment 1**.

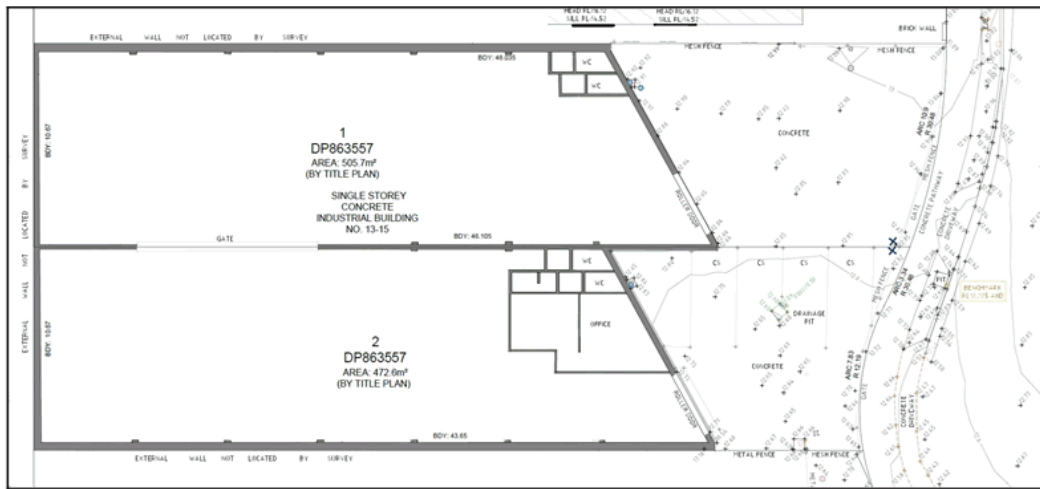


Figure 4: Extract from the survey of the Site. Refer Attachment 1.

The Site contains a single storey concrete building and associated at grade car parking. **Photograph 1** shows the Site.



Photograph 1: This photograph shows the Dunlop Street frontage of the Site.

Dunlop Street is a cul-de-sac and is developed mainly as light industrial development although there are some sites which are currently vacant.

A survey of the internal area of the existing building on the Site has been prepared, a copy of which is at **Attachment 2**. An extract from that survey is at **Figure 5**. The Gross Floor Area of the existing building is 663m².

There is residential development located at the intersection of Dean Street with Dunlop Street.

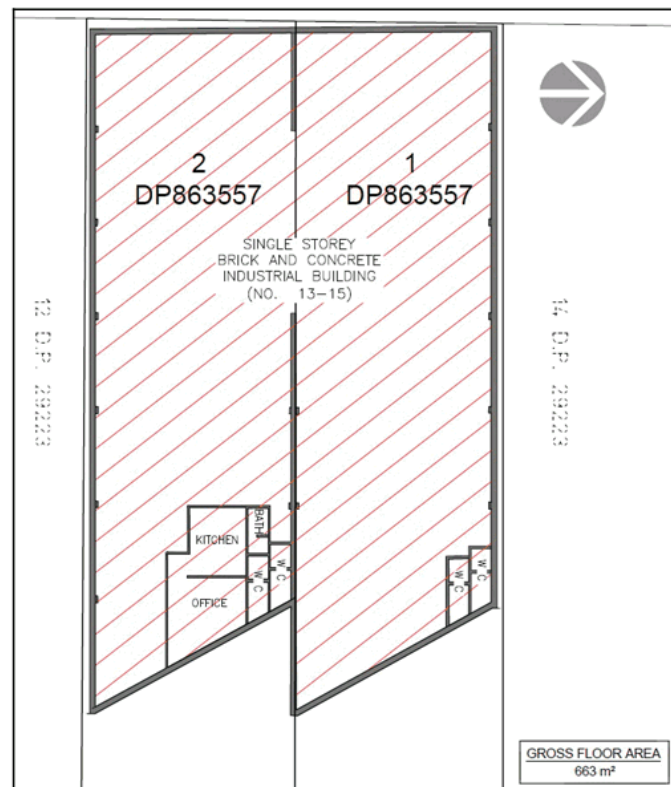


Figure 5: Diagram showing the Gross Floor Area of the existing building on the Site.

3. PROPOSED DEVELOPMENT

The proposed development is "light industry" which is defined in the Strathfield Local Environmental Plan 2012 (**LEP 2012**) as:

light industry means a building or place used to carry out an industrial activity that does not interfere with the amenity of the neighbourhood by reason of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit or oil, or otherwise, and includes any of the following:

- (a) high technology industry,
- (b) home industry.

The proposal is to modify the use of the Dunlop Street facility to enable the wet out of a cured in place pipe (**CIPP**) liner to be undertaken within the existing building as well as storage of the plant and materials required to undertake the liner wet out.

A site layout plan has been prepared by the Applicant, an extract from which is provided as **Figure 6**.

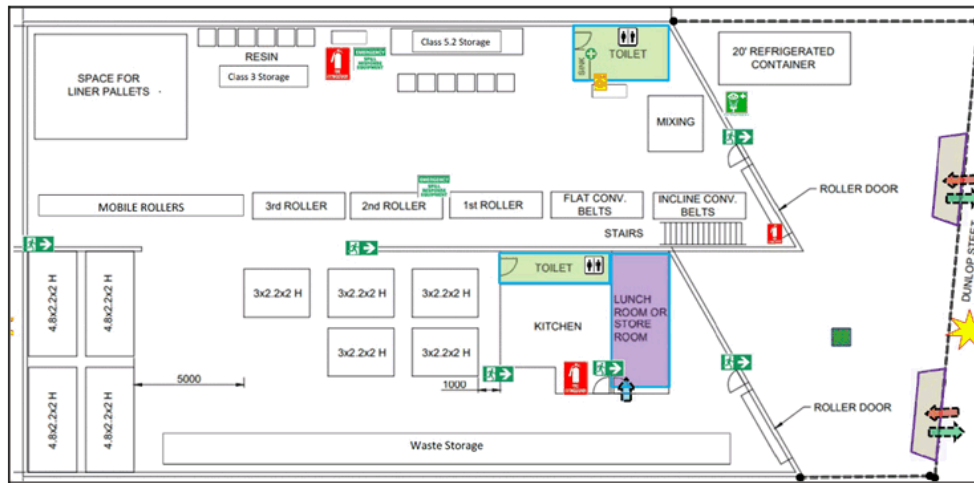


Figure 6: Extract from the site layout plan for the proposed development.

Storage of resins are in designated storage areas with separation zones identified. A dangerous goods storage approval has been issued for the storage of materials on the Site, a copy of which is submitted with the development application.

Deliveries will be via flat bed truck and semi trailers. Deliveries are unloaded by hand as well as by forklift. The forklift will be hired and floated to the Site for works and floated out at the end of the wet out.

Items moved into and within the building are the liner pallet, resin drums and ancillary items.

On-site refrigeration keeps the resins cool.

A refrigeration truck would be on site during the wet out works which will reverse to the roller door adjacent to the wet out roller bed to enable the wet out liner to be loaded into the back of the refrigeration truck.

The felt liner is selected based on the diameter of the host pipe to be lined. The liner is taken from the pallet and a length suitable for the lining is selected and the liner is cut using a Stanley knife.

Resins are then poured together into a mixing vat, with "perkonox" and "trigonox" added and mixed with an electric mixer within the building until the consistency is correct. If it is a large liner, the mixing generally occurs the day prior to the wet out and the mixed resins are stored in the refrigeration container.

The liner is then manually pulled across the wet out roller bed and attached at the top. A transfer hose is then placed within a void in the liner and the mixed resins are then pumped into

the liner to saturate the liner with resin.

The resin impregnated liner is loaded by hand into the refrigeration truck once the wet out process is completed.

The building can be closed and the refrigerated truck closed and left idling until the next day to enable delivery to site, however, the refrigerated truck is generally taken off site overnight.

Spent drums are mixed together within the building to enable the leftover resins to cure and solidify. Once cured, the spent drums can be removed as general solid waste to an accredited waste facility.

The CIPP lining crew consists of 6 to 7 personnel as well as a supervisor. During Audit there may be another 3 to 4 office staff supervising the process.

Hours of work are predominately during the day. Construction hours are 7:00am to 5:00pm. Larger liners require overtime to be worked and works may continue into the night.

Once wetting out of a liner commences, the process must be completed otherwise the liner would become unusable.

4. ENVIRONMENTAL IMPACT OF PROPOSED DEVELOPMENT

This section provides an analysis of the proposed development in terms of its impact on the environment. Specific reference is made to the relevant heads of consideration contained in Section 4.15 of the *Environmental Planning and Assessment Act 1979*. The relevant Section 4.15 head is shown in italics with comments as appropriate.

- Section 4.15(1)(a)(i) (a) *the provisions of:-*
- (i) *any environmental planning instrument.*

4.1 State Environmental Planning Policy No.55 - Remediation of Land

State Environmental Planning Policy No.55 - Remediation of Land (SEPP 55) aims:

... to promote the remediation of contaminated land for the purpose of reducing the risk of harm to human health or any other aspect of the environment.

Clause 7 of SEPP 55 states:

7. (1) *A consent authority must not consent to the carrying out of any development on land unless:*
- (a) *it has considered whether the land is contaminated, and*
 - (b) *if the land is contaminated, it is satisfied that the land is suitable in its contaminated state (or will be suitable, after remediation) for the purpose for which the development is proposed to be carried out, and*
 - (c) *if the land requires remediation to be made suitable for the purpose for which the development is proposed to be carried out, it is satisfied that the land will be remediated before the land is used for that purpose.*
- (2) *Before determining an application for consent to carry out development that would involve a change of use on any of the land specified in subclause (4), the consent authority must consider a report specifying the findings of a preliminary investigation of the land concerned carried out in accordance with the contaminated land planning guidelines.*
- (3) *The applicant for development consent must carry out the investigation required by subclause (2) and must provide a report on it to the consent authority. The consent authority may require the applicant to carry out, and provide a report on, a detailed investigation (as referred to in the contaminated land planning guidelines) if it considers that the findings of the preliminary investigation warrant such an investigation.*
- (4) *The land concerned is:*
- (a) *land that is within an investigation area,*
 - (b) *land on which development for a purpose referred to in Table 1 to the contaminated land planning guidelines is being, or is known to have been, carried out,*
 - (c) *to the extent to which it is proposed to carry out development on it for residential, educational, recreational or child care purposes, or for the purposes of a hospital land:*
 - (i) *in relation to which there is no knowledge (or incomplete knowledge) as to whether development for a purpose referred to in Table 1 to the contaminated land planning guidelines has been carried out, and*
 - (ii) *on which it would have been lawful to carry out such*

development during any period in respect of which there is no knowledge (or incomplete knowledge).

Figure 7 is an extract from a 1943 aerial photograph. At that time, the Site and the surrounding area was rural land and a quarry prior to development as an industrial area and, as such, is unlikely to contain any contamination.



Figure 7: Extract from a 1943 aerial photograph with the Site highlighted in yellow. © Six Maps

Pursuant to **sub-clause 7(1)**, the Council can be satisfied that the proposed development is suitable for the Site.

4.2 Strathfield Local Environmental Plan 2012 (LEP 2012)

The Site is zoned IN1 General Industrial pursuant to LEP 2012.

An extract from the LEP 2012 Map is at **Figure 8**.

The proposed development is for use of the Site as a Light Industry which is permissible, with the consent of the Council, in the zone.

An extract from the LEP 2012 Table is at **Table 1**.

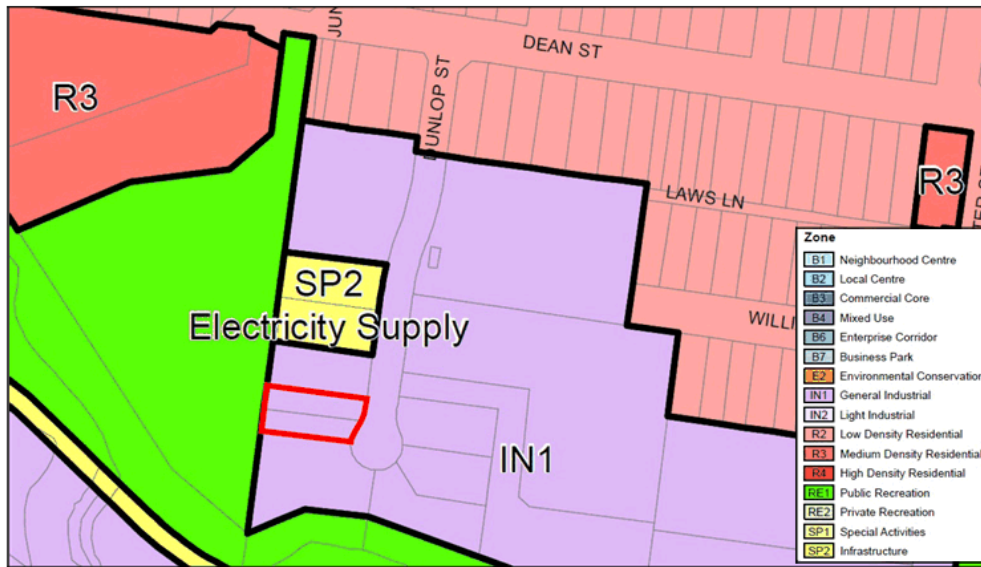


Figure 8: Extract from the LEP 2012 Map with the Site outlined in red.

Table 1: Extract from the Land Use Table of LEP 2012

IN1 General Industrial Zone	
Permitted without consent	Nil
Permitted with consent	Agricultural produce industries; Animal boarding or training establishments; Boat building and repair facilities; Car parks; Depots; Environmental protection works; Freight transport facilities; Garden centres; General industries; Hardware and building supplies; Industrial retail outlets; Industrial training facilities; Kiosks; Landscaping material supplies; Light industries ; Neighbourhood shops; Places of public worship; Plant nurseries; Recreation areas; Roads; Sex services premises; Signage; Storage premises; Take away food and drink premises; Timber yards; Transport depots; Truck depots; Vehicle body repair workshops; Vehicle repair stations; Veterinary hospitals; Warehouse or distribution centres; Wholesale supplies
Prohibited	Any development not specified in item 2 or 3

Sub-clause 2.3(2) of LEP 2012 states:

- (2) The consent authority must have regard to the objectives for development in a zone when determining a development application in respect of land within the zone.

The objectives of the IN1 General Industrial zone are:

- To provide a wide range of industrial and warehouse land uses.
- To encourage employment opportunities.

- *To minimise any adverse effect of industry on other land uses.*
- *To support and protect industrial land for industrial uses.*
- *To minimise fragmentation of valuable industrial land, and provide large sites for integrated and large floorplate activities.*

The proposed development would:

- provide for a light industrial development with commensurate generation of employment,
- minimise the effect of the proposed development on other land uses and,
- ensure that a high level of amenity is achieved and maintained in the locality.

The proposal would be consistent with the objectives of the IN1 General Industrial zone.

Clause 4.4 deals with floor space ratio and states, in part:

- (2) *The maximum floor space ratio for a building on any land is not to exceed the floor space ratio shown for the land on the Floor Space Ratio Map.*

The Floor Space Ratio Map indicates that the floor space ratio applying to the Site is 1:1.

The survey report provided in **Attachment 2**, and extract form which is at **Figure 5**, shows that the Gross Floor Area of the built form on the Site is 663m². With a site area of 978.3m² the existing built form on the Site has a floor space ratio of 0.68:1.

Clause 6.4 relates to services and states, in part:

- (1) *Development consent must not be granted for development unless the consent authority is satisfied that any of the following services that are essential for the development are available or that adequate arrangements have been made to make them available when required:*
 - (a) *the supply of water,*
 - (b) *the supply of electricity,*
 - (c) *the disposal and management of sewage,*
 - (d) *the disposal and recycling of waste,*
 - (e) *stormwater drainage or on-site conservation,*

(f) *suitable vehicular access.*

The Site is connected to reticulated water, electricity, sewerage services and Council's stormwater drainage system. The Site contains suitable vehicular access as demonstrated in the Traffic and Parking Report submitted with the development application.

The Site will be serviced by a recognised waste management contractor in addition to the Council service provided.

There are no other provisions of Strathfield LEP 2012 which are considered relevant to the proposed development.

Section 4.15(1)(a)(ii) (a) *the provisions of:*
(ii) *any proposed instrument.*

There are no proposed instruments affecting the proposed development of which the writer is aware.

Section 4.15(1)(a)(iii) (a) *the provisions of:*
(iii) *any development control plan.*

4.3 Strathfield Development Control Plan 2005

Strathfield Development Control Plan 2005 (**DCP 2005**) is used by Council to assess development applications submitted under Strathfield LEP 2012.

In assessing a development application, Council is to have regard to **Sub-clause 4.15(3A)** of the *Environmental Planning and Assessment Act 1979* which states:

(3A) ***Development control plans***

If a development control plan contains provisions that relate to the development that is the subject of a development application, the consent authority:

(a) *if those provisions set standards with respect to an aspect of the development and the development application complies with those standards-is not to require more onerous standards with respect to that aspect of the development, and*

- (b) *if those provisions set standards with respect to an aspect of the development and the development application does not comply with those standards-is to be flexible in applying those provisions and allow reasonable alternative solutions that achieve the objects of those standards for dealing with that aspect of the development, and*
- (c) *may consider those provisions only in connection with the assessment of that development application.*

In this subsection, standards include performance criteria.

Part D of DCP 2005 deals with Industrial Development and states:

The purpose of Part D is to ensure developers and property owners submit industrial development proposals which are attractive and sympathetic to the streetscape, are appropriate for the surrounding built and natural environments, have a minimum impact on surrounding non-industrial uses and are functionally and economically viable.

The following provisions of DCP 2005 relate to the proposed development.

4.3.1 Parking and Manoeuvring

The objectives for Parking are:

- a. *To ensure sufficient car parking spaces are provided on-site for employees and visitors;*
- b. *To ensure the effective design of car parking areas;*
- c. *To encourage the provision of parking areas that will integrate with proposed the building(s) and be suitably landscaped to reduce large expanses of hard paving; and*
- d. *To ensure car parking areas are accessible for persons with a disability and safe for all pedestrians to use.*

The objectives for Access and Manoeuvring are:

- a. *To ensure that provision is made for safe vehicular ingress and egress having regard to the nature of vehicles likely to patronise the site;*
- b. *To ensure satisfactory on-site manoeuvring for vehicles, including the loading/unloading of goods;*

- c. *To minimise potential for congestion or hazard on adjoining roads at points of ingress/egress;*
- d. *To ensure that traffic generated by industrial development does not adversely affect local or regional traffic movements; and*
- e. *To ensure that any traffic generated by the development will not impact unreasonably upon the amenity of any residential areas in the vicinity.*

In order to determine the impact of the proposed development with regard to traffic and parking, a Traffic and Parking Statement has been prepared by PDC Consultants, a copy of which is at **Attachment 3**. With regard to parking, PDC state:

The site is served by two (2) separate entry/ exit driveways onto Dunlop Street, both having widths of approximately 4.0 metres. These provide access to the 3 car parking spaces and 2 on-site loading bays located at the front of the site.

Car Parking

The Strathfield DCP 2005 stipulates a rate of 1.0 car space per 55m² GFA or 1.0 space for every 2 employees, whichever provides the greater number of spaces, for industry developments. With 663m² industry GFA and an average of 8 staff on-site at any one time, it is evident that the development is required to provide the greater of the two below requirements:

- *12 car parking spaces, based on the GFA; or*
- *4 car parking spaces, based the number of on-site employees.*

It is evident from the above that the proposed development requires a minimum of 12 car parking spaces under the strict application of the Strathfield DCP 2005. However, it is critical to note that the development will operate with an average of 8 staff on-site at any one time and accordingly, would only generate a maximum parking requirement for 8 car parking spaces, or 1 space per staff member. Indeed, it is highly unlikely that each individual staff would drive their own private vehicle for journeys to/from the site and accordingly, the actual parking demand would likely be in the order of 6-7 car parking spaces.

.... the proposed development will provide a total of 5 on-site car parking spaces at the front of the site and will therefore require the reliance of on average, 1-2 on on-street spaces along Dunlop Street.

A 'spot' on-street parking survey was conducted on 4th April 2018 between the hours of 12-2pm to determine existing parking demands. This period is considered to coincide with the on-street peak, given that all of the neighbouring businesses in Dunlop Street would be in operation and hence would provide a 'worst-case' basis for determining existing on-street parking demands and spare capacity. The survey confirmed that the Dunlop Street

cul-de-sac has a total capacity of 25 on-street car parking spaces. A total of 17 cars was observed to park within the cul-de-sac during our survey, with a minimum of 8 car parking spaces available for additional parking.

It is evident from the above that the parking demand for 1-2 car parking spaces would be readily accommodated within Dunlop Street. Furthermore, even in the unlikely event that the development was to generate a peak parking demand for 8 parking spaces (or 1 space per staff member), the reliance on three (3) spaces by the development would be readily accommodated on-street with minimal impacts.

The proposed car parking provision is therefore considered acceptable.

Accessible Car Parking

The Strathfield DCP 2005 does not stipulate a rate for the provision of accessible car parking and accordingly, reference was made to the Disability (Access to Premises – Buildings) Standards 2010 (Disability Standards 2010) to determine the minimum number of accessible car parking spaces required by the development. The Disability Standards 2010 stipulates a rate of 1.0 space per 100 car parking spaces or part thereof for Class 8 Buildings. With 7 car parking spaces, it is evident that the development is required to provide a single accessible car space.

In response, the Car Parking Arrangement Plan included in Attachment 2 [of the PDC Statement] demonstrates that the development will provide a single accessible car space and therefore satisfies the requirements of the Disability Standards 2010.

Service Vehicle Parking

The Strathfield DCP 2005 does not stipulate a rate for the provision of service vehicle parking and accordingly, reference was made to the RMS Guide to determine the minimum number of service vehicle parking spaces required by the development. The RMS Guide stipulates a service vehicle rate of 1.0 space per 100m² GFA for industry developments. With 663m² industry GFA, it is evident that the development is required to provide a single service vehicle bay. In response, the development proposes to retain the 2 existing service vehicle bays located at the front of the site and therefore satisfies the minimum requirements of the RMS Guide.

Service vehicles up to and including a 12.5m heavy rigid vehicle (HRV) via the northern access driveway and an 8.8m medium rigid vehicle (MRV) via the southern access driveway will continue to reverse into the site / loading bays and exit the site in a forward direction, in accordance with Clause 3.2. of AS 2890.2-2002. Whilst the development does not incorporate any change to the existing service vehicle parking arrangements, changes would be made to the car parking area. Accordingly, it was considered appropriate to undertake swept path analysis of the servicing arrangements to confirm that a 12.5m HRV and 8.8m MRV would still be able to satisfactorily enter / exit the site. The analysis results are included in Attachment 3 [of the PDC Statement], confirming compliance and

that a 12.5m HRV and 8.8m MRV would still be able to satisfactorily enter and exit the site.

4.3.2 Site Drainage and Water Management

The objective for Site Drainage and Management is:

To ensure that potable water use and stormwater quantities are reduced whilst stormwater quality is improved.

The Site is in an established industrial area where suitable stormwater drainage facilities are provided to ensure the integrity of the Council's stormwater system.

4.3.3 Utilities

The objective for Utilities is:

To ensure a development is satisfactorily serviced by all utilities.

The Site is connected to reticulated water, electricity, sewerage services and Council's stormwater drainage system.

4.3.4 Air, Noise and Water Pollution

The objective for Air, Noise and Water Pollution is:

To ensure industrial developments do not create a pollution problem by the discharge of an unacceptable level of air, noise and/or water emissions.

The DCP guidelines states:

The emission of any air impurities including offensive odours, the discharge of any waste into any waters or the emission of noise associated with any development shall not contravene the Protection of the Environment Operations Act 1997.

Acoustic Impact

In order to assess the acoustic impact of the proposed development, a report has been prepared by Wilkinson Murray Pty Limited, copies of which have been submitted with the development

application. Wilkinson Murray state:

The noise impact assessment evaluates potential noise impacts associated with the operation of the facility in accordance with the NSW EPA's Noise Policy for Industry (NPfI).

To establish existing noise levels at the most potentially affected residential receivers, unattended noise monitoring was conducted between 2 May and 14 May 2018 at 1 Gregory Street, Strathfield South.

Operational noise emissions from the site have been modelled using the CONCAWE noise prediction algorithm, as implemented in the "CadnaA" acoustic software. Factors that are addressed in the noise modelling are:

- Equipment noise level emissions and locations;*
- Shielding from ground topography and structures;*
- Noise attenuation due to geometric spreading;*
- Ground absorption; and*
- Atmospheric absorption.*

Wilkinson Murray assessed the impact of the proposed development and concluded:

... the predicted noise levels, due to the operation of the proposal, exceed the established noise trigger levels by up to 9 dBA at the most potentially affected residential receiver and by up to 3 dBA at the most potentially affected industrial receiver.

With regard to sleep disturbance:

the predicted LA_{max} noise levels at the most potentially affected receiver exceed the maximum noise trigger level by up to 12 dBA.

Wilkinson Murray recommend the following mitigation measures:

An acoustic barrier is recommended along the northern boundary of the site to reduce both $LA_{eq, 15_{min}}$ and LA_{max} noise levels at nearby sensitive receivers. The barrier should be approximately 4 metres high and should extend from near the eastern façade of the building to the eastern site boundary so as to block the "line of sight" between the refrigeration unit on the truck and receivers to the north, as depicted in Figure 5-1 [of the Wilkinson Murray report]. It is noted that the industrial building to the north of the site will provide beneficial shielding and will reduce the length of the required barrier. However, if the adjacent building were to be removed in future development on that site, the barrier may require an extension to maintain its effectiveness.

The barrier should be constructed of a material with a weighted sound reduction index (Rw) of 25 or greater; such as hebel, timber/plywood or 4 kg/m² loaded vinyl. Due to the sporadic nature of the operations, the barrier could be a moveable structure and would

only need to be in place while a refrigerated truck is operating on the site, such as during the wet-out process.

.... the predicted LA_{eq} , 15min LA_{max} operational noise levels, with the recommended mitigation, comply with the established trigger levels.

Air Quality Impact

In order to assess the air quality impact of the proposed development, a report has been prepared by Wilkinson Murray Pty Limited, copies of which have been submitted with the development application. Wilkinson Murray state:

Wilkinson Murray has been commissioned by Abergeldie Complex Infrastructure (Abergeldie) to undertake an operational Air Quality Impact Assessment (AQIA) for the Proposal. This AQIA has been prepared in general accordance with the Approved Methods for the Modelling and Assessment of Air Pollutants in New South Wales (the Approved Methods).

Air quality criteria are benchmarks set to protect the general health and amenity of the community in relation to air quality.

During a trial of the wet-out process, an Occupational Chemical Exposure Assessment was conducted by Statewide Air and Noise. The assessment report, issued on 19 September 2017, identified three chemicals of interest:

- Styrene;
- Toluene; and,
- Acetone.

The NSW EPA's Approved Methods for the Modelling and Assessment of Air Pollutants in New South Wales (the Approved Methods) sets out applicable impact assessment criteria for a wide range of air pollutants. Depending upon the pollutant, the impact assessment criteria are designed to avoid:

- *Health impacts from long term exposure to common pollutants that tend to accumulate from multiple sources;*
- *Health impacts from short term exposure to toxic air pollutants; and,*
- *Amenity (i.e. nuisance) impacts from odorous pollutants.*

Both styrene and toluene are classified in the Approved Methods as "individual odorous air pollutants". They have 1-hour average impact assessment criteria of 0.12 mg/m^3 and 0.36 mg/m^3 , respectively. The impact assessment criteria for odorous pollutants are to apply to the 99.9th percentile of dispersion model predictions in a Level 2 assessment and are to apply at the nearest existing or likely future off-site sensitive receptor. In the case

of odour impacts, sensitive receptor locations are typically taken as those locations where people live.

Land uses other than residential, e.g. industrial, commercial, recreation, etc., are typically not considered sensitive receptors with respect to odour impacts. Notwithstanding, NSW legislation prohibits emissions that cause offensive odour to occur at any off-site receptor. Offensive odour is evaluated in the field by authorised officers, who are obliged to consider the odour in the context of its receiving environment, frequency, duration, character and so on and to determine whether the odour would unreasonably interfere with the comfort and repose of the normal person.

Acetone is classified in the Approved Methods as an “individual toxic air pollutant”. It has an impact assessment criterion of 22 mg/m³ for a 1-hour average concentration, which is to be applied at and beyond the facility boundary. For a Level 2 impact assessment, such as that conducted herein, the impact assessment criterion is to apply to the 99.9th percentile of the dispersion model predictions.

Wilkinson Murray has identified the following impacts associated with the proposed development:

... the predicted ground level concentrations of toluene at sensitive receivers are negligible and comply with the impact assessment criterion.

... the predicted ground level concentrations of acetone comply with the impact assessment criterion at all receivers, including on the site boundary.

The predicted ground level concentration of styrene at the most potentially affected residential receptor is 0.22 mg/m³, almost twice the impact assessment criterion. This predicted level indicates the potential for residences to notice some level of odour, however they would be unlikely to be able to characterise the odour or describe it as annoying. It is considered unlikely that styrene emissions from the site would cause nuisance at residential receptors.

At the most affected point within the playground to the west of the site, the predicted styrene concentration is 0.69 mg/m³, approximately six times the impact assessment criterion. This predicted level indicates that people in the playground may detect the odour and may be able to describe it – most likely as a “sweet” or “floral” odour. However, it is unlikely that the odour would be regarded as offensive or cause a nuisance. Additionally, the highest predicted styrene concentrations are likely to occur late in the night or early in the morning, i.e. before dawn. It is most likely that the park and playground would be vacant during these times.

The highest predicted styrene concentration at industrial receptors is 2.02 mg/m³, almost 20 times the impact assessment criterion. Based on this predicted level, there is potential for individuals to detect styrene odours at nearby industrial premises, particularly if these premises are in use late at night and early in the morning. However, for the reasons

described in Section 4.2.1 [of the Wilkinson Murray Report], it is unclear whether the odour would cause nuisance or be regarded as offensive.

Wilkinson Murray recommend the following air quality mitigation measures:

Reducing pollutant concentrations at off-site receptors is generally achieved by one or more of the following means:

- *Reducing the amount of pollutants emitted;*
- *Better controlling their emissions and providing for better dispersion i.e. mixing; or*
- *Managing site operations to limit emissions during unfavourable meteorological conditions.*

As noted in Section 6.1 [of the Wilkinson Murray Report], pollutant emissions via the roller door are uncontrolled and regarded as fugitive emissions. Emissions from fugitive sources generally have very poor dispersion as they are released close to the ground and with no significant velocity. Pollutants emitted via a stack have significantly better dispersion as they can be released at height, and with velocity. The velocity of the air leaving the stack results in "plume rise" which further aids in dispersion. Directing pollutants through a stack can reduce their concentrations at receptors by a factor of more than 100.

Installing a mechanical ventilation system in the building that draws air in through the roller door and directs it out via a stack on the roof would eliminate fugitive emissions and significantly reduce ground level concentrations of pollutants at nearby receptors.

The installation of mechanical ventilation also provides the opportunity to add control devices that reduce the amount of pollutants emitted. In the case of organic compounds such as styrene, activated carbon adsorbers can reduce emissions by more than 99%.

In some cases, it is possible to control activities on a site so that pollutant emissions are avoided when the prevailing weather conditions are likely to lead to the greatest levels of impact. In the case of odour impacts, the greatest impacts are typically experienced during calm conditions, especially at night under temperature inversions, and when there are light winds blowing from the source to the sensitive receptors. In the case of the Proposal, the most potentially affected sensitive receptors are the residences to the north of the site. Therefore, potential odour impacts would be most likely to occur during still conditions or when there are light winds with a southerly component.

Recommendations

The preceding discussion has identified the potential for odours from styrene emissions to be noticeable at nearby industrial premises to the extent that nuisance may occur.

Measures to mitigate these impacts have been identified.

However, implementing these measures can be costly and may not be warranted since it is unclear whether odours from the Proposal would cause a nuisance. Therefore, it is recommended that the Proposal is approved and allowed to operate without implementing odour mitigation measures, subject to the following:

- *During three wet-out processes, field odour surveys are conducted with a “Nasal Ranger” or similar device to confirm that odours from the Proposal are acceptable at the nearby residential receptors. The surveys should be done as early as practicable after the commencement of operations but should also be conducted with a view to capturing the worst-case impacts and should therefore be done during calm or light southerly winds.*
- *In the event that the odour surveys indicate potential odour impacts at sensitive receptors, or odour complaints are received, and the Proposal is confirmed to be causing nuisance, the site should immediately limit operations such that wet-outs are only conducted when forecast winds have a northerly component.*
- *Prior to operations returning to “normal”, a suitable mechanical ventilation system should be designed and installed, subject to relevant planning approvals.*

It would be prudent for the above items to be documented in an Operational Odour Management Plan (OOMP), prepared by a suitably qualified air quality consultant.

Water Pollution

The Applicant has a proven spill management procedure in place, a copy of which is at **Attachment 3**. With that procedure in place at the Site, it is considered unlikely that water pollution off-site would occur.

Section 4.15(1)(a)(iiia)

(a) *the provisions of:*

(iiia) *any planning agreement or draft planning agreement.*

There are no planning agreements or draft planning agreements relating to the proposed development.

Section 4.15(1)(a)(iv)

(a) *the provisions of:*

(iv) *the regulations.*

There are no specific requirements of the *Environmental Planning and Assessment Regulation 2000*.

- Section 4.15(1)(a)(v) (a) *the provisions of:*
- (v) *any coastal management plan.*

Not applicable.

- Section 4.15(1)(b) (b) *the likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality.*

Impacts to the natural and built environment

The proposed development would have no adverse impact on the built environment.

Traffic Impact

PDC Consultants have prepared a traffic impact assessment of the proposed development (refer **Attachment 3**). The PDC assessment states:

The RMS Guide stipulates a trip generation rate of 1.0 trip / 100m² GFA for industry / factory developments during the PM peak period. This rate has also been adopted for the AM peak period as it is considered that both the AM and PM peak period traffic generations would be comparable. Application of the above rate to the 663m² GFA results in the following peak period traffic generation:

- *7 vehicle trips / hour (6 in, 1 out), during the AM peak period*
- *7 vehicle trips / hour (1 in, 6 out), during the PM peak period*

Comparing the above to the historical use as a warehouse, it is evident that the development would result in the following net increase in traffic generation:

- *4 vehicle trips / hour (4 in, 0 out), during the AM peak period*
- *4 vehicle trips / hour (0 in, 4 out), during the PM peak period*

As discussed above, the proposed development will result in a net increase of 4 vehicle trips / hour during both the AM and PM peak periods. This equates to only one (1) additional vehicle trip every 15 minutes which will have no material impact on the performance of the external road network or key intersections in the locality and accordingly, no external improvements will be required to facilitate the development.

Furthermore, computer modelling techniques available to analyse intersection performances are not sensitive to such small changes in traffic volumes and hence, such an assessment is not considered to be required. The traffic impacts of the proposed development are therefore considered acceptable.

Social Impacts

There would be a significant, positive social impact resulting from the proposed development in that a light industrial use would be established in the Strathfield South industrial area with commensurate employment generation.

Economic Impact

The economic impact associated with the proposed development would be that employment opportunities would be available during the operation of the proposed development.

Section 4.15(1)(c) (c) *the suitability of the site for the development.*

The Site is within the IN1 General Industrial zone.

The proposed development is permitted in the zone and the Site is suitable for the proposed development.

Section 4.15(1)(d) (d) *any submissions made.*

Not applicable.

Section 4.15(1)(e) (e) *the public interest.*

The public interest is an overarching requirement, which includes the consideration of matters discussed in this SEE. Implicit to the public interest is the achievement of future built outcomes adequately responding to and respecting the future desired outcomes expressed in environmental planning instruments and development control plans.

The proposed development is considered to have satisfactorily addressed Council's criteria and would provide a development outcome that, on balance, would result in a positive impact for the community.

Suitably conditioned as recommended by the Traffic, Acoustic and Air Quality consultants, the proposed development would have negligible impact on the environment and would be

consistent with Strathfield LEP 2012.

The proposed development would utilise the existing built form on the Site and provide employment in the locality consistent with the IN1 General Industrial zone objectives.

Accordingly, it is considered that approval of the proposal would be in the public interest.

5. CONCLUSION

It is proposed to utilise the existing built form on the Site for a Light Industry.

It has been demonstrated that, with appropriate mitigation measures relating to the acoustic impact and air quality impact of the proposed development, the proposal would have an acceptable impact on the environment.

The proposed development would have a positive impact in that it would utilise the existing built form on the Site and provide for employment.

The Site is located within the IN1 General Industrial zone and the proposed development is permitted with the consent of the Council.

The development has planning merit and should be approved by the Council.

Attachment 1
Site Survey Plan

NOTES

ANY ELECTRONIC FILE IS PROVIDED WITHOUT WARRANTY AND SHOULD BE USED ONLY IN CONJUNCTION WITH THE SUPPLIED PAPER COPY OF THIS PLAN.

LIMITED BOUNDARY SURVEY HAS BEEN UNDERTAKEN. BEARINGS, DIMENSIONS AND AREA ARE FROM TITLE ONLY AND ARE SUBJECT TO CONFIRMATION BY FULL BOUNDARY SURVEY.

SERVICES SHOWN ARE INDICATIVE ONLY. POSITIONS ARE BASED ON SURFACE INDICATORS LOCATED DURING FIELD SURVEY. CONFIRMATION OF THE EXACT POSITION SHOULD BE MADE TO THE RELEVANT AUTHORITIES PRIOR TO ANY EXCAVATION WORK. OTHER SERVICES MAY EXIST WHICH ARE NOT SHOWN.

LEVELS ARE BASED ON AUSTRALIAN HEIGHT DATUM (AHD) USING SSM134373 WITH RL OF 17.62m (ACC D4).

ORIGIN OF CO-ORDINATES SSM134373 WITH MGA CO-ORDINATE VALUES OF E322,484,525 N6,247,948,567.

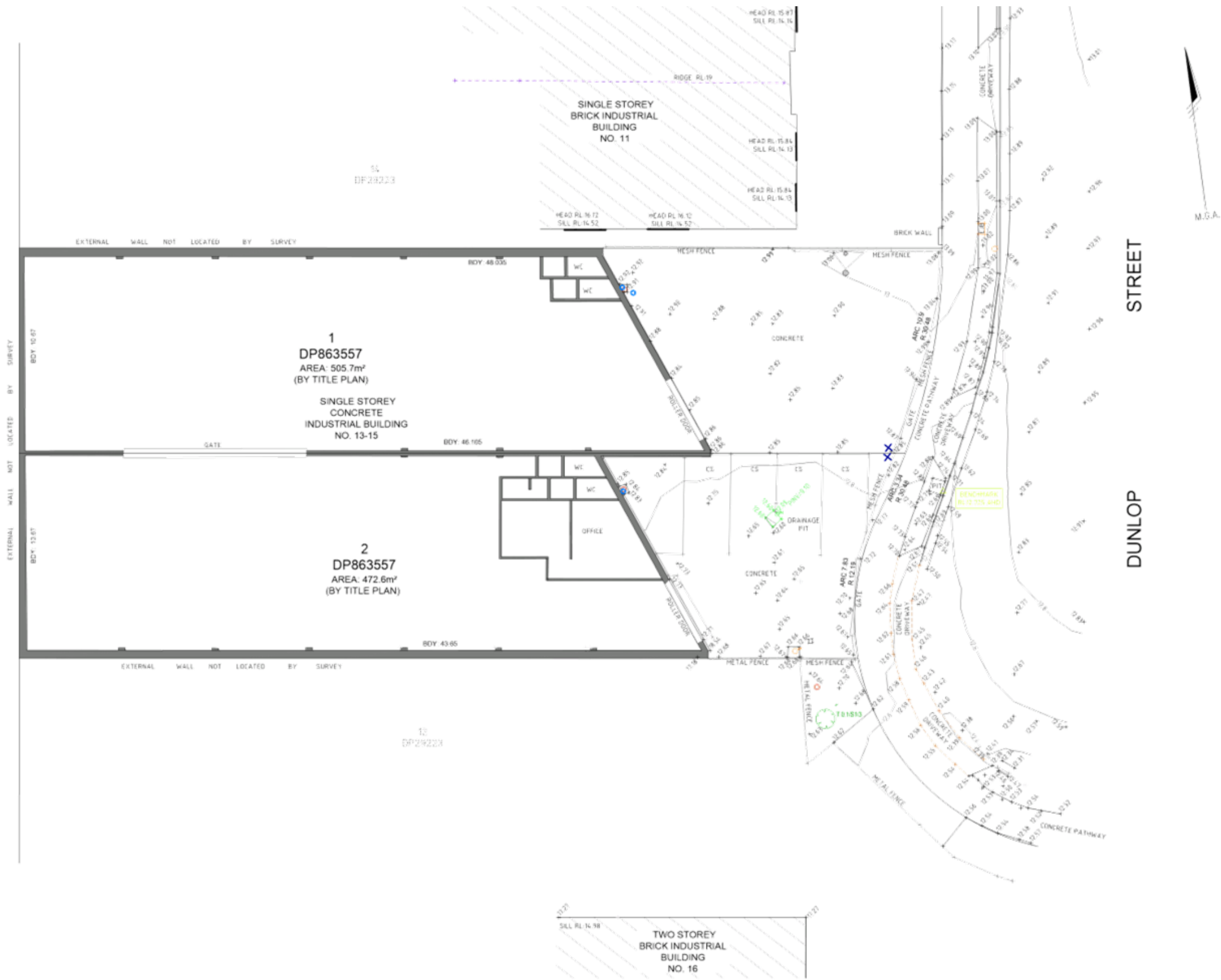
RIDGE, EAVE & GUTTER HEIGHTS HAVE BEEN OBTAINED BY AN INDIRECT METHOD AND ARE ACCURATE TO ±0.05m.

ADJOINING DWELLING, BUILDING HAVE BEEN PLOTTED FOR DIAGRAMMATIC PURPOSES ONLY.

CONTOURS ARE AN INDICATION OF LANDFORM AND SHOULD NOT BE TAKEN IN PREFERENCE TO SPOT LEVELS SHOWN.

CONTOUR INTERVAL 0.2m.

- LEGEND**
- WATER TAP
 - × WATER VALVE
 - GRATE
 - POWER POLE LIGHT
 - POWER POLE
 - GAS VALVE
 - SEWER MANHOLE
 - DRAINAGE PIP INVERT
 - T.O. 15810
 - TREE
 - SPREAD 1:0
 - TRUNK DIAMETER 0.1
 - HEIGHT 3.0
 - CS DENOTES CAR SPACE
 - WC DENOTES WATER CLOSET



<p>PROJECT SCALE: 1:100</p> <p>DATE: 12/06/2018</p> <p>DESIGNER: SP MS MS</p> <p>CLIENT: J&J MS MS</p> <p>PROJECT: INITIAL VERSION</p>		<p>PROJECT SCALE: NA @ A1</p> <p>DATE: 12/06/2018</p> <p>DESIGNER: J&J MS MS</p> <p>CLIENT: J&J MS MS</p> <p>PROJECT: INITIAL VERSION</p>		<p>COORDINATES: MGA</p> <p>ORIGIN: SSM134373</p> <p>SCALE: AHD</p> <p>ORIGIN: SSM134373</p>		<p>PROJECT: ABERGELDIE</p> <p>DATE OF SURVEY: 05/06/2018</p> <p>DATE OF PLAN: 08/06/2018</p> <p>DATE LAST SHOWN: 12/06/2018</p> <p>DATE APPROVED: 12/06/2018</p>		<p>TITLE: CONTOUR & DETAIL SURVEY</p> <p>13-15 Dunlop Street, Strathfield South</p> <p>PROJECT NO: PR140703 (S-001) (dwg)</p>		<p>PROJECT NO: PR140703</p> <p>DATE: 12/06/2018</p> <p>SCALE: 1:100</p> <p>SHEET 1 OF 1</p>	
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Attachment 2
Internal Site Survey



**Sydney West Office**

Suite 404, Level 4, 29-31 Lexington Drive, PO Box 6843, Baulkham Hills NSW 2153

T +61 2 8884 6900 F +61 2 8884 6999 E sydney.west@rpsgroup.com.au W rpsgroup.com.au

Our Ref: PR140703
Date: 24TH May 2018

Attn:
JAMES SIDWELL
Abergeldie Complex Infrastructure
PO Box 3019
Regents Park NSW 2143

Via: email jsidwell@abergeldie.com

Dear James,

RE: GROSS FLOOR AREA (GFA)
PTY: 13-15 DUNLOP STREET, STRATHFIELD SOUTH

This report schedules the results of our Gross Floor Area survey from site inspection for the abovementioned property.

The areas have been determined in accordance with and under our interpretation of Strathfield Local Environmental Plan 2012. Definition outlined as follows:

gross floor area means the sum of the floor area of each floor of a building measured from the internal face of external walls, or from the internal face of walls separating the building from any other building, measured at a height of 1.4 metres above the floor, and includes:

- (a) the area of a mezzanine, and
- (b) habitable rooms in a basement or an attic, and
- (c) any shop, auditorium, cinema, and the like, in a basement or attic,

but excludes:

- (d) any area for common vertical circulation, such as lifts and stairs, and
- (e) any basement:
 - (i) storage, and
 - (ii) vehicular access, loading areas, garbage and services, and
- (f) plant rooms, lift towers and other areas used exclusively for mechanical services or ducting, and
- (g) car parking to meet any requirements of the consent authority (including access to that car parking), and
- (h) any space used for the loading or unloading of goods (including access to it), and
- (i) terraces and balconies with outer walls less than 1.4 metres high, and
- (j) voids above a floor at the level of a storey or storey above.



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RPS Australia East Pty Ltd ABN 44 140 292 762 A member of the RPS Group Plc

Areas have been calculated from site inspection/measurements.

The schedule and diagrams attached highlight the limit of the GFA in relation to the building footprint, while we summarise the total GFA as follows:

- **GFA – 663 square metres**

Should you have any questions or if we can be of further assistance please do not hesitate to contact us.

Yours sincerely

A handwritten signature in blue ink, appearing to read 'PYRUZ', with a long horizontal flourish extending to the right.

SAM PYRUZ (Surveyor)
RPS Group

**Sydney West Office**

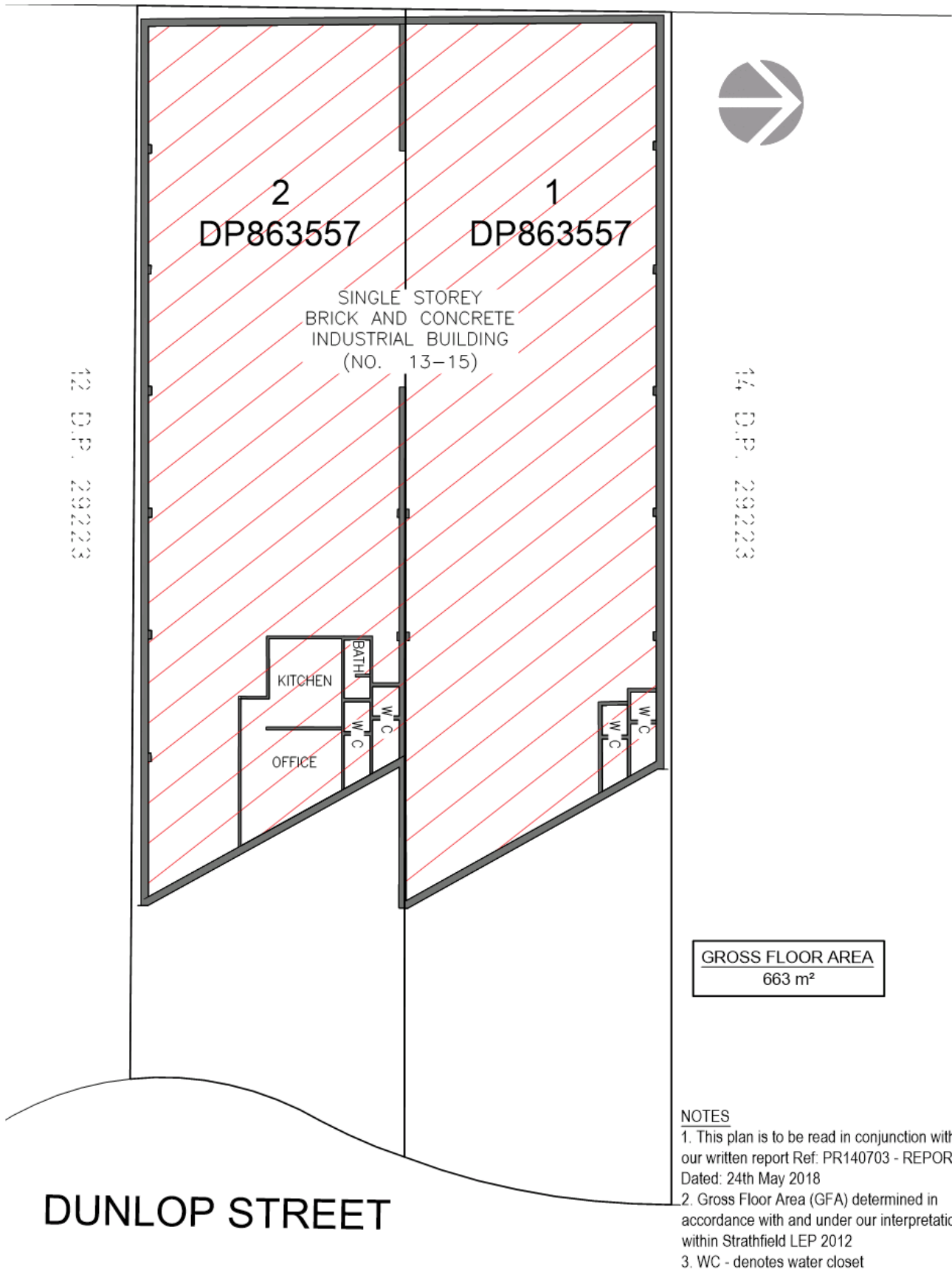
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**SCHEDULE OF GROSS FLOOR AREAS (GFA)
13-15 DUNLOP STREET, STRATHFIELD SOUTH, NSW**

LEVEL	GFA (SQM)
GROUND LEVEL	663
TOTAL	663

- Notes:
1. This schedule to be read in conjunction with the written report
Ref: PR140703-Report Dated: 24TH May 2018
 2. Areas calculated from site inspection / measurements and under interpretation of
Strathfield LEP 2012 Definition for Gross Floor Area
 3. Areas are approximate and stated in square metres.



GROSS FLOOR AREA
663 m²

NOTES

1. This plan is to be read in conjunction with our written report Ref: PR140703 - REPORT Dated: 24th May 2018
2. Gross Floor Area (GFA) determined in accordance with and under our interpretation within Strathfield LEP 2012
3. WC - denotes water closet

TITLE: PLAN SHOWING GROSS FLOOR AREA OF
No.13-15 DUNLOP STREET, STRATHFIELD
SOUTH LOT 1 & 2 IN D.P.863557

DATE: 24/5/2018
SCALE: 1:200
SHEET SIZE: A4

AUTOCAD REF: PR140703-GFA-001.dwg
CIVILCAD REF:
REVISION: A

AGENT: ABERGELDIE	RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762) 29-31 LEXINGTON DRIVE, PO BOX 6843 BAULKHAM HILLS NSW 2153	creativepeople	DRG
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Attachment 3

PDC Traffic and Parking Statement





PDC Consultants
Level 5, 104 Commonwealth Street
Surry Hills NSW 2010
T: +61 2 7900 6514
ABN: 70 615 064 670

Ref: 0024r01v03

6/07/2018

Abergeldie Watertech Pty Ltd
5 George Young Street
Regents Park NSW 2143

Attention: James Sidwell

**RE: 13-15 DUNLOP STREET, STRATHFIELD SOUTH
DA FOR THE CHANGE OF USE OF AN EXISTING DEVELOPMENT
TRAFFIC & PARKING STATEMENT**

Dear James,

PDC Consultants has been commissioned by Abergeldie Watertech Pty Ltd to prepare a Traffic & Parking Statement to accompany a Development Application (DA) for the site at 13-15 Dunlop Street, Strathfield South. Specifically, the DA seeks approval for the change of use of an existing warehouse development to an industrial development, having the following characteristics;

- No change to the existing 663m² gross floor area (GFA);
- At-grade car parking with a total of 5 car spaces;
- Retention of the two (2) existing driveways onto Dunlop Street;
- Retention of the two (2) existing on-site loading areas;
- An average of 8 staff on-site at any one time.

The site is located within the Strathfield local government area (LGA) and has therefore been assessed in accordance with the Strathfield Consolidated Development Control Plan 2005 (Strathfield DCP 2005) and Local Environmental Plan 2012 (Strathfield LEP 2012).

Additionally, we note that the development is not of a scale that requires the DA to be referred to the Roads and Maritime Services (RMS) under the provisions of the State Environmental Planning Policy (Instructure) 2007.

BACKGROUND

We understand that the existing development is approved as a warehouse under DA consent DA 97/45. Abergeldie Watertech Pty Ltd has since then commenced industrial processes and manufacturing at the site, related to their business operations which consent has not been covered for under DA 97/45.



A subsequent DA (DA 2018/021) for the change of use of the development, from warehouse to industrial, was lodged with Strathfield Council although determination of the DA has been deferred to allow Abergeldie Watertech Pty Ltd to provide some additional information including a Traffic and Parking Assessment. Accordingly, it is recognised that this Traffic and Parking Assessment report has been prepared in response to the preliminary feedback from Strathfield Council to allow for the assessment and determination of DA 2018/021.

LOCATION AND SITE

The subject site is located at 13-15 Dunlop Street, Strathfield South, being approximately 2.6 kilometres north of Belmore Railway Station and 12 kilometres west of the Sydney CBD. Specifically, the site is located at the southern end and western side of the Dunlop Street cul-de-sac.

The site comprises two (2) separate lots, Lot 1 DP 863557 and Lot 2 DP 863557 and is generally rectangular in configuration with a total area of approximately 980m². The site has a single street frontage, being Dunlop Street to the east, having a length of 20 metres. The northern and southern boundaries border industrial developments having lengths of 50 metres and 45 metres respectively. The western boundary borders a public park, Dean Reserve, having a length of 20 metres.

The site accommodates a single warehouse building having a total GFA of 663m². The site is served by two (2) separate entry/ exit driveways onto Dunlop Street, both having widths of approximately 4.0 metres. These provide access to the 3 car parking spaces and 2 on-site loading bays located at the front of the site.

Figure 1 overleaf provides an appreciation of the location of the site.

ROAD NETWORK

The road network in the vicinity of the site is shown in **Figure 2** overleaf with the following roads considered noteworthy:

- **Liverpool Road:** forms part of the Hume Highway (HW 2), being a major RMS arterial road that runs from Parramatta Road at Ashfield to Albury Wodonga at the NSW-Victoria State border. Liverpool Road generally runs in an east-west direction between Parramatta Road to the east and the Centenary Drive in the west. Near the site, Liverpool Road carries approximately 57,000 vehicles per day, is generally subject to 60 km/h speed restrictions and carries three (3) lanes of traffic in each direction.
- **Roberts Road:** forms part of an RMS Main Road, MR 200, that generally runs in a north-south direction connecting with Centenary Drive in the north and King Georges Road in the south. It is subject to 70km/h speed zoning restrictions and carries three (3) lanes of traffic in each direction.
- **Dunlop Street:** a local road, in the form of a cul-de-sac, that runs in a north-south direction, and intersects Dean Street in the north. It is subject to 50km/h speed restrictions and carries a single lane of traffic in each direction. Dunlop Street generally permits unrestricted parallel parking on both kerbsides.

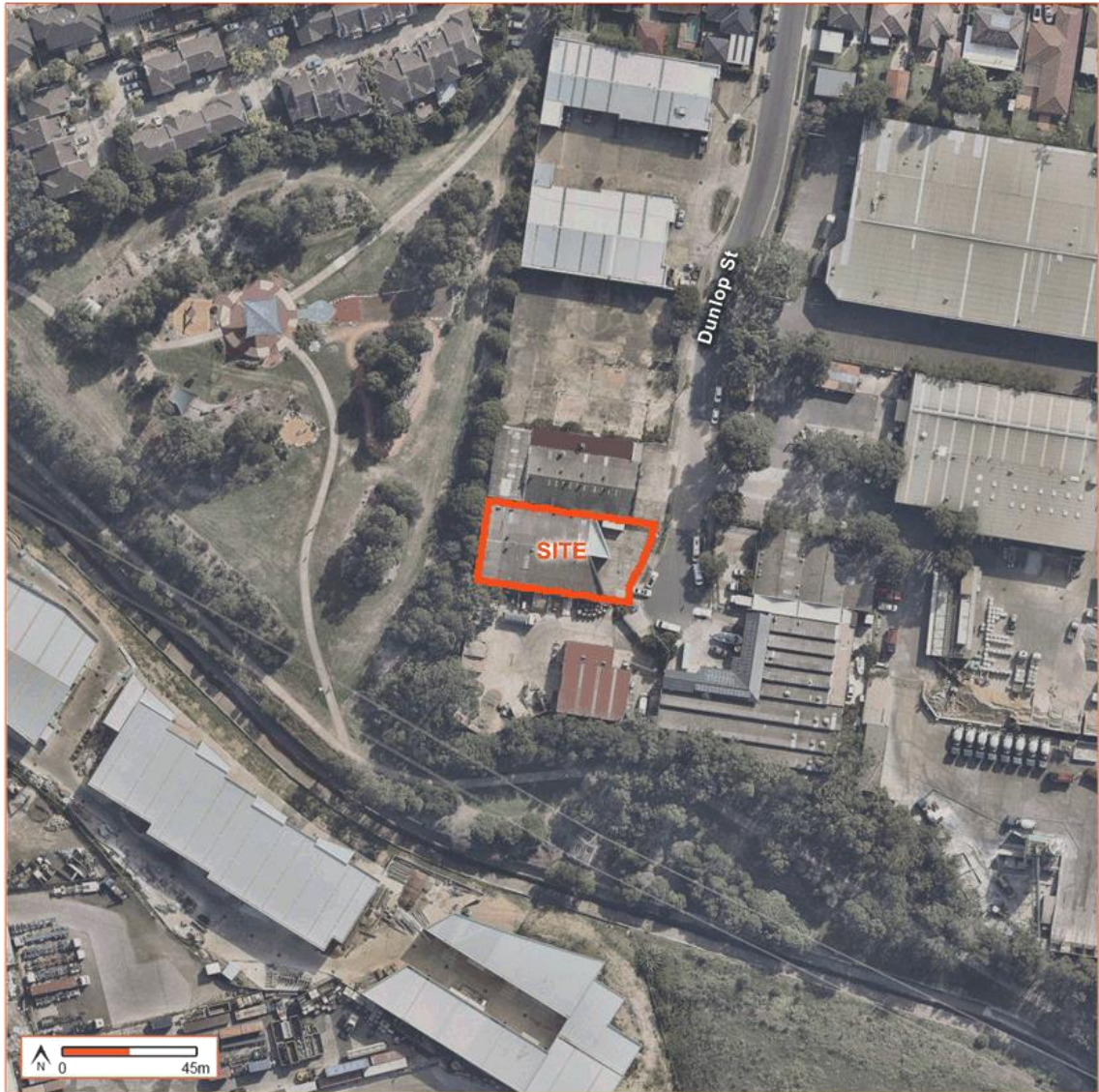


Figure 1: Site Plan

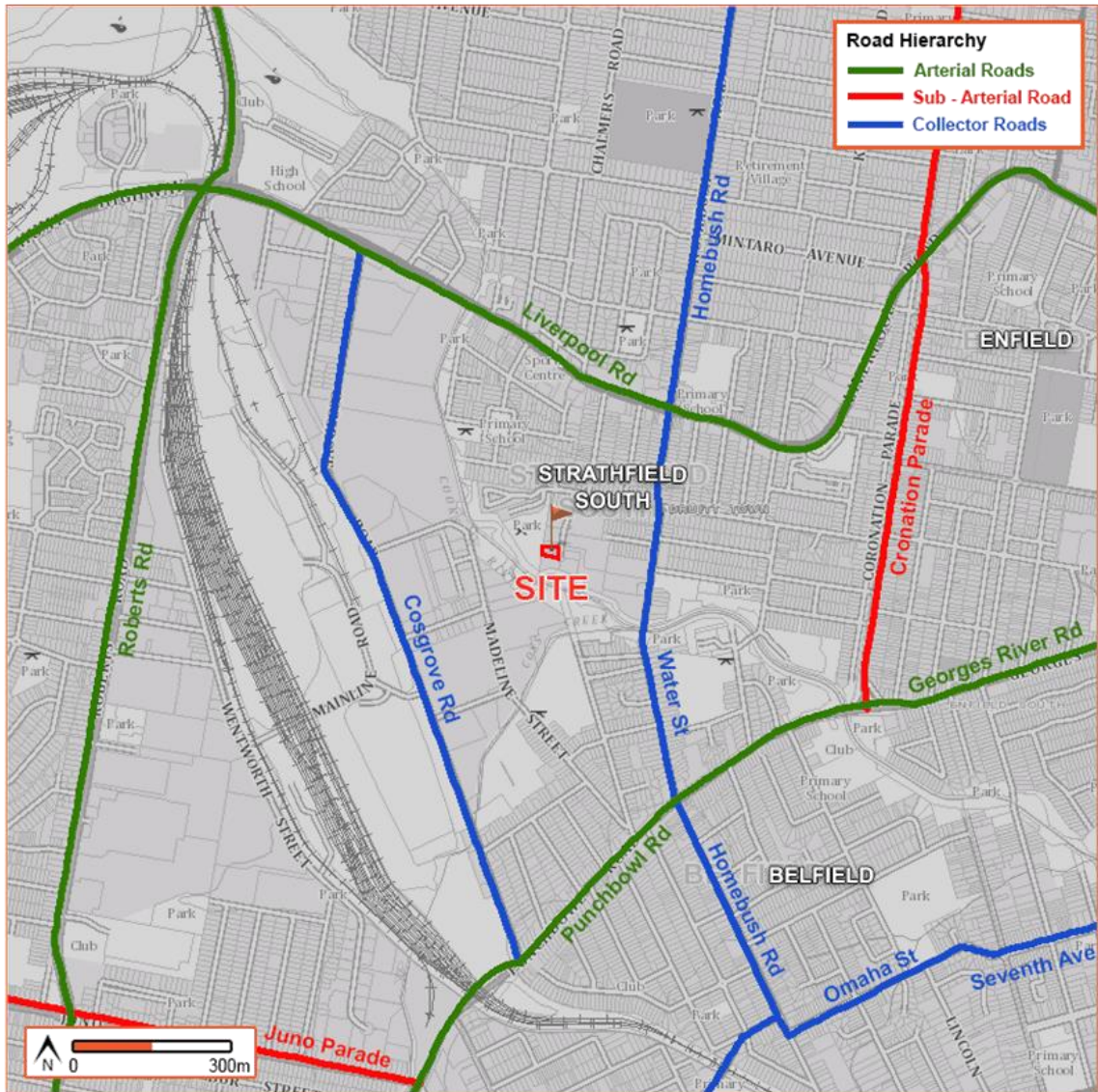


Figure 2: Existing Road Hierarchy



ACTIVE TRANSPORT SERVICES

Figure 3 overleaf shows the public bus services that operate in the vicinity of the site. It is evident that the site has relatively good access to bus services with six (6) bus routes available within 800 metres of the site, one of which is provided within 400 metres of the site operating along Dean Street and Water Street.

As also indicated on **Figure 3**, the site is situated 2.6 kilometres from Belmore Railway Station. Whilst the site resides outside the typical 800 metre walking catchment area of a railway station, as stated within the *Integrated Public Transport Service Planning Guidelines, Sydney Metropolitan Area*, staff would still be able to access Belmore Railway Station and the Sydney rail network, as shown by **Figure 4**, within a 24 minute multi-modal (bus and walk) or 13 minute cycle commute.

HISTORICAL TRAFFIC GENERATION

As previously mentioned, the site is currently approved and was historically used as a warehouse development with a total area of 663m² GFA. The *RMS Guide to Traffic Generating Developments (RMS Guide)* stipulates a trip generation rate of 0.5 trips / 100m² GFA / hour during the 7-9am (AM) peak period. This rate has also been adopted for the 4-6pm (PM) peak period as it is considered that both the AM and PM peak period traffic generations would be comparable. Application of the above rate to the existing warehouse GFA of 663m² results in the following peak period traffic generation:

- 3 vehicle trips / hour (2 in, 1 out) during the AM peak period
- 3 vehicle trips / hour (1 in, 2 out) during the PM peak period

The above assumes an inbound / outbound split of 80 / 20 during the AM peak period, and vice versa during the PM peak period. This assumption is appropriate for warehouse developments noting that the AM and PM trip generation is generally associated with the arrival and departure of staff.

THE PROPOSAL

A detailed description of the proposed development is provided in the Statement of Environmental Effects prepared separately by Nexus Environmental Planning. In summary, the DA seeks approval for the change of use of an existing warehouse development to an industrial development, having the following characteristics;

- No change to the existing 663m² gross floor area (GFA);
- At-grade car parking with a total of 5 car spaces;
- Retention of the two (2) existing driveways onto Dunlop Street;
- Retention of the two (2) existing on-site loading areas;
- An average of 8 staff on-site at any one time.

The parking and traffic implications arising from the proposed change of use are discussed below. Reference should be made to the contour and detail survey plan, prepared by RPS Group that is included in **Attachment 1**, which shows the built form of the existing development.

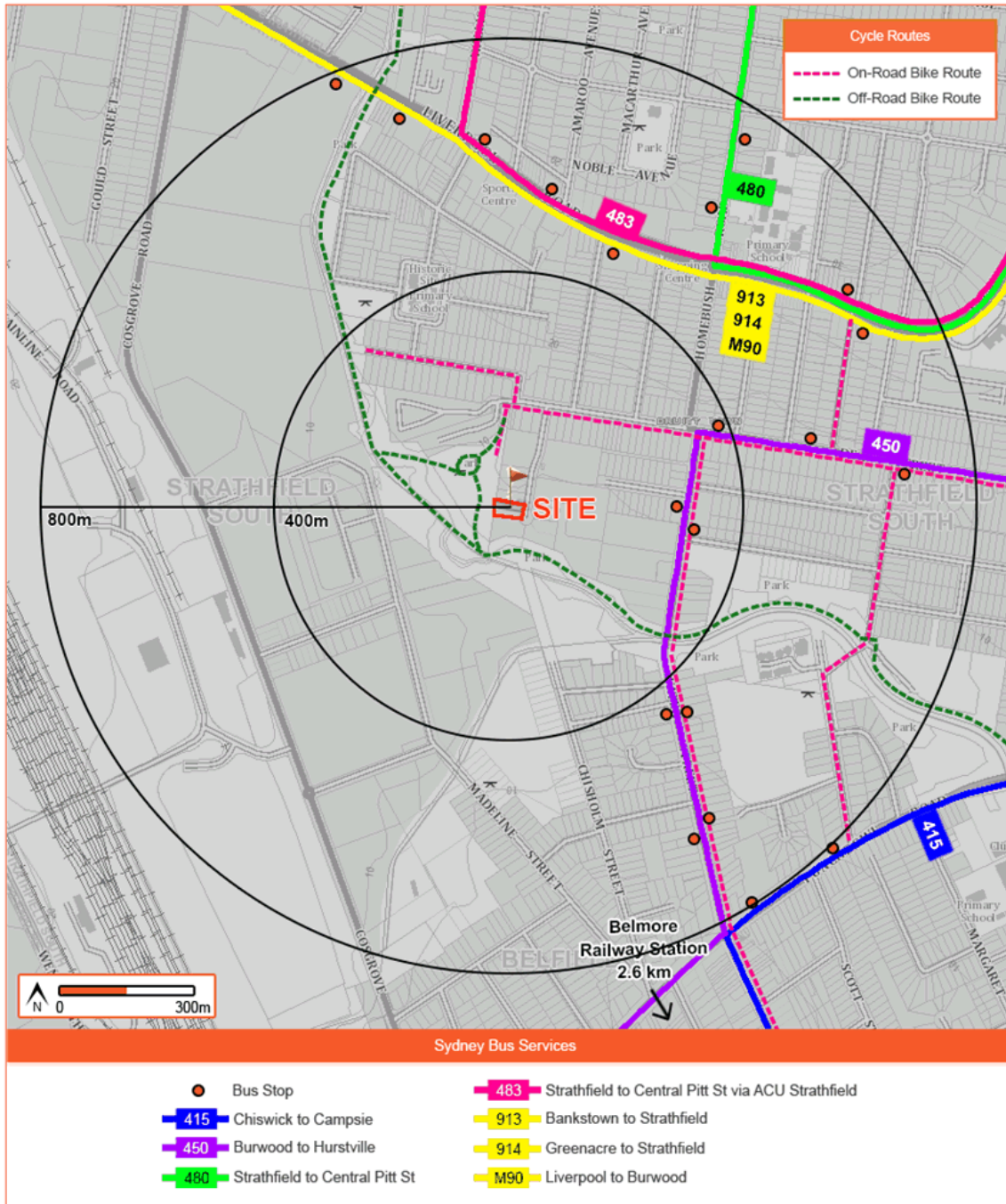


Figure 3: Active Transport



Figure 4: Sydney Rail Network – Suburban



PARKING REQUIREMENTS

Car Parking

The Strathfield DCP 2005 stipulates a rate of 1.0 car space per 55m² GFA or 1.0 space for every 2 employees, whichever provides the greater number of spaces, for industry developments. With 663m² industry GFA and an average of 8 staff on-site at any one time, it is evident that the development is required to provide the greater of the two below requirements:

- 12 car parking spaces, based on the GFA; or
- 4 car parking spaces, based the number of on-site employees.

It is evident from the above that the proposed development requires a minimum of 12 car parking spaces under the strict application of the Strathfield DCP 2005. However, it is critical to note that the development will operate with an average of 8 staff on-site at any one time and accordingly, would only generate a maximum parking requirement for 8 car parking spaces, or 1 space per staff member. Indeed, it is highly unlikely that each individual staff would drive their own private vehicle for journeys to/from the site and accordingly, the actual parking demand would likely be in the order of 6-7 car parking spaces.

As demonstrated by the Car Parking Arrangement Plan included in **Attachment 2**, the proposed development will provide a total of 5 on-site car parking spaces at the front of the site and will therefore require the reliance of on average, 1-2 on-street spaces along Dunlop Street.

A 'spot' on-street parking survey was conducted on 4th April 2018 between the hours of 12-2pm to determine existing parking demands. This period is considered to coincide with the on-street peak, given that all of the neighbouring businesses in Dunlop Street would be in operation and hence would provide a 'worst-case' basis for determining existing on-street parking demands and spare capacity. The survey confirmed that the Dunlop Street cul-de-sac has a total capacity of 25 on-street car parking spaces. A total of 17 cars was observed to park within the cul-de-sac during our survey, with a minimum of 8 car parking spaces available for additional parking.

It is evident from the above that the parking demand for 1-2 car parking spaces would be readily accommodated within Dunlop Street. Furthermore, even in the unlikely event that the development was to generate a peak parking demand for 8 parking spaces (or 1 space per staff member), the reliance on three (3) spaces by the development would be readily accommodated on-street with minimal impacts.

The proposed car parking provision is therefore considered acceptable.

Accessible Car Parking

The Strathfield DCP 2005 does not stipulate a rate for the provision of accessible car parking and accordingly, reference was made to the *Disability (Access to Premises – Buildings) Standards 2010* (Disability Standards 2010) to determine the minimum number of accessible car parking spaces required by the development. The Disability Standards 2010 stipulates a rate of 1.0 space per 100 car parking spaces or part thereof for Class 8 Buildings. With 5 car parking spaces, it is evident that the development is required to provide a single accessible car space.

In response, the Car Parking Arrangement Plan included in **Attachment 2** demonstrates that the development will provide a single accessible car space and therefore satisfies the requirements of the Disability Standards 2010.



Service Vehicle Parking

The Strathfield DCP 2005 does not stipulate a rate for the provision of service vehicle parking and accordingly, reference was made to the RMS Guide to determine the minimum number of service vehicle parking spaces required by the development. The RMS Guide stipulates a service vehicle rate of 1.0 space per 100m² GFA for industry developments. With 663m² industry GFA, it is evident that the development is required to provide a single service vehicle bay. In response, the development proposes to retain the 2 existing service vehicle bays located at the front of the site and therefore satisfies the minimum requirements of the RMS Guide.

Service vehicles up to and including a 12.5m heavy rigid vehicle (HRV) via the northern access driveway and an 8.8m medium rigid vehicle (MRV) via the southern access driveway will continue to reverse into the site / loading bays and exit the site in a forward direction, in accordance with Clause 3.2. of AS 2890.2-2002. Whilst the development does not incorporate any change to the existing service vehicle parking arrangements, changes would be made to the car parking area. Accordingly, it was considered appropriate to undertake swept path analysis of the servicing arrangements to confirm that a 12.5m HRV and 8.8m MRV would still be able to satisfactorily enter / exit the site. The analysis results are included in **Attachment 3**, confirming compliance and that a 12.5m HRV and 8.8m MRV would still be able to satisfactorily enter and exit the site.

TRAFFIC IMPACTS

The RMS Guide stipulates a trip generation rate of 1.0 trip / 100m² GFA for industry / factory developments during the PM peak period. This rate has also been adopted for the AM peak period as it is considered that both the AM and PM peak period traffic generations would be comparable. Application of the above rate to the 663m² GFA results in the following peak period traffic generation:

- 7 vehicle trips / hour (6 in, 1 out), during the AM peak period
- 7 vehicle trips / hour (1 in, 6 out), during the PM peak period

Comparing the above to the historical use as a warehouse, it is evident that the development would result in the following net increase in traffic generation:

- 4 vehicle trips / hour (4 in, 0 out), during the AM peak period
- 4 vehicle trips / hour (0 in, 4 out), during the PM peak period

As discussed above, the proposed development will result in a net increase of 4 vehicle trips / hour during both the AM and PM peak periods. This equates to only one (1) additional vehicle trip every 15 minutes which will have no material impact on the performance of the external road network or key intersections in the locality and accordingly, no external improvements will be required to facilitate the development.

Furthermore, computer modelling techniques available to analyse intersection performances are not sensitive to such small changes in traffic volumes and hence, such an assessment is not considered to be required. The traffic impacts of the proposed development are therefore considered acceptable.

DESIGN

The traffic and parking arrangements of the development has been assessed in accordance with AS 2890.1-2004 and AS 2890.6-2009, with the following design aspects considered noteworthy:



- No changes are proposed or required to the existing access arrangements. Accordingly, these will continue to operate satisfactorily.
- All car parking spaces are provided in accordance with the User Class 1A requirements of AS 2890.1-2004, having a minimum space width of 2.4m and length of 5.4m, with an aisle width of 5.8m.
- The accessible parking space is provided with a minimum width of 2.4m and length of 5.4m, with an aisle width of 5.8m. This is located immediately adjacent to a shared area having a minimum width of 2.4m and length of 5.4m, thereby satisfying the requirements of AS 2890.6-2009.
- A 1.0m aisle extension has been provided beyond the last parking space, in accordance with Figure 2.3 of AS 2890.1-2004.

The proposed access and internal parking arrangements therefore comply with the relevant requirements of AS 2890.1-2004 and AS 2890.6-2009, and will operate safely and efficiently.

SUMMARY

In summary:

- PDC Consultants has been commissioned by Abergeldie Watertech Pty Ltd to prepare a Traffic & Parking Statement to accompany a Development Application (DA) for the site at 13-15 Dunlop Street, Strathfield South. Specifically, the DA seeks approval for the change of use of an existing warehouse development to an industrial development, having the following characteristics;
 - No change to the existing 663m² GFA;
 - At-grade car parking with a total of 5 car spaces;
 - Retention of the two (2) existing driveways onto Dunlop Street;
 - Retention of the two (2) existing on-site loading areas;
 - An average of 8 staff on-site at any one time.
- The traffic assessment confirms that the development will generate a total of 7 vehicle trips / hour during the 7-9am (AM) and 4-6pm (PM) peak periods. The net increase will however be only 4 vehicle trips / hour during both the AM and PM peak periods, once the generation of the historical warehouse use is taken into consideration. This equates to an increase of only one additional vehicle trip every 15 minutes, which will have no material impact on the performance of the external road network or key intersections in the locality and accordingly, no external improvements will be required to facilitate the development. The traffic impacts of the proposed development are therefore considered acceptable.
- The development requires a minimum of 12 car parking spaces under the strict application of the Strathfield DCP 2005. The development will however operate with an average of 8 staff on-site at any one time and accordingly, would only generate a maximum parking requirement for 8 car parking spaces. The actual demand is expected to be 6-7 car parking spaces as it is highly unlikely that each individual staff would drive their own private vehicle for journeys to/from the site. The development proposes 5 on-site car parking spaces and will therefore be required to rely on Dunlop Street for 1-3 on-street spaces, with 3 spaces being the worst-case scenario. A 'spot' on-street parking survey was undertaken along Dunlop Street, confirming the reliance on up to three (3) spaces by the development would be readily accommodated on-street with minimal impacts. The proposed car parking provision is therefore considered acceptable.



- The proposed access and internal parking arrangements comply with the relevant requirements of AS 2890.1-2004, AS 2890.2-2002 and AS 2890.6, and will operate safely and efficiently.

The proposed development is therefore supportable on traffic planning grounds. Please contact the undersigned should you have any queries or require any further information.

Yours sincerely,

A handwritten signature in black ink, appearing to read 'Julius Boncato', written over a light grey horizontal line.

Julius Boncato

Traffic Engineer, PDC Consultants

Email: jboncato@pdconsultants.com.au

Attachments: 1) *Contour and Detail Survey Plan*
 2) *Car Parking Arrangement Plan*
 3) *Swept Path Analysis Drawings*



Attachment 1

NOTES

ANY ELECTRONIC FILE IS PROVIDED WITHOUT WARRANTY AND SHOULD BE USED ONLY IN CONJUNCTION WITH THE SUPPLIED POKER PAPER COPY OF THIS PLAN.

LIMITED BOUNDARY SURVEY HAS BEEN UNDERTAKEN. BEARINGS, DIMENSIONS AND AREA ARE FROM TITLE ONLY AND ARE SUBJECT TO CONFIRMATION BY FULL BOUNDARY SURVEY.

SERVICES SHOWN ARE INDICATIVE ONLY. POSITIONS ARE BASED ON SURFACE INDICATORS LOCATED DURING FIELD SURVEY. CONFIRMATION OF THE EXACT POSITION SHOULD BE MADE TO THE RELEVANT AUTHORITIES PRIOR TO ANY EXCAVATION WORK. OTHER SERVICES MAY EXIST WHICH ARE NOT SHOWN.

LEVELS ARE BASED ON AUSTRALIAN HEIGHT DATUM (AHD) USING SSM134373 WITH RL OF 17.62m (ACC. DH).

ORIGIN OF CO-ORDINATES: SSM134373 WITH MGA CO-ORDINATE VALUES OF E322,484,525 N6,247,948,567

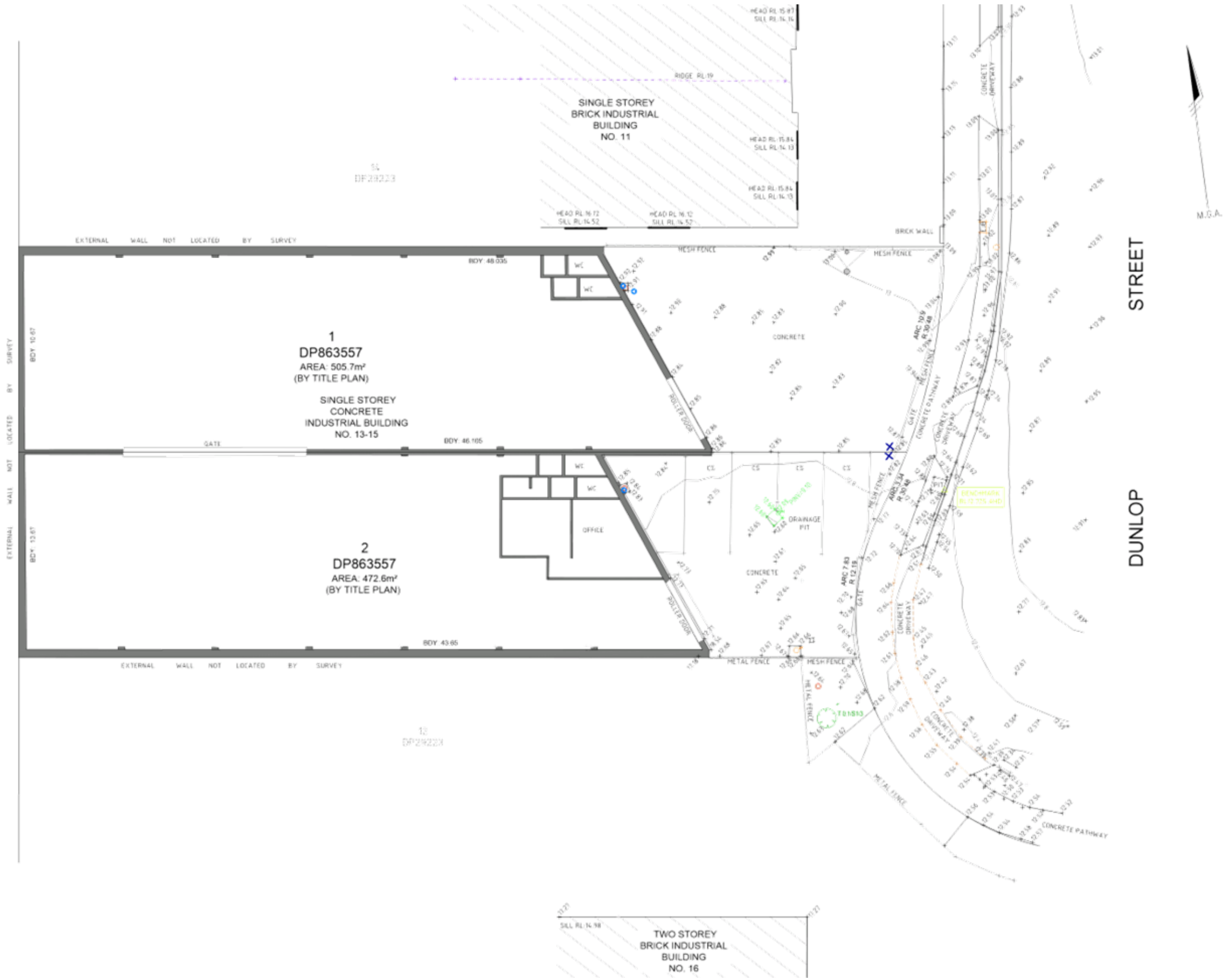
RIDGE, EAVE & GUTTER HEIGHTS HAVE BEEN OBTAINED BY AN INDIRECT METHOD AND ARE ACCURATE TO ± 0.05m.

ADJOINING DWELLING, BUILDING HAVE BEEN PLOTTED FOR DIAGRAMMATIC PURPOSES ONLY.

CONTOURS ARE AN INDICATION OF LANDFORM AND SHOULD NOT BE TAKEN IN PREFERENCE TO SPOT LEVELS SHOWN.

CONTOUR INTERVAL: 0.2m

- LEGEND
- WATER TAP
 - WATER VALVE
 - GATE
 - POWER POLE LIGHT
 - POWER POLE
 - GAS VALVE
 - SEWER MANHOLE
 - DRAINAGE PIP. INVERT
 - T.O. 1810 TREE
 - SPREAD 1.0
 - TRUNK DIAMETER Ø1
 - HEIGHT 3.0
 - CS DENOTES CAR SPACE
 - WC DENOTES WATER CLOSET



<p>1:100</p> <p>NA @ A1</p> <p>CO-ORDINATES: MGA</p> <p>SSM134373</p> <p>SSM134373</p> <p>SCALE: 1:1 METRES AT ORIGINAL REDUCED PRINTING</p>		<p>ABERGELDIE</p> <p>CONTOUR & DETAIL SURVEY</p> <p>13-15 Dunlop Street, Strathfield South</p> <p>PR140703</p> <p>DATE OF SURVEY: 05.06.2018</p> <p>DATE OF PLAN: 08.05.2018</p> <p>DATE LAST SHOWN: 12.06.2018</p> <p>DATE APPROVED: 12.06.2018</p>	
<p>12.08.2018</p> <p>08.06.2018</p> <p>13.07.2018</p>	<p>FENCES ADDED</p> <p>INITIAL VERSION</p> <p>PROVISIONAL</p>	<p>SP</p> <p>JAU</p> <p>SP/MS</p>	<p>MS</p> <p>MS</p> <p>MS</p>



Attachment 2






No.	Date	Description	Swept Path Key - - - - - Vehicle Wheel Path ——— Vehicle Body Envelope ——— 300mm Vehicle Clearance	North 	Drawing Prepared By PDC Consultants Level 5, 104 Commonwealth Street Surry Hills NSW 2010 t: +61 2 7900 6514 w: www.pdcconsultants.com.au ABN: 70 615 064 670	Architect	Project	Drawing Title	Drawing No.	Revision No.
						Client	Project No	Sheet Status	Scale	Drawn By
				Abergeldie Watertech Pty Ltd	0024	NOT FOR CONSTRUCTION	1:200 @ A3	JB	6/07/2018	A






Attachment 3



No.	Date	Description	Swept Path Key	North	Drawing Prepared By	Architect	Project	Drawing Title	Drawing No.	Revision No.
			<ul style="list-style-type: none"> --- Vehicle Wheel Path — Vehicle Body Envelope — 300mm Vehicle Clearance 		<p>PDC Consultants Level 5, 104 Commonwealth Street Surry Hills NSW 2010 t: +61 2 7900 6514 w: www.pdcconsultants.com.au ABN: 70 615 064 670</p> 	- Client Abergeldie Watertech Pty Ltd	13-15 Dunlop Street, Strathfield South Project No 0024	Contour & Detail Survey Plan 8.8m MRV Vehicle Swept Path Analysis Site Entry / Exit Movements 2 of 2 Sheet Status NOT FOR CONSTRUCTION	003 Drawn By JB	A Date 6/07/2018
Scale 1:250 @ A3 										



No.	Date	Description	Sweep Path Key	North	Drawing Prepared By	Architect	Project	Drawing Title	Drawing No.	Revision No.
			<ul style="list-style-type: none"> --- Vehicle Wheel Path --- Vehicle Body Envelope --- 300mm Vehicle Clearance 	<p>N</p> 	<p>PDC Consultants Level 5, 104 Commonwealth Street Surry Hills NSW 2010 t: +61 2 7900 6514 w: www.pdcconsultants.com.au ABN: 70 615 064 670</p> 	<p>Client Abergeldie Watertech Pty Ltd</p>	<p>Project 13-15 Dunlop Street, Strathfield South</p> <p>Project No 0024</p>	<p>Contour & Detail Survey Plan 12.5m HRV Sweep Path Analysis Site Entry / Exit Movements 1 of 2</p> <p>Sheet Status NOT FOR CONSTRUCTION</p>	004	A
									Drawn By	Date
									JB	6/07/2018
									Scale	
									1:250 @ A3	
										

Attachment 4

Spill Management Procedures



1 PURPOSE

Define the requirements and actions to be taken in the event of a hydrocarbon or chemical spill occurring at a workplace.

2 SCOPE

Applies to all Abergeldie operations, employees, contractors and visitors.

3 DEFINITIONS

Absorbent Material	Materials that recover oil spills through absorption (oil enters the material) or adsorption (oil clings to the surface of the material).
Bund	A raised, impermeable barrier forming the perimeter of a secondary containment area.
Bunded Area	An area that is fully contained, and does not allow the escape of fluids.
Environmental Incident	Pollution incidents, non-conformance and community complaints.
Environmental Records	Environmental monitoring records, audits, reports and reviews for which there is a statutory or operational requirement to retain.
Hydrocarbon	All fuels, oils and most lubricants are classed as hydrocarbons.
Non-Conformance	Where any statutory requirement has not been met.
Oil Spill	Any spill of oil, fuel, lubricant, Hydraulic fluids, waste oil, cutting fluids, and cleaning solvents, no matter how small.
Pollution Incident	An incident or set of circumstances during or as a consequence of which there is or is likely to be a leak, spill or other escape or deposit of a substance, as a result of which pollution has occurred, is occurring or is likely to occur. It includes an incident or set of circumstances in which a substance has been placed or disposed of on premises, but it does not include an incident or set of circumstances involving only the emission of any noise.
Reportable Serious Incident	<p>Different states and industry sectors have various definitions used in classifying a "Serious Incident" therefore reference to the local jurisdictional legislation or client documents is recommended.</p> <ul style="list-style-type: none"> ▪ In NSW – Serious Incident refers to "material harm to the environment" that would / could exceed \$10 000 to make good the harm. ▪ In QLD – A reportable incident may be defined as "Material Harm" that would / could exceed \$5000 to make good the



	<p>harm or "Serious Harm" that results in irreversible damage, incident in a high conservational valued area or that would / could exceed \$50 000 to make good the harm.</p> <ul style="list-style-type: none"> ▪ In ACT – A reportable incident may be defined as "Material Harm" that would / could exceed \$5000 to make good the harm or "Serious Harm" that results in irreversible damage, incident in a high conservational valued area or that would / could exceed \$50 000 to make good the harm. ▪ On vent shaft sites a spill of over 20 litres or that leaves the site. Can cause regional ecosystem damage.
Spill Kit	A kit containing absorbent material used in the cleanup of spills.
Spills	A spill of 5 – 20 Litres, can cause damage to local plant, animal, aquatic life and may enter drains. A spill under 5 litres can cause localised nuisance and aesthetic damage.

4 RISK ASSESSMENT

- Immediately following a hazardous chemical spill but prior to implementing any emergency response, the Project Manager shall ensure all workers involved in the response have participated in the development of the task specific SWMS using Form [WHS001-F05 – Safe Work Method Statement](#) and have reviewed the applicable Safe Work Procedure, Safety Data Sheet applicable to the task and substance to be handled (cleaned up) so as not to put themselves or the environment in risk of harm and all are aware of the cleanup / containment process.
- The risk assessment shall include the following factors:
 - the nature of the spilled chemical (high / low hazard material),
 - the quantity spilled (large / small amounts),
 - the location of the spill (difficult access, public site / bunded area),
 - the training required to handle the situation, and
 - the special equipment required to clean up the spill, inclusive of Self Contained Breathing Apparatus (SCBA), PPE, pumps, hoses, mobile spill kits, booms, etc.

5 SPILL KITS

- The following table details typical application / usage of various spill kits.

Kit Type / Application	General Workshop Liquids	Oils / Fuels / Solvents	Herbicide & Pesticide	Acids & Caustics
General Purpose	Yes	Yes	Yes	No
HazChem	Yes	Yes	Yes	Yes
Hydrocarbons	No	Yes	No	No



Note: Contents of spill kits vary depending on the type of substance to be contained and cleaned up. Ensure the spill kit selected is appropriate for the material to be cleaned up.

- The following table details typical contents of a spill kit.

Item	Description
Booms	<ul style="list-style-type: none"> ▪ Long 'socks' filled with absorbent material for placing around spills to contain them. ▪ Generally used in major or serious spills. ▪ Each boom has to overlap at least 100mm. ▪ Floating booms available for containing spills on water.
Mats	<ul style="list-style-type: none"> ▪ Absorbent fabric pads made from polypropylene microfibres. ▪ Absorbs acids, caustics, alkalis, oils and fuels. ▪ Absorbs up to 20 times its weight ▪ No known incompatible materials ▪ Mats are generally used for maintenance clean ups, e.g. around or under drums, vehicle service areas, small spills or the containment of small leaks.
Absorbent Materials	<ul style="list-style-type: none"> ▪ Various absorbent materials available (clay, saw dust, coconut husks, "Kitty Litter", sand). ▪ Used to pour over hydrocarbon spills to absorb it, enabling clean up and disposal. ▪ Convenient for use in cleaning up any size and shape spill.
PPE (dependent on kit type and kit provider)	<ul style="list-style-type: none"> ▪ Pair/s of oil resistant gloves. ▪ Pair/s of safety eyewear / goggles. ▪ Disposable coveralls / overalls.
Contaminated Waste Bags	<ul style="list-style-type: none"> ▪ Waste bags coloured coded and labelled according to the intended waste to be placed in it. ▪ Heavy-duty chemical resistant polyethylene. ▪ It is important to use these bags at the end of your spill cleanup procedure to ensure that the waste is deposited into the correct waste stream.
Miscellaneous (dependent on kit type and kit provider)	Kit may contain: <ul style="list-style-type: none"> ▪ instruction card detailing simple use, ▪ placards / signage to be installed at spill kit location, ▪ dust pan / broom / shovel / SDS pocket.



- Spill Kits shall be inspected at intervals as per Form [WHS020-F01 – Workplace Inspection Checklist](#), to identify any deficiencies in stock as per the inventory checklist contained within the spill kit (wheelie bin).

6 SPILL RESPONSE

- Site specific Emergency Response Plans previously developed shall be referenced and applicable responses implemented following any potential spills at a workplace.
- The following shall apply to the management of all spill response activities:
 - ensure your own safety by evaluating a spill situation,
 - ensure any fire or health hazards are identified before commencing a cleanup,
 - ensure appropriate PPE (as detailed in the relevant SDS) is available for use,
 - ensure all spills are immediately controlled, contained and cleaned up.
- The five main steps in any spill response are as follows:
 - identification,
 - control,
 - containment,
 - clean up, and
 - report.

6.1 IDENTIFICATION

- The first step is identifying the spilt substance. After it has been identified, clean up processes shall be determined according to the product's relevant SDS (available in the site SDS register).
- If the spill is unable to be identified, it shall be assumed the material is hazardous.
- Protective clothing and equipment shall be worn at all times. Special consideration should be given to additional PPE prior to performing cleanup activities.
- Clean up of a hazardous chemical spill shall not be undertaken if the hazards of the material or the correct clean up procedures are not known.
- General spill kits provided are designed for the cleanup of hydrocarbons and other general fluid spills (though not acid spills). If the spill is of a chemical substance, the Project Manager / environmental representative shall be immediately notified.
- If in doubt, do not place yourself or others at risk.

Note: It is the responsibility of all persons to assist in the recognition of potential conditions that may lead to environmental incidents in accordance with the Environmental Management Plan.

6.2 CONTROL

- If it is safe to do so, isolate the source of the spill by:
 - closing bund valves,
 - shutting down equipment, or
 - closing pipe valves to ensure prevention of further spillage.
- If it is safe to do so, remove people from the immediate area of the spill or any area that the spilt material has the potential to impact upon.



- Remove any ignition sources.
- If the product is hydrocarbon based (fuel), ensure that dry chemical powder fire extinguisher/s are on standby.
- Wear the correct safety equipment such as rubber boots, gloves, goggles and other protective clothing as specified in the SDS.

Note: Never endanger yourself by trying to isolate a source if it is dangerous to do so.

Note: In the event that the incident has occurred off-site and will (or has the potential to) cause environmental harm, the Site Manager is to immediately inform both the local Fire and Rescue Service and relevant state based Environmental Regulatory Authority (EPA or similar) after consulting the client.

6.3 CONTAIN

- Spills shall be contained before they are able to leak into watercourses and the natural environment.
- Typical spill stations contain absorbent mats, booms, and absorbent material that can be poured over spills.
- Dependant on the size of each work site, spill stations are typically located:
 - at a hydrocarbon storage area,
 - in the vicinity of stationary equipment that has potential to cause oil leaks,
 - at a vehicle / item of plant servicing bay / point,
 - in workshops,
 - in the vicinity of the drill rig if working on a vent shaft site, and
 - other areas as deemed necessary through completion of Form [WHS001-F04 – Workplace Risk Assessment](#).
- It is important to use the provided materials when containing a spill, as the appropriate containment materials will be more effective, easier to handle and to dispose of.
- Do not allow people or machinery to pass through the spill area.

Note: Never flush spills down drains or other outlets.

6.4 CLEAN UP

- Ensure the relevant SDS is available, read and understood by all personnel responsible for the cleanup of the spilled substance.
- The following should be used as a guide:
 - Ensure adequate PPE (P2 dust mask and goggles as a minimum),
 - Encircle the spill area by forming a mound around it with the absorbent material,
 - Cover the surface area by throwing handfuls of the absorbent material over the spills surface,
 - Allow liquid to be absorbed then mix with a shovel or “broom in” to complete the absorption process,
 - Collect the solidified waste with a shovel, and
 - Dispose of waste in chemical bins.



Note: Always clean up a spill substance in accordance with the relevant SDS, this procedure and the Environmental Management Plan.

Note: If it is not possible or safe to deal with the spill immediately on site, the Project Manager shall contact the local Emergency Services immediately.

Note: Do not place contaminated material inside spill kits.

- Employees shall wash their hands thoroughly:
 - prior to eating or smoking,
 - before and after going to the toilet, and
 - after handling any hazardous substances even where appropriate PPE was worn.

6.5 REPORTING

- All spills shall be reported in the HSEQ Incident Database refer to Procedure [WHS010 – Incident Management](#) for further details.
- Where required, the client shall be formally notified of spills.
- Depending on the severity on the spill, the state based environmental regulatory authority may need to be notified, refer to Procedure [WHS010 – Incident Management](#) for further details.

7 MANAGEMENT OF TYPICAL SPILL TYPES

- The contaminated / used absorbent material shall be correctly disposed of in an appropriately licensed waste facility.
- Waste receipts shall be obtained from the facility and all waste recorded on Form [ENV005-F01 – Waste & Material Tracking Log](#).

7.1 SPILLS ON SEALED SURFACES (CONCRETE / BITUMEN)

- Block inlets to surface water drains, sewage systems, natural water courses, pipe and cable ducts with a physical barrier (e.g. mats, booms, sand / soil or bunding).
- Concentrate the spill so it can be transferred into containers.
- Wear appropriate PPE and scoop or pump as much pooled substance as possible into a sealed container for either reuse or appropriate disposal.
- Upon removal of the majority of the spill, apply the absorbent product from the spill kit onto the contaminated area.
- Mix the absorbent material with a stiff-bristled broom into the spill until absorbed.
- Once absorbed, immediately scoop or shovel the saturated absorbent material into an appropriate chemical waste container and labelled as hazardous waste ready for disposal, and
- Document the clean-up activities.

7.2 SPILLS ON NON-SEALED SURFACES

- Excavate the contaminated soil.
- Store contaminated soil in contained / banded area on site for potential remediation purposes.
- Clearly mark the stockpile or banded area.
- Back-fill excavated area with clean fill material.



- If the spill is significant, soil samples should be collected by a competent third party and analysed to ensure all contaminants have been removed, and
- Document the clean-up activities.

7.3 SPILLS ON A WATER BODY (DAMS & LARGER BODIES)

- Cease any pumping or siphoning activities immediately to prevent contamination of other water sources.
- Control and isolate the source of the spill.
- Where possible, install a series of barriers (booms etc.) angled across the stream to prevent further contamination downstream. As a guide, an effective distance is 3-5 metres between barriers, this may vary with water velocity.
- Where practical, use a suction truck to remove the majority of the spill.
- Apply particulate absorbent over the surface of the spill.
- When saturated, skim or suction the absorbent off the surface of the water.
- If skimmed off the surface, place into plastic bags or appropriate chemical waste containers (labelled as hazardous waste) and dispose of, and
- Document the clean-up activities.

7.4 OFF SITE SPILLS / UNCONTROLLED SPILLS

- In the event that a spill has occurred either directly or indirectly off-site or the spill cannot be contained / controlled using workplace resources, the incident shall be referred immediately to the local Fire and Rescue Service for containment and clean up by calling "000".

Note: All persons must comply with any direction given by the Fire and Rescue Service upon their arrival. All cleanup activities must be documented.

8 ACCOUNTABILITIES

Project Manager	<ul style="list-style-type: none"> ▪ Ensure all necessary equipment is available and maintained in the case of a chemical spill and they are clearly labelled and signage clearly placed. ▪ Ensure risk assessments are carried out to identify risk control measures to protect the health and safety of people and potential harm to the environment. ▪ Ensure all staff receive appropriate training to deal with chemical spills where identified as necessary. ▪ Ensure appropriate responses are documented in the Site Specific Emergency Response Plan. ▪ Ensure appropriate personnel are informed of any chemical spills and external providers contracted to clean up where necessary.
Supervisors	<ul style="list-style-type: none"> ▪ Ensure information is provided to all site personnel to safely clean up spilled chemical substances. ▪ Ensure appropriate equipment is cleaned, stored and maintained by qualified persons.



	<ul style="list-style-type: none"> ▪ Ensure weekly workplace inspections are conducted, inspecting the availability and contents of any spills kits.
Workers (employees / subcontractors)	<ul style="list-style-type: none"> ▪ Ensure that they do not place themselves or others at risk of injury. ▪ Ensure the immediate reporting of chemical spills to their supervisor ▪ Ensure that they know where SDS's are kept or how they can be accessed. ▪ Ensure response equipment is stored and maintained as appropriate.

9 REFERENCES

Document No
<i>ENV005-F01 – Waste & Material Tracking Log</i>
<i>WHS001-F04 – Workplace Risk Assessment</i>
<i>WHS001-F05 – Safe Work Method Statement</i>
<i>WHS020-F01 – Workplace Inspection Checklist</i>
<i>WHS010 – Incident Management</i>
<i>WHS010-F02 – Incident Notification and Investigation Report</i>

TO: Strathfield Local Planning Panel Meeting - 4 October 2018
REPORT: SLPP – Report No. 5
SUBJECT: DA2018/046 - 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH
LOT 38 & 39 DP 2043
DA NO. DA2018/046

SUMMARY

Proposal: Demolition of existing structures and construction of a three (3) storey boarding house consisting of thirty-six (36) rooms including one (1) Manager's room set above one basement car parking level containing seventeen (17) car parking spaces, landscaping and associated site works.

Applicant: Hong Dieu Duong
Owner: Hong Dieu Duong
Date of lodgement: 10 April 2018 (Amended plans submitted 20/07/2018)
Notification period: 24/04/2018 to 16/05/2018
Submissions received: 21 submissions received including a petition of 27 signatories

Assessment officer: Consultant
Estimated cost of works: \$3,350,000
Zoning: B4 Mixed Use - SLEP 2012
Heritage: Sydney Water owned pressure tunnel and shafts from Potts Hill Road to Waterloo Pumping Station traverse the site in an east/west direction and are listed on the State Heritage Register under the NSW Heritage Act 1977.

Flood affected: No
Is a Clause 4.6 variation proposed? No
RECOMMENDATION OF OFFICER: APPROVAL

EXECUTIVE SUMMARY

1.0 On 10 April 2018, DA 46/2018 was lodged for the demolition of existing structures and construction of a mixed-use development comprised of a boarding house to accommodate (69) lodgers with one (1) Manager's room under the Affordable Rental Housing SEPP 2009, and a commercial unit to the ground floor.

2.0 On 25 June 2017, an '*Unsatisfactory letter*' was provided to the applicant following a preliminary review of the application. The key issues raised were height non-compliance, privacy to adjoining development, deep soil landscaping provision, car parking layout, external materials, and activation of the adjoining public domain.

**DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)**

- 3.0 On 20 July 2017, amended plans and an amended SEE were submitted to Council. Key changes include a reduced number of boarding rooms, deletion of Level 3 to achieve compliance with the 11m LEP height standard, deletion of the ground floor commercial tenancy and replacement with a communal space area, increased on-site car parking provision to comply with the amended SEPP (ARH) 2009, increased plantings and deep soil zones, inclusion of privacy amelioration measures, and changes to the external materials and finishes.
- 4.0 The amended plans and supporting documentation have satisfactorily addressed the concerns raised by Council. The amended proposal is considered to be suitable to the B4 Mixed Use zoning of the site and is considered to achieve an appropriate transition in height, bulk and scale to adjoining land zoned R2 Low Density Residential.
- 5.0 The site is burdened by Sydney Water owned *pressure tunnel and shafts* (from Potts Hill Road to Waterloo Pumping Station), which traverse the site in an east/west direction and are listed on the *State Heritage Register* under the *NSW Heritage Act 1977*. This constraint was not identified at the DA stage. The applicant has approached Sydney Water who are currently evaluating the proposed development. Conditions are recommended requiring written approval from Sydney Water and the Heritage Council of NSW to ensure no adverse impact to the Pressure tunnel and shafts and disturbance of an archaeological relic.
- 6.0 The proposal is acceptable against the relevant considerations under SEPP (Affordable Rental Housing) 2009, SEPP 55 (Remediation of Land), SEPP (BASIX) 2004, SEPP (Vegetation in non-rural areas) 2017, Strathfield LEP 2012, and Strathfield Consolidated DCP 2005.
- 7.0 The application was notified to all adjoining and nearby property owners in accordance with Council's notification policy, with 21 submissions received including a petition of 27 signatories. The main concerns raised include non-compliance with the LEP height standard, traffic and parking impacts, inconsistency with the character of the local area, privacy and overshadowing impacts, noise impacts, and adaptable housing provision. The submissions relate to the original scheme. The amended plans were not required to be re-notified as the modifications will have no greater cumulative environmental impact or amenity impact. The amended plans are considered to have satisfactorily addressed the concerns raised. A detailed response to each of the matters raised is provided later in this report.
- 8.0 The application has been reviewed by Council's internal referral staff, who have raised no objections to the proposal, subject to recommended conditions of consent.
- 9.0 The application is recommended for approval, subject to conditions.

BACKGROUND

Pre-lodgement meeting

22/08/2017 - A pre-lodgement meeting was held with the applicant regarding the construction of a four storey mixed-use development comprised of one (1) commercial tenancy, 38 boarding rooms and one manager's residence over two (2) levels of basement car parking at 251-253 Homebush Road, Strathfield South. The following issues were raised:

Streetscape character

- Clause 30A of the SEPP (ARH) 2009 requires development for the purposes of a boarding house to be compatible with the character of the local area.
- The site adjoins R2 zoned land to the south and west.
- A stepped setback ranging from 3.5m (north) to 5.2m (south) is recommended).
- The commercial use is to be changed to the northern side.
- The lift and fire stairs should be located further to the west to allow for articulation of the corner element.
- A 6m rear setback is to be provided

Building height

DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)

- An 11m maximum height applies.
- Any variation requires the submission of a clause 4.6 written request.

Overshadowing

- The east-west orientation of the site increases overshadowing to the adjoining property to the south.
- Any future development shall be accompanied by elevation shadow diagrams.

Stormwater

- The proposed butter pit is not supported by Council's Engineer. The pit shall be relocated to align with the proposed basement entry ramp.

Waste

- Consideration should be given to alternative waste collection to basement level onsite waste collection in accordance with Part H of SCDCP 2005, given the building height constraints and visual impact of the development.

Requests for additional information

25/06/2017 – An 'unsatisfactory letter' was provided to the applicant, raising a number of issues as follows:

Building height

- The proposal does not comply with the maximum building height of 11m permitted under SLEP 2012.
- The written clause 4.6 variation request is not considered to be well founded.
- The proposal shall be amended to ensure strict compliance with the 11m height control including any plant and lift overruns.
- The design of the proposal shall have regard to section 2.6 *Transition Zones of Part Q – Urban Design Controls* of SCDCP 2005 as the site adjoins land zoned R2 low density residential to the south.

Activation of ground floor public domain

- The proposed commercial component of the development does not provide adequate activation of the adjoining public domain.
- Consideration should be given to deleting the commercial component if the long-term viability and occupation rate is likely to be poor.

External materials and finishes

- The proposed face brick comprised of FB1 (Miro) and FB2 (Gaudi) shall be replaced with a face brick comprised of red/brown tones.
- The use of different brick colours for different sections of the building (as proposed) within the red/brown colour palette is encouraged in order to promote visual interest.

Deep soil landscaping

- Basement 1 extends to the northern, eastern and southern side boundaries.
- Consideration shall be given to a more efficient car parking layout in order to provide adequate deep soil landscaped areas around the perimeter of the site and within the front setback area.
- Clause 29(2)(b) of the Affordable Rental Housing SEPP 2009 prescribes that a consent authority cannot refuse an application if the landscape treatment of the front setback area is compatible with the streetscape in which the building is located.
- The streetscape of Homebush Road to the south is characterised by low density residential development with deep soil landscaped areas within the front setback and canopy trees. The proposal shall be amended to be consistent with this and include adequate screen planting in deep soil landscaped areas around the perimeter of the site.
Provision for new street trees of suitable species shall also be included.

Insufficient information

- The proposal has not addressed the urban design controls contained within Part Q of SCDCP 2005.
- The SEE shall be updated to demonstrate compliance this DCP Part.

Heritage

- Sydney Water owned pressure tunnel and shafts from Potts Hill Road to Waterloo Pumping Station traverse the site in an east/west direction and are listed on the State Heritage Register under the NSW Heritage Act 1977.
- A suitable condition can be imposed requiring approval from Sydney Water, however it is recommended that the applicant discuss the proposal with Sydney Water with evidence of the discussion provided to Council.

Privacy

- Using the building separation guidelines under Part 3F of the *Apartment Design Guide*, the

**DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)**

proposed north facing windows at Levels 1, 2 and 3 and south facing windows at levels 1 and 2 that are within 6m of the side boundary shall be fitted with fixed, external screens angled at 45 degrees in order to prevent overlooking to adjoining development.

- The external screens shall consist of high quality materials and shall be consistent with the architectural design of the building.

Front setback

- The proposed front building alignments of generally 3.5m on the northern portion of the site and 5.2 on the southern portion of the site shall be increased by 1.5m.

Amended Plans

20/07/2018 – Amended Plans (Issue 2) were submitted to Council, providing the following amendments:

Building height reduced to comply

- The proposed development has been amended to achieve compliance with the 11m height control under SLEP 2012. The proposed maximum building height is 10.5m.
- The proposed fourth storey (level 3) has been deleted.

Floor space ratio reduced

- The floor space ratio has been reduced from 1.43:1 to 1.21:1. The SLEP 2012 standard is 1.4:1.

Deletion of ground floor commercial tenancy

- The proposed commercial tenancy at ground floor has been replaced with a communal room, communal open space area, managers room and reception space.

Number of boarding rooms reduced

- The number of boarding rooms has been reduced from 38 to 35 plus a Managers room.

Number of accessible rooms increased

- The number of accessible rooms has increased from 4 to 6.

Basement layout amended

- Basement layout amended to increase boarding house car parking from 11 spaces to 17 spaces and motorbike parking from 8 spaces to 9 spaces.

Revised SEE

- A revised SEE has been submitted to include an assessment of the proposed development against *Part Q Urban Design Controls* of SCDCP 2005.

Revised Boarding House Plan of Management

- A revised Boarding House Plan of Management has been prepared.

Additional privacy screening

- Additional external window screens have been provided to the north facing windows at Levels 1, 2 and 3 and south facing windows at levels 1 and 2 that are within 6m of the side boundary.

Increased deep soil landscaping

- The basement has been reduced by relocating the rear stairs that previously intruded into the rear setback, increasing the deep soil area from 126m² to 146m².
- The landscaping bed along part of the southern boundary has been widened from 1.0m to 2.0m to provide additional space for medium sized shrubs.

Revised external materials and finishes

- The proposed face brick comprised of FB1 (Miro) and FB2 (Gaudi) has been replaced with a face brick comprised of red/brown tones.

Assessing officer's comments

External materials and finishes

- The external materials and finishes have generally incorporated the suggested amendments.
- It is recommended the proposed FB2 *Face Brick – 'Miro'* is replaced with FB1 *Face Brick – 'Gamet'* as proposed. A condition to this effect is included within the Recommendation.

Revised basement layout

- The revisions to the basement layout to create additional parking spaces are supported.
- Relocation of the OSD tank is supported.
- The amendments increase the deep soil landscaped area within the rear setback.
- Further reduction of the basement footprint is not possible on the basis of increased parking requirements under the amended SEPP (ARH) 2009.
- The proposed nil setback to the side boundaries is consistent with the development approved at 247-249 Homebush Road.

Deletion of the commercial tenancy

**DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)**

- Replacement of the proposed commercial tenancy with a communal area is supported. The revisions are considered to provide a more appropriate land use given the site adjoins land zoned R2 low density residential to the south and west.
- The communal room and adjoining communal open space area is considered to improve passive surveillance of the adjoining public domain.

Front setback

- Retention of the front setback as proposed ranging from 3.5m on the northern side to 5.2m on the southern side is consistent with advice given at the pre-lodgement stage, and is supported.

Heritage

- The updated SEE states that enquiries have been made with Sydney Water regarding the *Pressure Tunnel and Shafts*. Sydney Water are currently reviewing the proposal. A condition is recommended to require that the approved plans are approved by Sydney Water, prior to the issue of any Construction Certificate.

Privacy

- Additional external screening to the north facing windows at Levels 1, 2 and 3 and south facing windows at levels 1 and 2 that are within 6m of the side boundary, is considered to maintain acceptable privacy amenity to the adjoining properties.

Comparison of the original DA and amended proposal

A comparison of the original DA and amended proposal is included at **Figure 1** to **Figure 13**.



Figure 1. Amended proposal showing a photomontage of the front (eastern) elevation of the proposed three (3) storey building fronting Homebush Road.

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 Lot 38 & 39 DP 2043 (Cont'd)



Figure 2. Original proposal showing a photomontage of the front (eastern) elevation of the proposed four (4) storey building fronting Homebush Road.

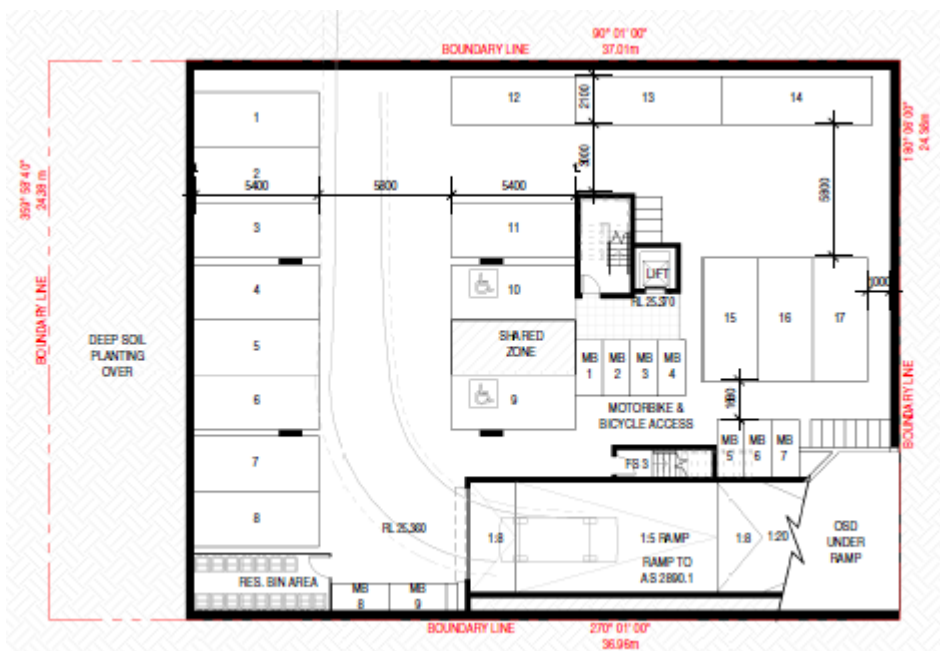


Figure 3. Amended Basement 1 layout. The stair at the rear has been deleted. The commercial car parking spaces have been deleted. Parking for the boarding house has increase from 11 spaces to 17 spaces.

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 Lot 38 & 39 DP 2043 (Cont'd)

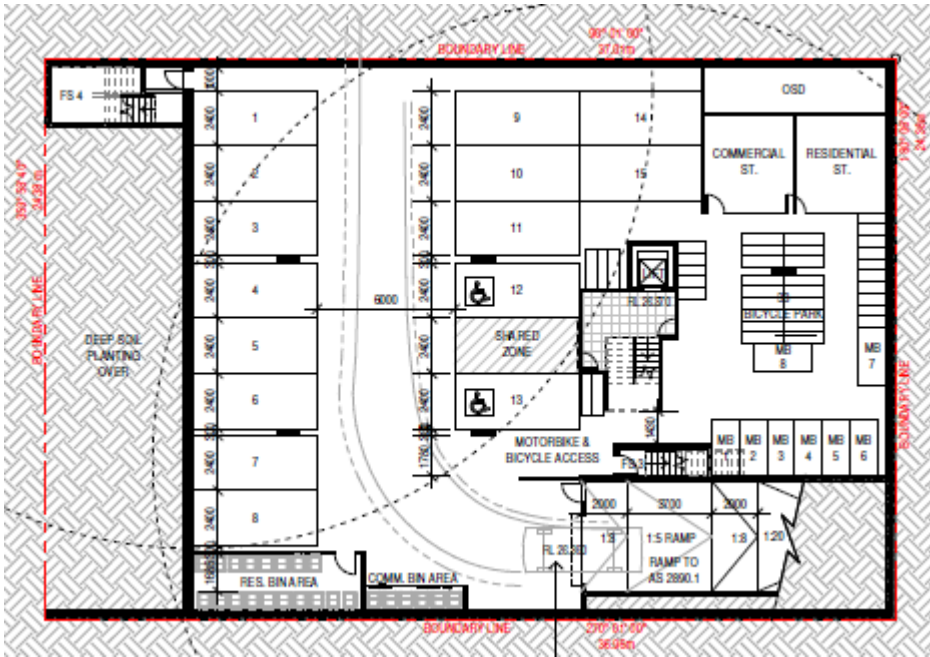


Figure 4. Original Basement 1 layout.



Figure 5. Amended ground floor plan showing replacement of the commercial area with a communal area, increased deep soil planting in the rear setback and increased perimeter planting on structures.

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 Lot 38 & 39 DP 2043 (Cont'd)

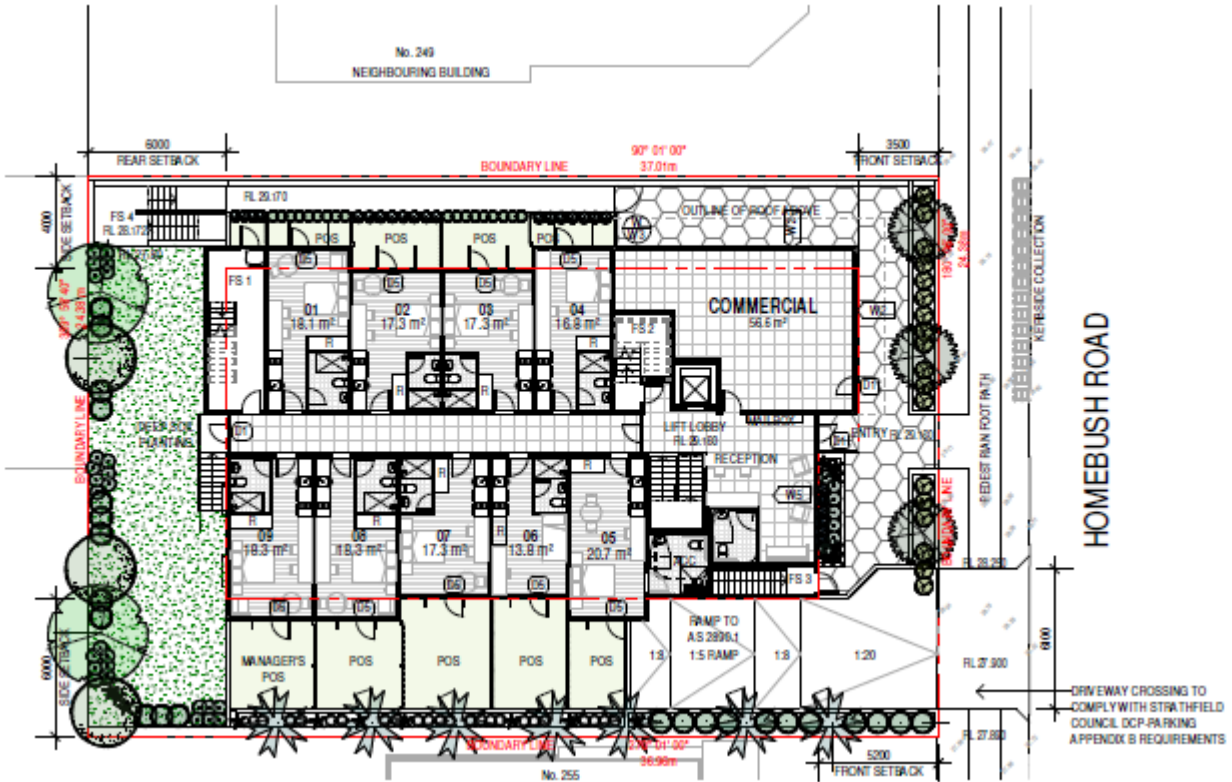


Figure 6. Original ground floor layout

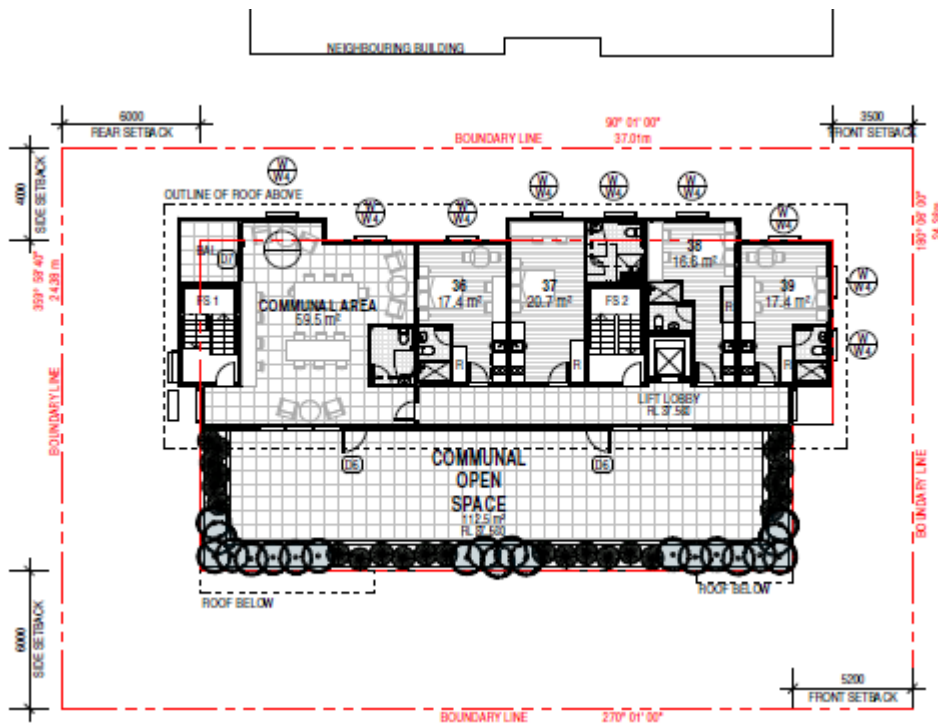


Figure 7. Original Level 3 floor plan, which has been deleted under the amended proposal.

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 Lot 38 & 39 DP 2043 (Cont'd)



2 SOUTH ELEVATION
 1:200

Figure 8. Amended southern elevation showing a compliant building height, three (3) storey presentation, additional external privacy screening, and incorporation of face-brick to complement the prevailing streetscape character.



2 SOUTH ELEVATION
 1:200

Figure 9. Original Southern elevation showing a part four (4) storey presentation and contravention of the 11m building height control.



Figure 10. Amended eastern (front) elevation



1 EAST ELEVATION
 1:200

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 Lot 38 & 39 DP 2043 (Cont'd)

Figure 11. Original eastern (front) elevation



Figure 12. Amended western (rear) elevation



Figure 13. Original western (rear) elevation

DESCRIPTION OF THE SITE AND LOCALITY

The subject site is legally described as Lots 38 and 39 in DP 2043 and is commonly known as 251-253 Homebush Road, Strathfield South. The site is located on the western side of Homebush Road and has a total area of 898m² (by Title).

The site is rectangular in shape and has a frontage of 24.38m to Homebush Road to the east, and depth of 37m.

The site has a cross fall from north to south of approximately 0.9m and fall from front (east) to rear (west) of approximately 1.2m.

Existing development on the site comprises two (2) single storey detached dwelling houses with shared vehicular access off Homebush Road.

The current streetscape is characterised by a mixture of single and two (2) storey detached dwelling houses to the south, east, and west of the site. The permitted maximum height and FSR increases to the north of the site to a maximum of 3:1 and 22m at the intersection of Homebush Road and Liverpool Road.

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A locality plan is included at **Figure 14**. Photographs of the adjoining and surrounding development are included at **Figure 15** to **Figure 20**.

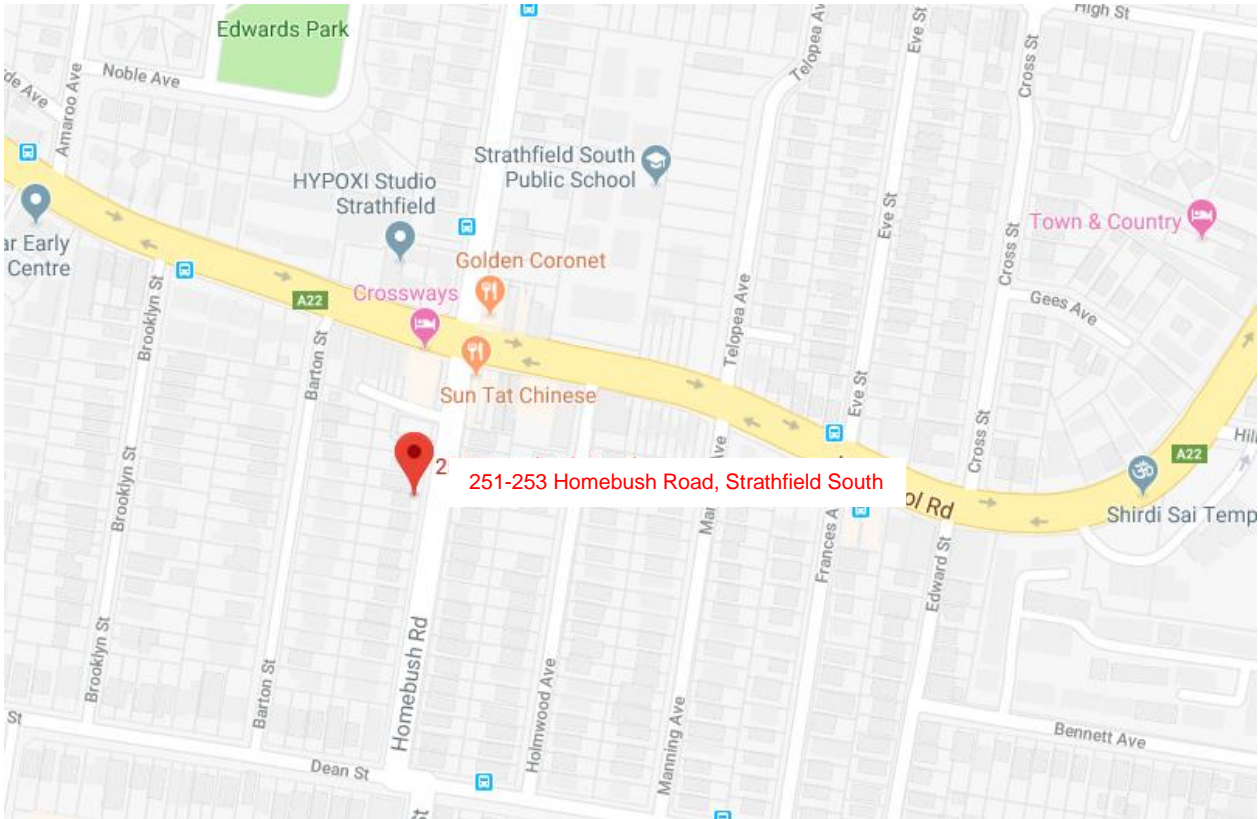


Figure 14. Locality plan (Source: Google Maps).



Figure 15. Existing buildings on the subject site at No. 251-253 Homebush Road, Strathfield South.

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Figure 16. Adjoining two (2) storey dwelling to the south of the site at No. 255 Homebush Road. The site adjoins land zoned R2 low density residential with 9.5m height limit to the south and west.



Figure 17. Existing development on the eastern side of Homebush Road, opposite the site. T

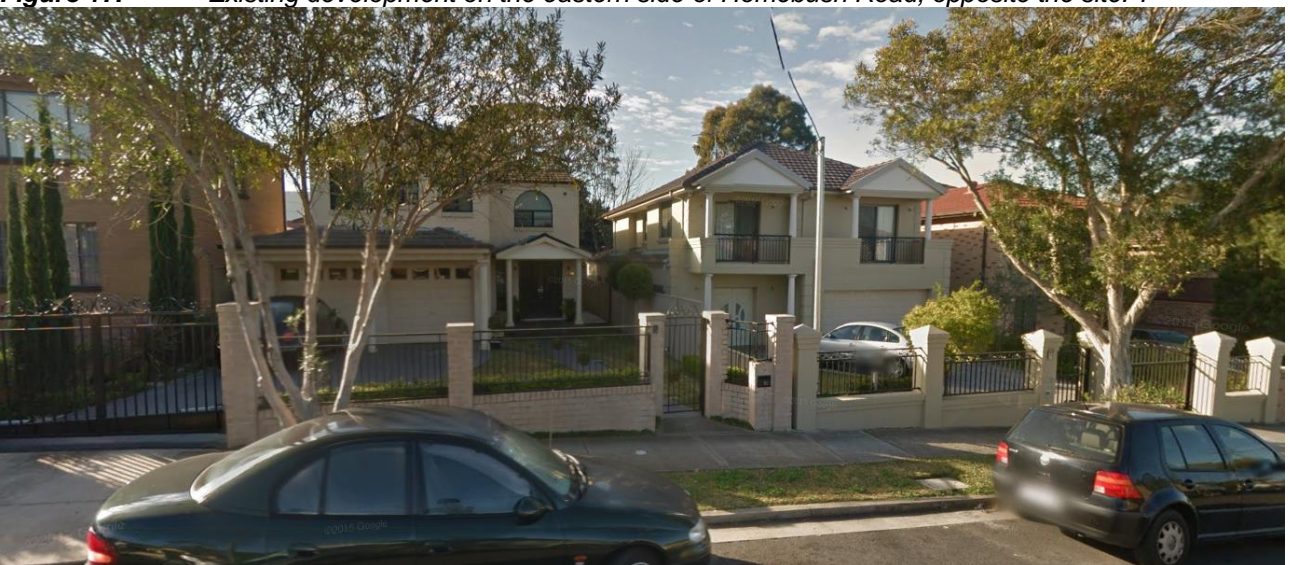


Figure 18. Existing two (2) storey dwelling houses at No. 12 and No. 14 Barton Street, which adjoin the site to the rear.

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 Lot 38 & 39 DP 2043 (Cont'd)

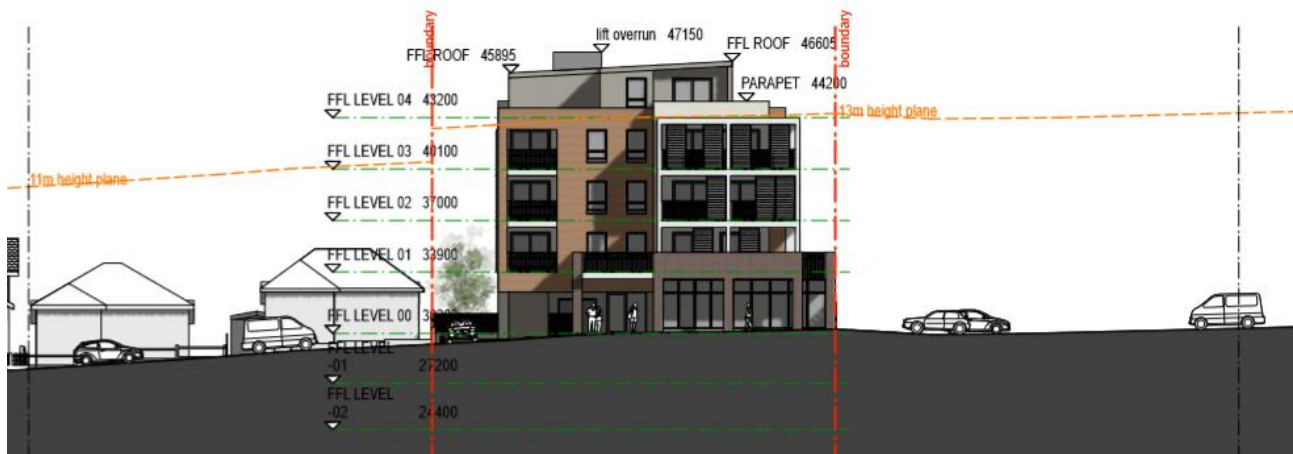


Figure 19. The single storey dwelling houses to the north of the site at No. 247-249 Homebush Road, Strathfield South have been demolished. Works have commenced for the approved development under DA 2016/78 involving demolition of existing structures and construction of a four (4) storey mixed-use development under the SEPP (ARH) 2009 comprising a total of (18) units and (1) commercial tenancy above two (2) basement levels of parking. Condition 6 requires the development to be amended to comply with the 13m height standard.



Figure 20. Existing 'Crossways Hotel' and ancillary at-grade car park to the north of the site on the corner of Homebush Road and Liverpool Road. The car park adjoins No. 247-249 Homebush Road (Figure 19).

PROPERTY BURDENS AND CONSTRAINTS

A Sydney Water owned pressure tunnel and shafts from Potts Hill Road to Waterloo Pumping Station traverse the site in an east/west direction and are listed on the State Heritage Register under the NSW Heritage Act 1977, as shown at **Figure 21**.

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Lot 38 & 39 DP 2043 (Cont'd)

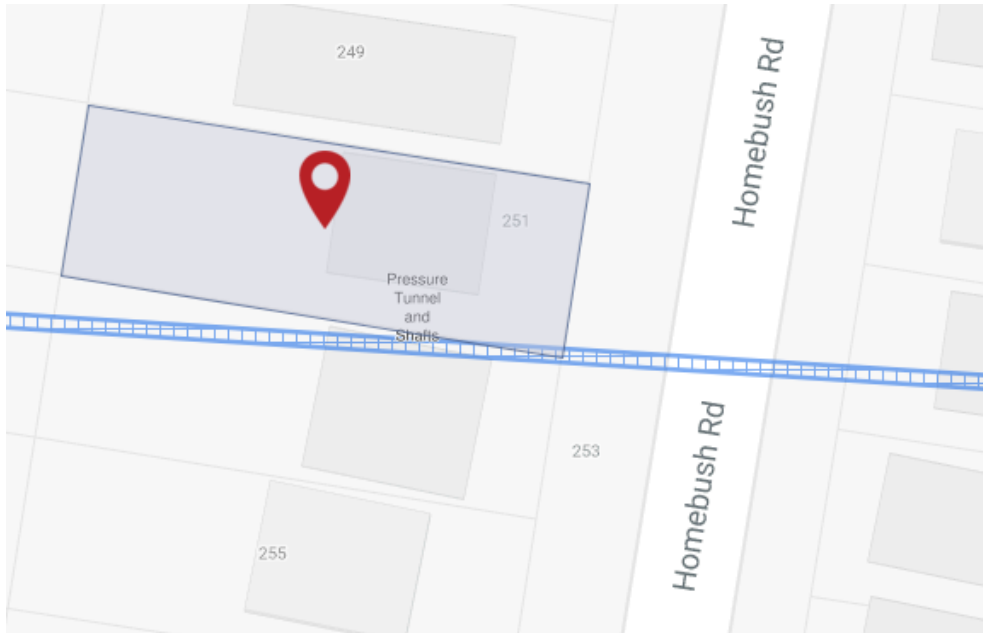


Figure 21. State Heritage listed Pressure Tunnel and Shafts, which traverses the site.

The applicant has contacted Sydney Water, who is presently undertaking an evaluation of the development's impact on the Pressure Tunnel. A condition is recommended which requires written confirmation from Sydney Water that the proposed development will not have any adverse impact on the Pressure tunnel and shafts.

DESCRIPTION OF THE PROPOSED DEVELOPMENT

The application seeks Council approval for demolition of existing structures and construction of a three (3) storey boarding house consisting of thirty-six (36) rooms including one (1) Managers room set above one basement car parking level containing seventeen (17) car parking spaces, landscaping and associated site works.

The specific elements of the proposal are:

Basement level

- 17 car parking spaces
- On-site-detention under the driveway ramp.
- 9 motorbike spaces
- 12 Bicycle spaces
- Lift and stair access
- Residential bin storage area

Ground floor level

- 9 rooms comprised on 1 x single room and 8 x double rooms
- 1 x Managers rooms (accessible)

Level 1

- 13 rooms comprised of 3 x single rooms and 10 x double rooms (including 2 x accessible rooms)

Level 2

- 13 rooms comprised of 3 x single rooms and 10 x double rooms (including 2 x accessible rooms).

External works

- Landscaping

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Lot 38 & 39 DP 2043 (Cont'd)**

The total capacity of the boarding house is 63 lodgers in 35 boarding rooms (7 x singles + 28 doubles), plus the Manager's accommodation.

A Plan of Management has been submitted with the application.

RELEVANT APPLICATION HISTORY

DA Number	Address	Proposal	Outcome
2015/140	251-253 Homebush Road, Strathfield South	Demolition of existing structures and construction of an 'Infill Affordable Housing' development under the SEPP(AFR) 2009 consisting of a part three (3) and part four (4) storey RFB containing (20) units comprising (5) x 2bedroom units, (14) x 2 bedroom and (1) x 3-bedroom units above two (2) levels of basement parking.	Refused by the L&E Court
2016/78	247-249 Homebush Road, Strathfield South	Demolition of existing structures and construction of a four (4) storey mixed use development under the provisions of SEPP (ARH) 2009 comprising a total of (18) units and one (1) commercial tenancy above two (2) levels of basement parking. Note: Special Condition 6 prescribes that no part of the building shall exceed the 13m maximum height standard.	Approved by SIHAP on 06/04/2017

REFERRALS

INTERNAL REFERRALS

Referral	Comments	Satisfactory
Engineering	No objections are raised by Council's Engineer, subject to recommended conditions of consent. The stormwater concept plans are generally satisfactory subject to the inclusion of additional details pertaining to roof runoff, and location of council infrastructure. A condition to this effect is included within the recommendation.	✓
Building	Suitable conditions are recommended with regards to building related matters including compliance with the BCA and fire-related matters.	✓
Traffic	Concerns were raised to the original car parking layout in terms of the proposed car stacker mechanism and compliance with AS2890.1-2004. The car stacker and commercial parking component have been deleted under the amended scheme. No further objections are raised, subject to the imposition of recommended conditions of consent including compliance with AS2890.1-2004.	✓

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SECTION 4.15 CONSIDERATIONS – EP&A Act, 1979

In determining a development application, the consent authority is to take into consideration the following matters of consideration contained within Section 4.15 of the Environmental Planning and Assessment Act, 1979 as relevant to the development application:

4.15(1)(a) the provisions of:

(i) any environmental planning instrument

STATE ENVIRONMENTAL PLANNING POLICY (SEPP) – BASIX 2004

In accordance with the BASIX SEPP all new housing in NSW is required to meet a designated target for energy and water reduction.

A BASIX Certificate was submitted as part of the application which indicates that the proposal meets the required reduction targets. A condition of consent is recommended to ensure future compliance with these targets.

State Environmental Planning Policy No 55 – Remediation of Land

SEPP 55 applies to the land and pursuant to Section 4.15 is a relevant consideration.

A Stage 1 Preliminary Environmental Site Investigation has been carried out by *Geo-Environmental Engineering*. The Report

A review of the available history for the site combined with surface sampling at accessible locations across the site indicated no significant contamination. In this regard, the site is considered to be suitable for the proposed development and associated land-use. Notwithstanding this, it is acknowledged that some structures on the site may contain fibrous cement sheeting, which contains asbestos fibres. An Asbestos Audit is recommended to identify any asbestos contained materials, prior to any demolition works. A condition to this effect is included within the recommendation.

Therefore, the proposal is considered to be acceptable with regards to SEPP 55.

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017

The *State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017* replaces the repealed Clause 5.9 of SLEP 2012 (Preservation of Trees and Vegetation).

The intent of this SEPP is consistent with the objectives of the repealed Standard where the primary aims/objectives are related to the protection of the biodiversity values of trees and other vegetation on the site.

An *Arboricultural Impact Assessment* Report has been prepared by Advanced Treescape Consulting. This supports the removal of seven (7) trees from No. 251 Homebush Road (Trees 2, 3, 6 to 9, and 11) and one (1) tree along the rear boundary of No. 253 Homebush Road (Tree 14). These trees are either in poor health, are located within the buildable area, or both. The removal of these trees is supported.

The existing *Tristaniopsis laurina* tree on Council's Road Reserve (Tree 1) is to be retained and protected in addition to four (4) trees along the sites rear boundary (Trees 4, 5, 12, and 13) in accordance with the *Tree Protection Plan* under Appendix 8 of the submitted *Arboricultural Impact Assessment* Report.

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The Arborist Report further recommends that suitable replacement trees are included in the front setback landscape plan to enhance the streetscape amenity. A condition to this effect is included within the recommendation.

Therefore, the proposed development is acceptable with regards to *State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017*.

State Environmental Planning Policy (Affordable Rental Housing) 2009

SEPP (ARH) 2009 adopts the LEP definition of *Boarding house*, as follows:

“boarding house means a building that:

- (a) is wholly or partly let in lodgings, and
- (b) provides lodgers with a principal place of residence for 3 months or more, and
- (c) may have shared facilities, such as a communal living room, bathroom, kitchen or laundry, and
- (d) has rooms, some or all of which may have private kitchen and bathroom facilities, that accommodate one or more lodgers,

but does not include backpackers' accommodation, a group home, hotel or motel accommodation, seniors housing or a serviced apartment.”

The proposed boarding house meets the above definition as it will be wholly let in lodgings, will provide lodgers with a principal place of residence for 3 months or more, has shared facilities, and rooms accommodating up to two (2) lodgers.

The proposed development is consistent with the aims of the Policy under Clause 3 of SEPP (ARH).

Clause 29 of SEPP (ARH) 2009 sets out the matters, which cannot be used as grounds for refusal, if satisfied. Clause 30 sets out the specific development standards for boarding houses. An assessment of the proposed development against Clause 29 and 30 of SEPP (ARH) 2009 is contained below:

Clause	Control	Proposed	Complies
29(1)(c) FSR	Maximum FSR is existing maximum FSR under an EP1 plus a bonus FSR of 0.5:1 if RFB's are permissible in the zone. FSR of 1.4:1 is permissible under SLEP 2012. A bonus FSR of 0.5:1 applies.	The proposed FSR of 1.2:1 complies.	Yes
29(2)(a) Height	Maximum height permitted under an EPI. 11m maximum height under SLEP	10.5m maximum	Yes
29(2)(b) Landscaping to front	Landscaping treatment of front setback to be compatible with streetscape.	The proposed stepped setback from 3.5m (northern end) to 5.2 (southern) end is consistent with the pre-DA lodgment advice, and acceptable having regard to the prevailing streetscape character. Plantings are proposed within the front setback area. A condition is recommended to require suitable trees to enhance the streetscape amenity.	Yes

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29(2)(c) Sunlight	Communal living spaces to receive three (3) hours sunlight mid-winter.	The proposed communal room in the north-eastern corner of the building will receive in excess of three hours solar access during mid-winter.	Yes
29(2)(d) Private open space	Minimum 20m ² (at least 3m wide) for lodgers. Minimum 8m ² (at least 2.5m wide) for manager.	A communal open space area of 42m ² is provided at ground floor level. A secondary open space area of 134m ² is provided at the rear. Each of the nine (9) ground floor boarding rooms also has a private terrace, which will enhance the internal amenity of these rooms.	Yes
29(2)(e)(iia) Car parking	The site is located in an accessible area (the site is located within 155m walking distance from a bus stop, which meets the frequency requirements under the Policy). Car parking requirement is 1 x space for each boarding room. This triggers a requirement of (0.5 x 35) = 17 spaces. Not more than 1 space for each person employed in connection with the development who resides on the site.	17 spaces Nil	Yes Yes
29(2)(f) Minimum accommodation size	Minimum accommodation size (excluding kitchenette and bathroom): 12m ² for single lodger 16m ² for doubles	Single rooms = 13m ² - 13.80m ² (excluding kitchenette and ensuite bathroom) Double rooms = 16.54m ² - 22m ² (excluding kitchenette and ensuite bathroom)	Yes
30(a) Communal living room	At least one communal living room if greater than five rooms proposed.	A communal room of 45.2m ² is proposed	Yes
30(b) Maximum room size	25m ² (excluding kitchenette and bathroom)	Maximum room size is 22.7m ² (excluding kitchenette and ensuite)	
30(c) Occupancy	Maximum 2 persons per room	Rooms will accommodate a maximum of 2 lodgers.	Yes (Conditioned)
30(d) Internal amenities	Adequate bathroom and kitchen facilities to be provided	Each boarding room contains an ensuite bathroom and kitchenette	Yes
30(e) Boarding house manager	On-site manager required for 20 or more lodgers	Proposed capacity is 63 lodgers. One Manager's room is proposed adjacent the communal room and reception	Yes
30(h) Motorcycle and bicycle parking	0.2 bicycle and motorcycle spaces per room. 7 of each required	14 bicycle spaces + 9 motorcycle spaces proposed	Yes
30A	Design compatible with local area	The proposed development is	Yes

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Design		considered to be contextually appropriate in terms of land-use, bulk and scale. The development has been amended to provide an appropriate transition to adjoining low density residential land to the south and west.	
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STRATHFIELD LOCAL ENVIRONMENTAL PLAN (SLEP) 2012

An assessment of the proposal against the general aims of SLEP 2012 is included below:

Cl. 1.2(2) Aims	Complies
(a) <i>To achieve high quality urban form by ensuring that new development exhibits design excellence and reflects the existing or desired future character of particular localities and neighbourhoods in Strathfield</i>	Yes
(b) <i>To promote the efficient and spatially appropriate use of land, the sustainable revitalisation of centres, the improved integration of transport and land use, and an appropriate mix of uses by regulating land use and development</i>	Yes
(c) <i>To promote land uses that provide a wide range of employment, recreation, retail, cultural, service, educational and other facilities for the local community</i>	Yes
(d) <i>To provide opportunities for economic growth that will enhance the local community</i>	Yes
(e) <i>To promote future development that integrated land use and transport planning, encourages public transport use, and reduced the traffic and environmental impacts of private vehicle use</i>	Yes
(f) <i>To identify and protect environmental and cultural heritage</i>	Yes
(g) <i>To promote opportunities for social, cultural and community activities</i>	Yes
(h) <i>To minimise risk to the community by identifying land subject to flooding and restricting incompatible development</i>	Yes

Permissibility

The subject site is Zoned B4 Mixed Use under Strathfield Local Environmental Plan (SLEP) 2012.

Boarding houses are permissible within the B4 Mixed Use Zone with consent.

The proposed development is consistent with the definition of *boarding houses* under SLEP 2012.

Zone Objectives

An assessment of the proposal against the objectives of the B4 Mixed Use zone is included below:

Objectives	Complies
➤ <i>To provide a mixture of compatible land uses.</i>	Yes
➤ <i>To integrate suitable business, office, residential, retail and other development in accessible locations so as to maximise public transport patronage and encourage walking and cycling.</i>	Yes
➤ <i>To facilitate mixed use urban growth around railway stations and transport nodes and corridors, commercial centres and open space.</i>	Yes
➤ <i>To provide local and regional employment and live and work opportunities</i>	No

Comments:

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The original proposal included a commercial tenancy at ground floor. Given the location of the development adjacent R2 zoned land to its south and west (**Figure 22**), it was considered that the commercial tenancy would most likely be difficult to lease. The amended proposed with a communal space area in its place is considered to deliver a better planning outcome by encouraging greater activation and passive surveillance of the adjoining public domain.

The proposed development provides residential accommodation in an accessible location to employment opportunities, an in this regard satisfies the objectives of the B4 Mixed Use zone.

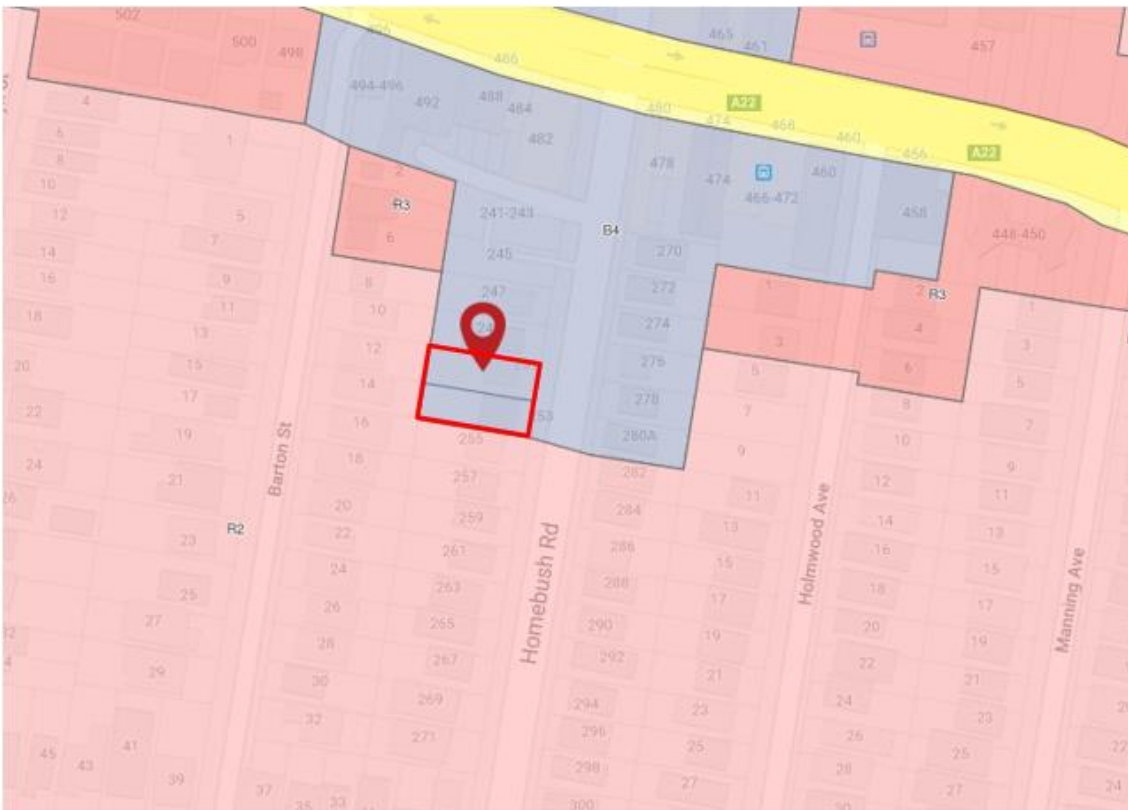


Figure 22. SLEP 2012 Zoning Map, showing the B4 Mixed Use zoning of the site and adjoining land zoned R2 Low Density Residential.

Part 4: Principal development standards

An assessment of the proposal against the relevant provisions contained within Part 4 of the SLEP 2012 is provided below.

Height of building

Cl.	Standard	Controls	Proposed	Complies
4.3	Height of building	11m	10.5m	Yes

	Objectives	Complies
(a)	To ensure that development is of a height that is generally compatible with or which improves the appearance of the existing area	Yes
(b)	To encourage a consolidation pattern that leads to the optimum sustainable capacity height for the area	Yes
(c)	To achieve a diversity of small and large development options.	Yes

Floor space ratio

Cl.	Standard	Controls	Proposed	Complies
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4.4	<i>Floor space ratio</i>	1.4:1	1.21:1	Yes
	Objectives			Complies
(a)	<i>To ensure that dwellings are in keeping with the built form character of the local area</i>			Yes
(b)	<i>To provide consistency in the bulk and scale of new dwellings in residential areas</i>			Yes
(c)	<i>To minimise the impact of new development on the amenity of adjoining properties</i>			Yes
(d)	<i>To minimise the impact of development on heritage conservation areas and heritage items</i>			Yes

Part 5: Miscellaneous Provisions

The relevant provisions contained within Part 5 of the SLEP 2012 are addressed below as part of this assessment:

5.6 Architectural roof features

No roof top plant is proposed. The roof form is contained within the prescribed building height of 11m.

5.10 Heritage Conservation

The applicant has contacted Sydney Water, who is presently undertaking an evaluation of the development's impact on the Sydney Water owned *pressure tunnel and shafts* which traverse the site in an east/west direction and are listed on the State Heritage Register.

The Department of Planning & Environment website states the following with respect to the *pressure tunnel*:

"The physical boundary curtilage of the Pressure Tunnel is to be taken as a distance of 3 metres around the existing infrastructure (NB this is not necessarily all owned by Sydney Water). The infrastructure associated with this item includes the original fabric and archaeological evidence including but not limited to the tunnel, vertical shafts and buildings attached to the shafts. The visual curtilage is restricted to the shafts from which access may be gained. Most of the pipeline is located below ground."

In light of the above findings, the following conditions are recommended:

- Approval is gained from the Heritage Council for the proposed works. The reason for this condition is that the proposed excavation may disturb an archaeological relic.
- Written confirmation is obtained from Sydney Water confirming that the proposed development will not have any adverse impact on the significance of the *pressure tunnel and shafts*.

Part 6: Local Provisions

The relevant provisions contained within Part 6 of the SLEP 2012 are addressed below as part of this assessment:

6.1 Acid sulfate soils

The site is identified as having Class 5 acid sulfate soils but is not within 500m of land within Class 1, 2, 3 or 4. A *Geotechnical Investigation Report*, prepared by *Geo-environmental Engineers*, confirms that an Acid Sulfate Soils Management Plan is not required.

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6.2 Earthworks

Excavation to accommodate a single basement car parking level is proposed. The matters of consideration in relation to *earthworks* have been assessed by Council's engineer, who has determined the proposed excavation to be satisfactory, subject to conditions.

6.3 Flood planning

The subject site is not flood affected.

Council's Engineer notes that the OSD tank has been designed with internal mechanisms to negate the likelihood of internal flooding.

6.4 Essential services

The subject site is located within an existing urban area with all essential services available.

4.15(1)(a)(ii) any draft environmental planning instruments

There are no applicable draft planning instruments that are or have been placed on public exhibition, to consider as part of this assessment.

4.15 (1)(a)(iii) any development control plan

STRATHFIELD CONSOLIDATED DEVELOPMENT CONTROL PLAN (SCDCP) 2005

The following is an assessment of the proposal's compliance with the relevant provisions contained within SCDCP 2005.

PART H - WASTE MANAGEMENT (SCDCP 2005)

A *Waste Management Plan* has been submitted with the application. This proposes kerbside collection of waste, and bases the number of bins proposed on a *typical occupancy* rate of 45 lodgers, rather than the potential maximum of 63 lodgers.

Section 3.3.1(a) of Part H of SCDCP 2005 states the following:

“Development for the purposes of multi-unit housing, residential flat buildings, serviced apartments, mixed use and commercial development must provide onsite, underground collection of waste.”

Section 3.3.2 sets out the circumstances in which an exemption to underground collection may be considered:

“On a merit basis, Council may consider alternative onsite collection arrangements to underground waste collection, provided the applicant can justify this departure with relevant reports. Only in exceptional circumstances will Council consider kerb-side collection for the above development types.

A decision to support an exemption to underground or onsite collection will be based on assessment of a range of factors, such as ability to comply with the broader objectives of the SCDCP, site constraints, locality constraints, scale of the development and alternative options.”

Section 3.3.3(a) states that where an exemption for onsite collection is deemed appropriate, alternative solutions including a loading dock facility or collection from an on-site, at grade location where bins are transferred to the collection area, may be considered.

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Due to the physical constraints of the site, the alternative onsite collection arrangements above are not viable.

Whilst Section 3.3.1(a) does not strictly apply to *Boarding houses*, due to the high-traffic volume along Homebush Road and the number of boarding rooms proposed, kerbside collection is not considered appropriate. In this regard, it is recommended that bins are collected directly from the bin store using a private waste contractor.

The proposed internal height clearance of the basement appears to be sufficient to accommodate a medium rigid vehicle. A condition is recommended, which requires evidence of a service agreement between the applicant and a private waste contractor to be provided. Furthermore, plans are to be detailed showing compliance with relevant Australian Standards and evidence that vehicles are able to exit the site in a forward direction.

In accordance with Part H of the SCDP 2005, waste storage is to be provided at the following weekly rates:

General Waste: 60L / occupant space
Recycling: 20L / occupant space

Having regard to the above rates and based on a maximum of 63 lodgers at any one time, 3780L (16 x 240L general waste bins) and 1260L (6 x 240L recycling bins) are required.

The proposed bin store is capable of accommodating 16 X 240L bins. A condition is recommended to require the bin store to be amended to meet Council's requirements.

Part I – Provision of off-street parking facilities (SCDCP 2005)

Part B – Main Provisions

The proposal is acceptable with regards to Section 2.0 of Part I of SCDP 2005, subject to compliance with the following recommended conditions:

- all off-street car parking is to comply with *AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking*; and
- all vehicles must be able to exit the site in a forward direction.

Part C: Parking Schedule

The on-site parking requirements for the proposed development are as follows:

Use	No. of rooms	Parking rate	Loading / unloading	Minimum spaces required	Spaces provided	Complies
Boarding houses	36 boarding rooms including one Managers Room.	1 space per bedroom plus 1 space per 6 employees plus 1 space for the manager	Nil	36	17	No

The car parking requirement under Council's DCP is inconsistent with the car parking requirement under the SEPP (ARH) 2009. The proposed on-site car parking provision of 17 spaces complies with the seventeen spaces required under the SEPP (ARH) 2009, and is therefore supported.

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Part Q – URBAN DESIGN

An assessment of the proposal against the objectives and development controls contained within Part Q of SCDCP 2005 is included below:

2.1: Public Domain and Place Making

2.1.1	Objectives	Satisfactory
A.	To ensure that development adjacent to the Public Domain complements the landscape character, public use and enjoyment of that land	✓
B.	To enhance the quality of the Public Domain	✓
C.	To ensure the Public Domain is attractive, safe, interesting, comfortable, readily understood and easily accessed	✓
2.1.2	Development Controls	Complies
1.	All development applications involving substantial external changes that are visible from or effect public space, or have significant land use implications, must be designed in accordance with this DCP Part Q to ensure a positive contribution to the public environment.	✓
2.	Development is to be designed to address elements of the public domain, including the building interface between private and public domains, circulation patterns and access ways, gateways, nodes, edges, landscape features, heritage items, ground floor activity and built form definition to the street.	✓
3.	Public access to the public domain is to be maximised	✓
4.	Development is to be located to provide an outlook to the public domain, without appearing to privatise that space.	✓
5.	Development is to provide passive surveillance to the public domain. Where appropriate, ground floor areas abutting public space should be occupied by uses that create active building fronts with pedestrian flow, and contribute to the life of the streets and other public spaces.	✓
6.	Continuous lengths of blank walls and fences at the public domain interface are to be avoided	✓

2.2: Streetscape

2.2.1	Objectives	Satisfactory
A.	To ensure that all development contributes positively to the street and locality.	✓
B.	To ensure new development responds to, reinforces and sensitively relates to the spatial characteristics of the existing urban environment.	✓
C.	To increase the legibility of streetscapes and urban spaces so that the interrelationship between development and the Public Domain is visually coherent and harmonious.	✓
D.	To maximise opportunities for buildings to define the Public Domain.	✓
E.	To encourage attractive street frontages and improve pedestrian amenity	✓
2.2.2	Development Controls	Complies
1.	Building height at the street frontage and building alignment must	✓

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	maintain a compatible scale with adjacent development, whilst having regard to this Plan's height controls;	
2.	Buildings and fences must be designed to complement and/or visually improve existing streetscapes	✓
3.	Development must respond and sensitively relate to the broader urban context including topography, block patterns and subdivision, street alignments, landscape, views and vistas and the patterns of development within the area (refer Figure 1a)	✓
4.	Building design and landscaping must be in harmony with the form, mass and proportions of the streetscape.	✓
5.	New buildings must recognise and reinforce the elements of facades within the street. Designs must have regard to the horizontal and vertical proportions of building elements which create the visual scene	✓
6.	New buildings must recognise and reinforce the elements of facades within the street. Designs must have regard to the horizontal and vertical proportions of building elements which create the visual scene	✓
7.	Buildings on corner sites must be designed and articulated to address each street frontage and must define corners (refer Figure 4).	n/a
8.	Development adjoining land use zone boundaries must provide a transition in form, height, scale, appearance, materials and setbacks with adjoining development and the Public Domain	n/a
9.	Buildings must be constructed of suitably robust and durable materials which contribute to the overall quality of the streetscape	✓
10.	The use of security devices, such as roller shutters or grilles on shopfronts, shall not compromise natural surveillance of streets and public places. Solid roller shutters will not be permitted as security devices on shop fronts (windows and doors).	✓
11.	Where side setbacks are an important part of the local streetscape character, these are to be maintained	✓

Comments:

Streetscape

The proposal has been amended to improve the presentation of the building façade to the public domain. It is recommended that suitable trees are incorporated into the front setback landscape plan capable of reaching a minimum height of 6m at maturity, in order to enhance the streetscape amenity.

It is further recommended that the proposed FB2 *Face Brick – 'Miro'* is replaced with FB1 *Face Brick: 'Gamet'* as proposed in order to achieve greater consistency with adjoining residential development.

2.3: Siting

2.3.1	Development Controls	Complies
1.	Development must respond to the scale of surrounding buildings and definition of the street networks and public spaces.	✓
2.	The distinctive and valued character of the surrounding area, particularly those elements that contribute to a sense of place and identity, must be protected and enhanced.	✓

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3.	Building forms (including heights and massing) are to be arranged to reinforce the future desired structure and character of the area	✓
4.	Buildings must address the street, laneway, new through-site link or open space.	✓
5.	Street edges must be defined with low rise buildings or appropriately scaled podiums to create a pedestrian scale and active frontages at street level.	✓
6.	Appropriate building separation must be provided to protect privacy and solar access to private property and the Public Domain.	✓
7.	Building corners on key streets must be emphasised to signify key intersections and enhance Public Domain legibility.	n/a
8.	Possible future development on adjoining sites must be considered as part of any design	✓

2.4: Building Envelope

2.4.1	Objectives	Satisfactory
A.	To ensure the scale and bulk of future development is compatible with site conditions, surrounding development and the existing and desired future character of the streetscape and locality.	✓
2.4.2	Development Controls	Complies
1.	The bulk and scale of any development must reflect the existing and future character of the existing street and surrounding locality	✓
2.	The bulk and scale of any development must be compatible with the amenity of the immediately and surrounding locality.	✓
3.	Buildings must not be designed to be outside the building envelope even if they do not achieve 100% of the permissible Gross Floor Area (GFA) or maximum height permitted.	✓

2.5: Building Massing and Scale

2.5.1	Objectives	Satisfactory
A.	To ensure buildings are compatible in form relative to the spatial characteristics of the local area.	✓
B.	To ensure building mass and form reinforces, complements and enhances the visual character of the street.	✓
C.	To ensure the building height and mass preserves and enhances the Public Domain, neighbourhood amenity, and site characteristics.	✓
D.	To ensure that where changes in building scale, mass and/or height is proposed, it occurs in a manner that is sensitive to amenity issues of surrounding or nearby development.	✓
2.5.2	Development Controls	Complies
1.	Buildings must be of a height that responds to the topography and shape of the site.	✓
2.	Buildings, or their individual elements, must be appropriately scaled to reinforce the surrounding character	✓

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3.	Building heights are to be reduced and setbacks increased to provide appropriate transitions to heritage buildings and places or sensitive uses such as public recreation areas and schools.	✓
4.	The proportion and massing of buildings must relate favourably to the form, proportions and massing of existing and proposed building patterns in the street.	✓
5.	Building height and mass must not result in loss of amenity to adjacent properties, open space or the Public Domain.	✓
6.	The form and massing of buildings must provide a transition between adjoining land use zones and building types.	✓
7.	Building form and massing must support individual and communal entries.	✓

2.6: Transition Zones

2.6.1	Development Controls	Complies
1.	Development proposing to be higher than adjoining development must incorporate gradual stepping up of the built form at its interface with existing low-rise development. Where there is a common boundary between areas where a different height limit is specified, one solution may be that the top storey of the development on the land with the higher height limit be stepped back to fit within a plane projected at a 45° angle from the floor below the topmost floor (refer Figure 7)	✓
2.	Development proposals are to be sensitive and complementary in scale and site location to surrounding properties of identified heritage and/or streetscape value, and which contributes positively to the desired character of the street or area concerned (refer Figure 8)	✓

2.7: Building Frontages to the Public Domain

2.7.1	Objectives	Satisfactory
A.	To ensure the appearance of buildings complement and enhance neighbourhood and streetscape character.	✓
B.	To encourage contemporary designs which integrate with the appearance of the streetscape.	✓
C.	To provide attractive building facades which establish identity and contribute to the streetscape.	✓
2.7.2	Development Controls	Complies
1.	Building design and architectural style must interpret and respond to the positive character of the locality, including the dominant patterns, textures and compositions of buildings	✓
2.	Demonstrated design consideration must be given to the underlying building elements that contribute to the character of the area. Such things include roof shape, pitch and overhangs; entry porches, verandas, balconies and terraces; materials, finishes, fixtures, patterns, fenestrations, colours and detailing; the location and proportion of windows and doors.	✓
3.	Building facades must be modulated in plan and elevation and articulated to reduce the appearance of building bulk and to express the elements of the building's architecture	✓
4.	Alterations and additions must be compatible with design elements of the	n/a

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	existing building.	
5.	Building frontages and entries must provide a sense of address and visual interest from the street. Stairwells must not be located at the front and in view of the Public Domain	✓
6.	Where security grilles/screens, ventilation louvres and car park entry doors are proposed, they must be integrated into facade designs. Solid security shutters will not be permitted.	✓
7.	New buildings and facades must not result in glare that causes discomfort or threaten the safety of pedestrians or motorists. A Reflectivity Report that analyses the effects of potential glare from the proposed new development on pedestrian and motorists may be required by Council	✓ (reflectivity report not required)
8.	Large areas of blank walls are not acceptable. Measures to avoid this may include windows, awnings, sun shading devices, pergolas, or a recognisable increased setback to the upper storey	✓
9.	New business and industrial buildings shall be designed so that entry points and client service areas are easily identified from the street and are clearly linked to car parking areas and pedestrian paths.	n/a
10.	Where dwelling houses do not face the street, they are to have recognisable entries and a sense of address as they would if they faced the street.	n/a
11.	For commercial and mixed-use development: (i) ground floor activities must activate the adjoining Public Domain to create a vibrant streetscape and promote a sense of community. Buildings shall be carefully designed to ensure active frontages contributes to the liveliness, interest, comfort and safety of the street for those who use it; and (ii) awnings or colonnades for weather protection and shade must be provided along active frontages.	n/a n/a
12.	Site services and related enclosures (such as for waste disposal and recycling, mail and deliveries, water and energy metering and emergency services) are to be integrated into the design of the development and not detract from the streetscape.	✓
13.	Development must respond to the positive attributes of an area by incorporating dominant patterns, textures and compositions into the built form.	✓
14.	Development must provide a sense of address and visual interest from the street through the use of insets and projections that create interest and, where relevant, the appearance of finer grain buildings. Recesses that undermine the safety of the Public Domain are to be avoided.	✓
15.	Building materials, finishes and colours must be of a high quality and compatible with those qualities that are dominant in and contribute to the streetscape and locality.	✓
16.	The reflectivity of external materials and finishes (including roofs and walls) must be minimal in accordance with industry standards. The use of reflective glass and curtain walling as a facade treatment is not generally favoured by Council. Council may require the lodgement of manufacturer's specifications of certain materials and finishes to demonstrate adequately low levels of glare and reflectivity from external surfaces in certain circumstances.	✓

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2.8: Roof Forms

2.8.1 Objectives		Satisfactory
A.	To treat roof spaces and forms as an important element of the overall building appearance.	✓
B.	To encourage roof forms that provide continuity and consistent character in the streetscape.	✓
C.	To encourage roof designs that integrate with the building composition and form.	✓
2.8.2 Development Controls		Complies
1.	Plant and lift overrun structures must be incorporated into the roof design. Plant equipment, vents or lift over-runs or solar energy and stormwater collectors are to be designed to avoid visibility from the surrounding spaces and buildings.	No roof plant is proposed – refer comments
2.	The roof is to be designed to provide for rainwater and solar energy collection.	
3.	The proposed roof form shall minimise the appearance of bulk and scale of the building and be treated as an important architectural element in the street, which can reinforce continuity and character	✓
4.	Roof forms are to respond to the neighbouring roofs, in particular in terms of scale and pitch. Roof forms should complement, but not necessarily replicate the predominant form in the locality and in particular those of adjacent buildings.	✓
5.	Roofs must be designed to avoid or minimise loss of views from adjacent and nearby properties and public spaces, however, this does not justify a roof form that is inconsistent with the prevailing streetscape character.	✓
6.	Attics are to be designed to fit within the building envelope with the exception of dormer windows.	n/a

Plant

No roof plant or details of any car park ventilation plant or air conditioning plant have been submitted with the DA.

A condition is recommended to require that no plant is installed on the roof or in a location that is visible from adjoining properties or the public domain.

3.1: Accessibility and Connectivity

3.1 Objectives		Satisfactory
A.	To improve pedestrian access and connectivity between housing, open space networks, community facilities, public transport, local activity centres and schools.	✓
B.	To encourage pedestrian through-site links that are designed to promote safety and amenity.	n/a
3.1.2 Development Controls		Complies
1.	Pedestrian links must be provided where possible through large development sites to improve connectivity between housing, open space networks, community facilities, public transport, local activity centres and	n/a

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	schools. Where Council considers it appropriate to have through site links, it will consider the public benefit that is derived from this in terms of potential loss of development potential.	
2.	Through-site links must be arranged on the site to enable casual surveillance from buildings on the site and from the street or Public Domain. Through-site links are to be landscaped appropriately and include provision for appropriate lighting.	n/a
3	Public, communal and private areas must be clearly delineated within the site	✓
4.	Pedestrian and cycle links must be provided on sites adjacent to waterways to improve accessibility to these natural systems.	n/a
5	Existing through-site pedestrian links are to be retained by all types of development, except where alternative access can be provided to Council's satisfaction.	n/a

3.2: Building Entries

3.2.1	Objectives	Satisfactory
A.	To create street entrances with a strong identity that provide a transition from the street to residential interiors.	✓
B.	To ensure car park entries do not detract from the street	✓
3.2.2	Development Controls	Complies
1.	Legible entry/lobby areas accessed from a public street are to be provided to encourage surveillance and activation of the Public Domain, thereby increasing safety.	✓
2.	Strong visual and physical connections must be provided between the street and lobby spaces. Entries and foyers must be designed to be comfortable, sheltered, safe, convenient and visible at all times of day and night.	✓
3.	Car park entrances and crossovers are not permitted unless there is no alternative in retail and commercial areas. Entrances must be located off streets that have a predominantly service role, and these streets should be upgraded as necessary to cater for this role.	✓ (no alternative to Homebush Road)
4.	Pedestrian access must be incorporated with car park entrances to reduce the visual impact of the car park entrance. Car park entrances must be carefully designed to avoid unattractive or extensive gaps in street frontages.	✓
5.	New commercial and industrial buildings must be designed so that entry points and client service areas are easily identified from the street and are clearly linked to car parking areas and pedestrian paths.	n/a
6.	Where a dwelling house does not face the street, it must have a recognisable entry and a sense of address.	n/a

3.3: Visual and Acoustic Privacy

3.3.1	Objectives	Satisfactory
A.	To ensure that development does not cause unreasonable overlooking of	✓

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	habitable rooms and principal private open spaces of dwellings.	
B.	To ensure that visual privacy is provided both within a development and between a development and its neighbours.	✓
C.	To ensure that the siting and design of development minimises the impacts of noise transmission between properties.	✓
3.3.2	Development Controls	Complies
1.	New development must ensure adequate visual and acoustic privacy levels for neighbours and residents	✓
2.	Development must be located, oriented and designed to maximise visual and acoustic privacy between buildings.	✓
3.	The internal layout of buildings must be designed to minimise overlooking of living areas, private open spaces and adjoining school yards.	✓
4.	Building elements such as balconies and decks must be designed to minimise overlooking of living areas, private open spaces of adjoining dwellings and adjoining school yards	✓
5.	The windows of dwellings must be located so they do not provide direct and close views into the windows of other dwellings, particularly those of living areas (refer Figure 9).	✓
6.	Building design elements shall be used to increase visual and acoustic privacy such as recessed balconies and/or vertical fins between adjacent balconies, oblique windows, fencing, vegetation and louvres and pergolas which limit overlooking of lower dwellings, private open space and adjoining school yards (refer Figure 10).	✓
7.	The internal layout of buildings including windows must be designed so as to reduce the effects of noise transmission.	✓
8.	Appropriate building materials shall be used to provide acoustic privacy.	✓
9.	Consideration to the relationship between residential and non-residential components of mixed use development with regard to noise attenuation and privacy must be demonstrated in the design of the development.	✓

3.4: Acoustic Amenity and Air Quality

3.4.1	Objectives	Satisfactory
A.	To ensure that the siting and design of buildings minimises noise impacts from abutting roads, rail corridors and other noise-generating land uses.	✓ (Acoustic assessment report submitted)
B.	To ensure that new commercial or industrial development does not unreasonably diminish the amenity of nearby residential uses by noise intrusion.	n/a
C.	To ensure mitigation measures such as building layout and design and building materials are taken into consideration where poor air quality is likely to affect inhabitants.	n/a
3.4.2	Development Controls	Complies
1.	Where dwellings are proposed within proximity to noise-generating land uses such as major roads and rail corridors; entries, halls, storage rooms, bathrooms and laundries must be located on the noise affected side of each dwelling and should be able to be sealed off by doors from living	✓

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	areas and bedrooms where practicable.	
2.	Where dwellings are proposed within proximity to noise-generating land uses, appropriate materials with acoustic properties shall be incorporated into the development	✓
3.	New non-residential development must not adversely affect the amenity of adjacent residential development in terms of noise, odour, poor air quality, hours of operation and/or service deliveries.	✓
4.	Noise generating developments particularly those adjacent residential developments and residential developments adjacent to noise generating sources such as busy roads and rail corridors, must submit an Acoustic Report prepared by a suitably qualified acoustic consultant with a development application	✓
5.	The provisions of State Environmental Planning Policy (Infrastructure) 2007 and Development near Rail Corridors and Busy Roads Interim Guideline must be taken into consideration, to minimise impacts of busy roads and railway corridors on residential and other sensitive development such as schools, child care centres, places of public worship and health services facilities when designing a development proposal	✓
6.	For residential development: – internal habitable rooms of dwellings affected by high levels of external noise must be designed to achieve internal noise levels of no greater than 50dBA; – The internal layout of buildings/dwellings shall be designed so as to minimize the impacts of poor air quality on inhabitants.	✓ (condition recommended)

Mechanical plant

The mechanical plant including the carpark ventilation plant and air conditioning plant have not yet been selected for this development.

The submitted *Acoustic Report* recommends that noise emissions from the mechanical plant are assessed at the *Construction Certificate* stage and/or once the plant selection is finalised. The mechanical plant and air conditioning units should be acoustically treated to reduce the noise emission level at nearby residences to levels complying with the noise criteria in Section 3.5 of the *Acoustic Report*. A condition to this effect is included within the Recommendation.

3.5: Solar Access and Cross Ventilation

3.5.1	Objectives	Satisfactory
A.	To provide thermal comfort for occupants.	✓
B.	To ensure that development does not unreasonably diminish sunlight to neighbouring properties and within the development site.	✓
C.	To ensure that sunlight access is provided to private open space and habitable rooms to improve amenity and energy efficiency.	✓
D.	To ensure sufficient volumes of fresh air circulate through buildings to create a comfortable indoor environment and to optimize cross ventilation.	✓
E.	To ensure that sunlight access is provided to the Public Domain.	✓
3.5.2	Development Controls	Complies
1.	Development must be designed and sited to minimise the extent of	✓

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	shadows that it casts on: - private and communal open space within the development; - private and communal open space of adjoining dwellings; - significant areas of the Public Domain, such as main streets, open space and plaza areas, main pedestrian links etc. - solar collectors of adjoining development; and - habitable rooms within the development and in adjoining developments. Note: Building setbacks may need to be increased to maximise solar access and to minimise overshadowing from adjoining buildings. Building heights may also need to be stepped to maximise solar access.	
2.	Generally, dwellings within the development site and adjoining properties are to receive a minimum of 3 hours sunlight in habitable rooms and in at least 50% of the private open space between 9am and 3pm on 21 June. Where existing development currently receives less sunlight than this requirement, this should not be unreasonably reduced. In order to demonstrate that this can be achieved, shadow diagrams may be required with the development application.	n/a (ARH SEPP 2009 prevails)
3.	Living areas of dwellings such as kitchens and family rooms shall be located on the northern side of dwellings and service areas such as laundries and bathrooms to the south or west.	n/a
4.	In habitable rooms, head and sill heights of windows must be sufficient to allow sun penetration into rooms.	✓
5.	Landscaping must provide shade in summer without reducing solar access in winter.	✓
6.	Buildings must have narrow cross sections, providing dual aspect for dwellings to allow for cross ventilation.	✓
7.	Buildings must be orientated to benefit from prevailing breezes.	✓
8.	All rooms must contain an external window to provide direct light and ventilation. Exceptions may be considered for non-habitable rooms where this cannot be achieved practicably and mechanical ventilation can be provided.	✓
9.	Natural cross ventilation shall be achieved by locating window openings in opposing walls and in line with each other.	✓
10.	Building elements such as operable louvres and screens, pergolas, blinds etc. shall be used to modify environmental conditions where required, such as maximizing solar access in winter and sun shading in summer.	✓

3.6: Safety and Security

3.6.1	Objectives	Satisfactory
A.	To ensure a safe physical environment by promoting crime prevention through design.	✓
B.	To encourage increased use of shopping centres, particularly at night.	✓
C.	To create a balance of uses that are safe and easily accessible	✓
D.	To ensure there is adequate lighting and signage.	✓
E.	To reduce crime risk and minimise opportunities for crime.	✓
F.	To increase and contribute to the safety and perception of safety in public and semi-public spaces.	✓
G.	To encourage the consideration and application of crime prevention	✓

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	principles when designing and siting buildings and spaces.	
H.	To encourage dwelling layouts that facilitate safety and encourage interaction and recognition between residents.	✓
3.6.2 Development Controls		Complies
1.	Development must be designed to incorporate and/or enhance opportunities for effective natural surveillance by providing clear sight lines between public and private places, installation of effective lighting, and the appropriate landscaping of public areas.	✓
2.	Development must be designed to minimise opportunities for crime through suitable access control. Physical or symbolic barriers should be used to attract, channel and/or restrict the movement of people. Landscaping and/or physical elements may be used to direct people to destinations, identify where people can and cannot go and restrict access to high crime risk areas such as car parks.	✓
3.	Development must incorporate design elements that contribute to a sense of community ownership of public spaces. Encouraging people to gather in public spaces through appropriate design techniques, helps to nurture a sense of responsibility for a place's use and condition.	✓
4.	Building entrances must be clearly identified and accentuated. Entries and associated elements including signs, street numbers, post boxes, landscaping etc. must be designed to emphasise their visible presence from various locations or approaches to the building.	✓
5.	Entrances must serve as points of orientation or way-finding within the development, and providing clear sightlines and visual connections between the street, the entry, foyers and residential interiors.	✓
6.	The installation of solid security shutters as a means of defining the boundaries between public and private spaces will not be supported.	✓ (no solid security shutters proposed)
7.	The incorporation of crime prevention measures in the design of new buildings and spaces shall not to detract from the quality of the streetscape. Subtle design techniques must blend into façades and places and be integrated with the overall design of the development.	✓
8.	A site management plan and formal crime risk assessment (Safer by Design Evaluation) involving the NSW Police Service may be required for large developments, which in Council's opinion, would create a crime risk	n/a
9.	The design of buildings adjoining laneways and through site connections must be designed to activate these spaces at ground level and provide casual surveillance from ground and upper levels	n/a
10.	Lighting of laneway space is required	n/a

3.8: Views

3.8.1 Objectives		Satisfactory
A.	To provide view sharing for both existing and proposed and future residents	✓
B.	To minimise disruption to views from adjacent and nearby development and views to and from public spaces, including accumulated view loss i.e. 'view creep'.	✓

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3.8.2	Development Controls	Complies
1.	The design of any development must minimise the loss of views from neighbouring and nearby dwellings and from public spaces.	✓
2.	Views between and over buildings are to be maximised and exceptions to side boundary setbacks will not be considered if they contribute to loss of primary views from living areas.	✓
3.	View assessment is to be carried out having regard to the Land and Environment Court Planning Principle in respect of view sharing, which was set out in Tenacity Consulting v Warringah Council (2004) NSWLEC40 ("Tenacity").	n/a

3.9: Landscaping

3.9.1	Objectives	Satisfactory
A.	To enhance the existing streetscape and promote a scale and density of planting that softens the visual impact of buildings	✓
B.	To ensure developments make an equitable contribution to the landscape setting of the locality	✓
C.	To maximise the provision of open space for recreational needs of the occupier and provide privacy and shade	✓

3.9.2	Development Controls	Complies
1.	The design, quantity and quality of open space must respond to the character of the street and surrounding area and contribute to the garden character of Strathfield.	✓
2.	Existing trees within the front setback shall be retained. Front setback areas must contain at least two (2) canopy trees adjacent the front boundary and comprise at least 50% of the setback is to be for deep soil planting.	✓ (conditioned to require suitable trees)
3.	Existing trees on Council's Road Reserve area must be retained and protected. New driveway locations that necessitate removal of a Council street tree will not be supported.	✓ (Conditioned for retention and protection)
4.	In higher density areas the provision of adequate private open space and landscaped areas must maximise residential amenity. Site works must be minimised to protect natural features.	✓
5.	Landscaping must be designed to protect the amenity of existing and future residents and minimise the impact of new development, including alterations and additions, on privacy, views, solar access and general amenity of adjoining and nearby properties including noise and vibration impacts.	✓
6.	Where the landscape pattern in the prevailing streetscape and surrounding locality is desirable, this must be retained.	✓
7.	In areas adjacent to native habitat, the design of development must be sympathetic to the natural environment in order to protect and enhance the area as habitat for native fauna	n/a
8.	In relation to conservation and energy efficiency, plant species must be retained, selected and planted to achieve: <ul style="list-style-type: none"> - shaded buildings in summer; - reduced glare from hard surfaces; 	Details of plant species are to be confirmed at CC stage.

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- sunlight access into living rooms in cooler months;
- cooling air currents channelled into the dwelling in summer; and
- windbreaks where desirable.

9.	Natural features on the site, such as trees, rock outcrops, cliffs, ledges, Indigenous species and vegetation communities must be retained and incorporated into the design of development.	n/a
10.	Landscaping must enhance the visual setting and accentuate the design qualities of the built form. Landscaping solutions shall be used to create a screening effect for visually obtrusive land uses or building elements	✓
11.	Trees must be planted at the front and rear of properties to encourage tree canopy, to soften the built environment and to encourage the continuity of the landscape pattern	✓
12.	Landscaping is to be designed so as to minimise overlooking between properties.	✓
13.	The amount of hard surface area shall be minimised to reduce run-off. Run-off from hard surfaces is to be directed to permeable surfaces such as garden beds.	✓ (Stormwater concept plans acceptable)

3.10: Private and Communal Open Space

3.10.1 Objectives	Satisfactory
A. To ensure that private open space is designed to provide residents with quality usable private outdoor living areas for recreational and outdoor activities.	✓
B. To ensure that private open space is designed for privacy, solar access, and is well integrated with living areas.	✓
C. To provide low maintenance communal open space areas for residents that facilitate opportunities for recreational and social activities, passive amenity, landscaping and deep soil planting	✓

3.10.2 Development Controls	Complies
1. Private open space must be: <ul style="list-style-type: none"> - Provided for all dwellings (with the exception of secondary dwellings, which are able to share the private open space of the principal dwelling). Open space within the front setback does not constitute private open space. - Directly accessible from the living area of the dwelling and capable of serving as an extension of the dwelling for relaxation, entertainment and recreation. Designed to ensure visual and acoustic privacy of the occupants of adjacent buildings and within the proposed development. - Located so as to maximise solar access. - - Designed to focus on the quality of the space in terms of its outlook, orientation, relationship to the dwelling, size and shape and its enclosure and landscape treatment. 	n/a (ARH SEPP 209 prevails)
2. Communal open space must be: <ul style="list-style-type: none"> - Located where it is highly visible and directly accessible to the maximum number of dwellings. - Designed with an integral role in the site and include uses such as circulation, BBQ or play areas, and areas of passive amenity, but excludes swimming pools. - Integrated with the deep soil zone to provide a landscaped setting with opportunities for large and medium size tree planting - - Located adjacent to surrounding public open spaces such as reserves 	n/a (ARH SEPP 209 prevails)

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and public through site links where appropriate.

3.11: Energy Efficient Design

3.11.1 Objectives		Satisfactory
A.	To promote sustainable development which uses energy efficiently and minimises non-renewable energy usage in the construction and use of buildings.	✓
B.	To ensure that development contributes positively to an overall reduction in energy consumption and greenhouse gas emissions.	✓
3.11.2 Development Controls		Complies
1.	Where applicable, development must demonstrate compliance with the design principles embodied in the Building Sustainability Index (BASIX). All commitments listed on a BASIX certificate must be marked on all relevant plans and specifications.	✓
2.	For residential development not subject to BASIX, the principles and properties of thermal mass, glazing, insulation and solar energy must be recognised and incorporated into the design of the development.	✓
3.	Water Sensitive Urban Design principles must be integrated into the development through the design of stormwater drainage, on-site detention and landscaping and in the orientation of the development rather than relying on 'end of pipe' treatment devices prior to discharge.	✓
4.	All commercial, mixed-use and residential flat buildings must achieve a 4-6 green star rating in accordance with the Green Building Council of Australia's Green Star program	n/a

4.1: Car Parking and Vehicular Access

4.1.1 Objectives		Satisfactory
A.	Vehicular access points are to be minimised and should not break the continuity of the streetscape. Landscaping should be used to minimise the visual intrusion of vehicular access points.	✓
B.	Garages and parking structures are not to dominate the building facade and front setback	✓
C.	To ensure that the location and design of driveways, parking spaces and other areas used for the movement of motor vehicles are efficient, safe, convenient and are integrated into the design of the development to minimise their visual impact.	✓
4.1.2 Development Controls		Complies
1.	Vehicle access points and parking areas must: <ul style="list-style-type: none"> - be easily accessible and recognisable to motorists; - not disrupt pedestrian flow and safety; - be located to minimise traffic hazards and the potential for vehicles to queue on public roads; and - - minimise the loss of on street car parking, and to minimise the number of access points. 	✓
2.	Car parking and service/delivery areas must be located so that they do not visually dominate either the development or the Public Domain surrounding the development.	✓

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3.	For mixed-use development: – loading/manoeuvring areas must be located within buildings or screened from adjacent residential uses; and – residential non-residential car parking spaces must be physically separated.	n/a
4.	For industrial development: - loading docks must be designed to allow heavy vehicles to enter and leave the site in a forward direction, without interfering with visitor and employee parking; - adequate and suitable on-site receiving areas and parking for trucks and large vehicles must be provided to prevent queuing or off-site parking of such vehicles; - materials for kerbs, gutters, footpaths, walkways and driveways must be selected to resist damage by large vehicles or frequent use; and - – a traffic management plan must be prepared detailing all transport options for the development, including type of transport used, size of trucks and frequency.	n/a
5.	Development on corner sites may be required to accommodate a splay corner to facilitate improved traffic conditions. This matter should be identified at the initial design stage in consultation with Council's development assessment officers.	n/a
6.	Where properties have access to a rear lane or secondary street frontage, parking and servicing access shall be provided from the secondary street/lane.	n/a
7.	Driveways must be designed to avoid a long and straight appearance by using landscaping and variations in alignment.	✓
8.	Car parking areas and vehicle access ways shall be landscaped to integrate sympathetically with the development and the landscape character of the locality.	✓
9.	The area between property boundaries and driveways, access ways and parking spaces must be of sufficient width to enable landscaping and screen planting.	✓ (planting on structures proposed)
10	All parking provision must be designed and sited to respond to and respect the prevailing streetscape. The visual impact of parking within the front setback is to be minimised.	✓
11	The width and number of footpath crossings shall be minimised	✓

4.2: Front Fences

4.2.1	Objectives	Satisfactory
A.	To avoid creating inactive frontages as a result of fencing private open spaces	✓
B.	To ensure fences complement and conserve the visual character of the street and neighbourhood.	✓
C.	To contribute positively to the Public Domain.	✓
4.2.2	Development Controls	Complies
1.	Front fences shall generally be low, open or partially transparent.	✓
2.	Front fences are to be consistent with the existing character or contribute to establishing a new neighbourhood character.	✓
3.	Front fences and landscaping shall allow residents to view street activity, thereby encouraging passive surveillance of the Public Domain.	✓

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4.	Colours and materials of new fences are to be consistent with the associated building and adjoining fences.	✓
5.	Fences must respond to the architectural character of the street and/or area and the buildings that they front, with streetscape character maintained on all street frontages.	✓
6.	Front fences must not be erected where the streetscape is characterised by an absence of front fences. In these instances, landscaping shall be used to create identifiable street address and privacy	✓
7.	Use of continuous lengths of blank walls at street level must be avoided	✓
8.	Suitable planting shall be used to soften the edges of fences at the interface of the Public Domain	✓
9.	Sheet metal fencing is not to be used at the street frontage or forward of the building line or in locations that have an interface with the Public Domain.	✓
10.	Fencing must respond to the topography of the site.	✓
11.	Front fencing, particularly on busy roads, must be designed to ensure an appropriate level of visibility and outlook, informal surveillance, privacy, security and frontage activity	✓
12.	Fences must not be constructed in floodways. Where this is unavoidable fences are to be constructed in a manner that will not restrict the flow of flood water or cause blockages.	✓

4.7: Development Adjoining Residential Zones

4.7.1	Development Controls	Complies
1.	Wherever the Council considers it to be appropriate, proposed buildings must be compatible with the height, scale, siting and character of adjoining buildings within the residential zone.	✓
2.	Any goods, plant, equipment and other material that will be on the land as a consequence of the proposed development shall be stored within a building, or be suitably screened and located at least 3m from the boundary with a residential zone.	✓
3.	The elevation of any proposed building facing land in a residential zone must be compatible with existing buildings within the residential zone, or be suitably screened.	✓
4.	The development must not inhibit reasonable solar access to existing buildings within the residential zone between the hours of 9am and 3pm during the winter solstice.	✓
5.	The development must not otherwise cause nuisance to residents, by way of hours of operation, traffic movement, parking, headlight glare, security lighting or the like.	✓
6.	Windows facing residential areas shall be treated to avoid overlooking of private open spaces or windows of adjoining residences.	✓

4.8: Development within Mixed Use Zones

4.8.1	Development Controls	Complies
1.	Any new mixed-use development must complement significant elements of the built environment in the vicinity of the land;	✓

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2.	Any new mixed-use development must improve the appearance of the built environment in the vicinity of the land, particularly in terms of the relationship of buildings to each other and from the perspective of motorists, pedestrians, employees and users of the Public Domain generally	✓
3.	Any proposal shall be consistent with the established landscaping themes that the Council considers to be desirable.	✓
4.	Activities associated with mixed use developments that impact on the use of the Public Domain such as management arrangements, including ownership/ leasing proposals must be considered at the design stage to ensure proper functioning of various components of the building.	n/a

4.15 (1)(iia) any planning agreement or draft planning agreement

No planning agreement has been entered into under section 7.4 of the *Environmental Planning and Assessment Act 1979*.

(i) matters prescribed by the regulations

Clause 92 of the *Environmental Planning and Assessment (EP&A) Regulation 2000* requires Council to take into consideration the provisions of the Government Coastal Policy and Australian Standard *AS2601-1991: The Demolition of Structures*, in the determination of a development application.

Having regard to these prescribed matters, the proposed development is not located on land subject to the Government Coastal Policy as determined by Clause 92(1)(a)(ii), however does involve the demolition of a building for the purposes of *AS2601 - 1991: The Demolition of Structures*. Suitable conditions are recommended to ensure compliance with *AS2601-1991*.

(ii) any coastal zone management plan

The NSW Government projects sea levels to rise by 40cm in 2050 and by 90cm in 2100 above the relative mean sea level in 1990. These planning benchmarks are to be considered in the assessment of development applications through the applicable coastal zone management plan or alternatively the provisions of the *NSW Coastal Planning Guideline: Adapting to Sea Level Rise*.

Although Council is not subject to a coastal zone management plan, the sea level rise planning benchmarks have also been established in order to assess the likely increase in the frequency, duration and height of flooding and as a consequence likely property and infrastructure damage on affected and potentially affected land. Council is therefore required to consider the impact of sea level rise and resultant flooding from Powell's Creek and Cook's River which are tributaries of Sydney Harbour (Parramatta River) and Botany Bay respectively.

The proposed development is not located on a site that is subject to flooding attributed to either Powell's Creek or Cook's River and is therefore not required to be considered under the provisions of the *NSW Coastal Planning Guideline: Adapting to Sea Level Rise*.

4.15(1)(b) the likely impacts of the development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality

All likely impacts have been addressed elsewhere in the report, or are considered to be satisfactory and not warrant further consideration.

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4.15(1)(c) the suitability of the site for the development

The proposed development is considered to be suitable to the site. The amended proposal is considered to complement the prevailing streetscape character and is consistent with the objectives of the B4 Mixed Use zone.

The proposed development is considered to provide a suitable transition in height, bulk and scale to adjoining land zoned R2 Low Density Residential.

4.15(1)(d) any submissions made in accordance with this Act or the regulations

The application was notified in accordance with Part L of the SCDCP 2005 from 24/04/2018 to 16/05/2018, with 21 submissions received including a petition of 27 signatories.

A summary of the issues raised is included below:

Issues	Comments
Traffic congestion	<ul style="list-style-type: none"> • Vehicular egress onto Homebush Road is already difficult during peak periods. • The proposed building of sixty-nine (69) units will further compound existing traffic problems. • Safety impacts to children of nearby Strathfield South Primary School due to increased traffic during peak hours. • Traffic congestion impacts during the construction period and during peak hour periods.
Car parking	<ul style="list-style-type: none"> • Existing area already has a customer parking shortfall for existing businesses. • Sharing secure residential parking with commercial parking severely limits the parking spaces available. Any retail, restaurants or professional rooms need customer parking. • Existing parking is already very restricted. • The proposed car parking would be non-compliant with the proposed amendments to the affordable housing SEPP. • The basement plan utilises stacked parking for the commercial spaces and provides little room to manoeuvre to enable egress from the site in a forward direction.
Stormwater/siltation run-off and flooding	<ul style="list-style-type: none"> • Potential runoff and flooding to No. 12 Barton Street from the proposed development, due to the subject land being higher. It is recommended that a flood assessment study and improved stormwater drainage design including structural retaining wall to the rear boundary for containment of runoff within the development site, is provided.
Privacy	<ul style="list-style-type: none"> • Loss of privacy to downstairs family/TV/dining room and rear garden at No. 14 Barton Street, Strathfield South from the western end of the communal open space on Level 3. Request a 1.8m high privacy screen is installed. • Screen planting is recommended prior to any occupation of the premises. • Overlooking from the kitchen window of Unit 35 at second floor level and the north facing balcony on Level 3, to No. 14 Barton Street. • Loss of privacy to No. 16 Barton Street. • Overlooking from the western end of the communal open space area on Level 2 • Loss of privacy to the kitchen/dining and living area of No. 12 Barton Street
Building height	<ul style="list-style-type: none"> • The height of the building exceeds the 11m height allowed under SLEP 2012 by 2.3m. • Concerns regarding four (4) storey presentation. • Removal of level 3 would ensure compliance with SLEP, reduce the bulk and scale of the building and overshadowing impacts, as well as ensure a more gradual transition to the adjoining two (2) storey dwelling.
Inconsistency with the character of the local area	<ul style="list-style-type: none"> • The proposed four (4) storey development will destroy the character of the neighbourhood and overwhelm the surrounding area. • The existing streetscape if characterised by single and two (2) storey dwelling houses.

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	<ul style="list-style-type: none"> The bulk and scale of the proposed four (4) storey building is out of character with the existing surrounding streetscape. The area was not intended for high density. The proposed development is out of step with community expectations and the character of the area.
Overshadowing	<ul style="list-style-type: none"> Overshadowing to No. 255 Homebush Road. Overshadowing to No. 16 Barton Street, Strathfield South. Overshadowing to No. 12 Barton Street. Homebush Road from 247 onwards slopes down to the south. A proposed 5 storey building on 251-253 Homebush Road will mean that residences further south from 255 will not receive any sunlight in the winter months.
Strain on local infrastructure	<ul style="list-style-type: none"> Cumulative impact of residential flat buildings in the vicinity will increase the strain on local infrastructure. This includes skewing of the residence to green space ratio. The existing area is inadequately serviced by local and community infrastructure. Increased overcrowding of existing bus services, particularly services to Strathfield Station.
Noise	<ul style="list-style-type: none"> Noise impacts from air conditioning units to adjoining properties. Noise impacts from over 69 occupants.
Waste Management	<ul style="list-style-type: none"> The proposed 12 bins for kerbside collection will cause significant traffic delays on Homebush Road and amenity issues. The basement should be capable of accommodating a garbage truck for collection.
Setbacks	<ul style="list-style-type: none"> The adjoining block at 247-249 Homebush Road to the north has a 3.5m setback and No. 255 Homebush Road to the south has a 5.2m front setback. The predominant front setback along Homebush Road is 5.2m. The proposed 3.5m front setback is non-compliant with the DCP front setback requirement and in conflict with the existing streetscape. The proposed development does not meet the setback requirements under the ADG.
Adaptable Housing	<ul style="list-style-type: none"> A minimum of 15% of the total number of dwellings must be adaptable in accordance with AS4299 – Adaptable Housing. Only one (1) adaptable unit is proposed. Six (6) units should be provided.
Landscaping	<ul style="list-style-type: none"> The DCP requires 50% (451m²) of the site to consist of landscaped area. Further, 35% of the site area is required to consist of deep soil zones. Only 117m² landscaped area is proposed at the rear, which is non-compliant. The proposal is inconsistent with Division 3 of the affordable housing SEPP, which requires the landscape treatment of the front setback to be compatible with the streetscape. The application notes trees and shrubs along the southern common boundary and the west elevation will be provided to improve privacy and separation however this does not appear to be true along our boundary and west elevation.
Affordable Housing SEPP	<ul style="list-style-type: none"> The proposed development does not meet the standard criteria to be assessed under Clause 29 of SEPP (ARH) 2009.
Reduced Property and Land Values	<ul style="list-style-type: none"> The close proximity of this development will have a detrimental effect on the values of my property and adjacent properties along Barton Street and Homebush Road.

Assessing officer’s comments:

The submissions relate to the original scheme. The amended plans were not required to be re-notified as the modifications will have no greater cumulative environmental impact or amenity impact.

Responses to each of the matters raised above are provided below:

Issues	Comments
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Traffic congestion	<ul style="list-style-type: none"> A Traffic and Parking assessment report has been submitted with the DA. Notwithstanding the findings are based on the original proposal, the findings are still relevant. The report concludes that the projected change in the traffic generation potential of the site as a consequence of the development proposal is minimal, and will not give rise to any unacceptable traffic implications in terms of road network capacity. The proposal including the submitted Traffic and Parking assessment report was referred to Council's Traffic Engineer who has raised no objection to the proposal subject to standard conditions.
Car parking	<ul style="list-style-type: none"> The commercial tenancy has been deleted under the amended scheme, thereby precluding the need for separate commercial parking. The proposed on-site parking car parking provision is consistent with the parking requirements under the SEPP (ARH) 2009 (as amended).
Stormwater/siltation run-off and flooding	<ul style="list-style-type: none"> The proposed concept stormwater plans are supported by Council's Engineer, subject to recommended conditions of consent to mitigate any adverse impact to adjoining properties.
Privacy	<ul style="list-style-type: none"> The rear building alignment is generally setback 6m from the rear boundary. This is consistent with the building separation guidelines under the <i>Apartment Design Guide</i> (ADG). Existing mature trees along the rear boundary are conditioned for retention and protection.
Building height	<ul style="list-style-type: none"> The proposed development has been amended to comply with the 11m height standard under SLEP 2012.
Inconsistency with the character of the local area	<ul style="list-style-type: none"> The proposal has been amended to comply with the 11m height control, thereby providing an appropriate transition in height, bulk, and scale to adjoining low density residential development.
Overshadowing	<ul style="list-style-type: none"> Overshadowing impacts to adjoining development is considered acceptable. The proposed development comprises a compliant scheme with appropriate setbacks incorporated to minimise overshadowing impacts to the adjoining properties.
Strain on local infrastructure	<ul style="list-style-type: none"> The proposed development is consistent with development envisaged for the precinct under Council's LEP, DCP, and State-led strategic planning documents.
Noise	<ul style="list-style-type: none"> Noise impacts from the proposed development are acceptable. This is verified in the submitted Noise Assessment Report. The submitted Plan of Management indicates that the use of outdoor recreation areas will be prohibited between 10pm and 7am.
Waste Management	<ul style="list-style-type: none"> A condition is recommended prohibiting the presentation of bins kerbside for collection.
Setbacks	<ul style="list-style-type: none"> The stepped front setback is considered to be consistent with the prevailing streetscape character and is acceptable on merit.
Adaptable Housing	<ul style="list-style-type: none"> Six (6) accessible rooms are provided, which exceeds the 15% minimum under AS4299 – Adaptable Housing.
Landscaping	<ul style="list-style-type: none"> The DCP controls relating to Multi-Unit housing under Part C do not apply to boarding houses. Accordingly, there is no numeric landscaping requirement beyond the Urban Design Controls in Part Q and requirements pertaining to the front setback area under the SEPP (ARH) 2009. The proposed deep soil areas and plantings to the front and side setback areas have been increased to a satisfactory level under the amended scheme. This includes provision of canopy trees.
Affordable Housing SEPP	<ul style="list-style-type: none"> The proposed development is consistent with the standard criteria under Clause 29 of SEPP (ARH) 2009, as evidenced in this assessment report.
Reduced Property and Land Values	<ul style="list-style-type: none"> Reduced property and land values is not a matter for consideration under the Section 4.16 Matters for Consideration of the EP&A Act 1979.

4.15(1)(e) the public interest

The public interest is served through the detailed assessment of this development application under the relevant local planning controls and legislation and consideration of any submissions

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Lot 38 & 39 DP 2043 (Cont'd)

received relating to it by Council. The proposed development is not considered to be contrary to the public interest.

SECTION 7.11 CONTRIBUTIONS

Section 7.11 of the EP&A Act 1979 relates to the collection of monetary contributions from applicants for use in developing key local infrastructure. The Act reads as follows:

- “(1) If a consent authority is satisfied that development for which development consent is sought will or is likely to require the provision of or increase the demand for public amenities and public services within the area, the consent authority may grant the development consent subject to a condition requiring:*
- (a) the dedication of land free of cost, or*
 - (b) the payment of a monetary contribution, or both.*
- (2) A condition referred to in subsection (1) may be imposed only to require a reasonable dedication or contribution for the provision, extension or augmentation of the public amenities and public services concerned.”*

STRATHFIELD DIRECT SECTION 7.11 CONTRIBUTIONS PLAN

Section 7.11 Contributions are applicable to the proposed development in accordance with the Strathfield Direct Development Contributions Plan 2010-2030 as follows:

Provision of Community Facilities	\$47,558.18
Provision of Major Open Space	\$216,375.47
Provision of Local Open Space	\$47,615.13
Provision Roads and traffic Management	\$11,002.47
Administration	\$4,100.82
TOTAL	\$326,652.07

CONCLUSION

The application has been assessed having regard to Section 4.15 of the Environmental Planning and Assessment Act 1979, State Environmental Planning Policy (Affordable Rental Housing) 2009, State Environmental Planning Policy 55 (Remediation of Land), State Environmental Planning Policy (BASIX) 2004, State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017, Strathfield Local Environmental Plan 2012, and Strathfield Development Control Plan 2005 and is considered to be satisfactory for approval, subject to the recommended conditions of consent.

RECOMMENDATION

That Development Application No. 46/2018 for the demolition of existing structures and construction of a three (3) storey boarding house consisting of thirty-six (36) rooms including one (1) Manager's room set above one basement car parking level containing seventeen (17) car parking spaces, landscaping and associated site works at 251-253 Homebush Road, Strathfield South be **APPROVED**, subject to the following conditions:

SPECIAL CONDITIONS (SC)

1. BIN STORE (SC)

The proposed bin store shall be amended to provide 16 x 240L general waste bins and 6 x 240L recycling bins. Plans are to be suitably amended to demonstrate compliance with this requirement, prior to the issue of the Construction Certificate.

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(Reason: To ensure compliance with Part H of SCDC 2005.)

2. WASTE COLLECTION (SC)

Bins are prohibited from being presented kerbside for collection. Bins are to be collected directly from the bin store from within the premises. Evidence of a written agreement between the applicant and the private waste collector is to be provided to Council, prior to the issue of any Construction Certificate.

Evidence that all relevant standards are satisfied with regards to waste collection in terms of internal height clearance of the basement, turning circles, manoeuvrability and ability for the waste collection vehicle to exit the site in a forward direction are to be provided to Council and the Principal Certifying Authority, prior to the issue of any Construction Certificate.

(Reason: To minimise traffic impacts on the surrounding road network and to maintain streetscape amenity.)

3. EXTERNAL MATERIALS (SC)

The proposed FB2 Face Brick – 'Miro' is to be replaced with FB1 Face Brick: 'Gamet'. Plans are to be amended accordingly, prior to the issue of the Construction Certificate.

(Reason: To achieve greater consistency with adjoining residential development.)

4. BOARDING HOUSE – APPROVED USE (SC)

This approval is granted for the use of the land for the purposes of a Boarding House comprising a maximum of thirty-five boarding rooms (plus one Manager's Room). This includes 28 x double rooms (maximum 2 persons per room) and 7 single rooms (maximum 1 person per room). No more than 63 lodgers are to be present on the site at any one time.

All lodgers shall be required to reside on the premises for a minimum period of three (3) months in accordance with the definition of a Boarding House under the Strathfield Local Environmental Plan 2013 and SEPP (ARH) 2009.

The approved boarding house must operate within the definition of 'boarding house' under SLEP 2012 and SEPP (ARH) 2009. The approved boarding house cannot be strata or community title subdivided.

(Reason: To clarify the terms of the approval and to control the maximum number of lodgers living in the premises at any one time.)

5. BOARDING HOUSE – CONSTRUCTION OF (SC)

The construction of the boarding house shall comply with all applicable legislation/regulation and standards, including:

- i) Boarding Houses Act 2012;
- ii) Boarding Houses Regulation 2013;
- iii) Local Government Act 1993; and
- iv) Local Government Regulation 2005.

Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior to occupation of the premises.

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(Reason: To ensure compliance with relevant legislation.)

6. RESTRICTION AS TO USER OF THE BUILDING (SC)

Prior to the issue of any Occupation Certificate, a restriction as to user is to be registered against the title of the property on which the development is to be carried out, in accordance with section 88E of the Conveyancing Act 1919, limiting the use of the subject property for the purposes of a boarding house, as defined under State Environmental Planning Policy (Affordable Rental Housing) 2009.

7. CONSTRUCTION NOISE MANAGEMENT PLAN (SC)

A Construction Noise Management Plan (CNMP) is to be prepared prior to any works commencing (including demolition) to limit the impact on nearby residences.

The CNMP is to be prepared in accordance with the guidance provided in AS2436-2010 and the Interim Construction Noise Guideline, and submitted to the Principal Certifying Authority, prior to the commencement of any works.

(Reason: In accordance with the recommendation of the Noise Assessment Report, prepared by DK Acoustics Pty Ltd, dated 20 March 2018.)

8. NOISE FROM PLANT EQUIPMENT (SC)

Any plant equipment including any air conditioning plant or car park ventilation plant shall be suitably enclosed in a sound proof enclosure.

Once the carpark ventilation plant and air conditioning plant are selected, the submitted Noise Assessment Report is to be updated to include a noise assessment of the selected plant, demonstrating compliance with any applicable guidelines including those of the Environmental Protection Authority (EPA). The drawings accompanying the Construction Certificate application shall demonstrably comply with any recommendations of the amended Noise Assessment Report.

(Reason: To mitigate any adverse acoustic impacts to the adjoining properties).

9. LOCATION OF PLANT EQUIPMENT (SC)

No roof plant is to be installed, without the prior approval of Council. Any plant equipment including any air conditioning plant or car park ventilation plant shall be suitably enclosed and integrated with the design of the development, concealed from adjoining properties and the public domain and designed in accordance with any relevant standards and the Building Code of Australia.

(Reason: To ensure the proposed development does not contravene the 11m height control under SLEP 2012, and to mitigate any adverse visual impact on the surrounding area.)

10. CLOTHES DRYING FACILITIES (SC)

Clothes drying facilities are to be provided at the rear of the property and integrated into the design of the development.

(Reason: To mitigate any adverse visual impact on the streetscape).

11. TREE PROTECTION MEASURES (SC)

Trees 1, 4, 5, 12 and 13 in the submitted Arboricultural Impact Assessment Report prepared by Advanced Treescape Consulting dated 09/03/2018 shall be retained and protected in accordance

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with the tree protection recommendations contained within the Report.

Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior to the issue of the Construction Certificate.

(Reason: To ensure the protection of trees to be retained).

12. CANOPY TREES IN THE FRONT SETBACK (SC)

At least two (2) canopy trees are to be provided within the front setback adjacent the front boundary. New canopy trees shall be at least three (3) metres high at the time of planting and capable of reaching a mature height of 6m.

Species shall be selected from Council's Recommended Tree List (Available from Council's website or from the Customer Service Centre). Compliance with this condition is to be demonstrated to the Principal Certifying Authority, prior to the issue of any Occupation Certificate.

(Reason: To ensure the landscape character of the locality is maintained.)

13. GEOTECHNICAL REPORT RECOMMENDATIONS (SC)

The recommendations contained within the Geotechnical Investigation Report prepared by Geo Environmental Engineering, dated 08/03/2018, are to be fully complied with.

Details demonstrating compliance shall be submitted to the Principal Certifying Authority.

(Reason: Site stability).

14. ROOF DRAINAGE PLAN (SC)

The Stormwater drainage concept plan does not reflect roof runoff drainage to the OSD tank. An amended concept plan is required to address this inconsistency, prior to the issue of the Construction Certificate.

(Reason: Recommendation by Council's Engineer).

15. BASEMENT PUMP WELL (SC)

The contributing catchment to the pump well system is to be limited to the basement access ramps and subsoil drainage only. The stormwater concept plan is to be amended accordingly, prior to the issue of the Construction Certificate.

(Reason: Recommendation by Council's Engineer to ensure compliance with Council's requirements).

16. COUNCILS DRAINAGE INFRASTRUCTURE (SC)

Driveway crossings, including the apron and layback are to be located a minimum of 1m clear of any stormwater pits, lintels or poles and 2m clear of the trunk of any trees within the road reserve. The concept stormwater plans shall be amended to demonstrate compliance, prior to the issue of the Construction Certificate.

(Reason: Recommendation by Council's Engineer to ensure compliance with Council's requirements).

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17. SYDNEY WATER APPROVAL - PRESSURE TUNNEL AND SHAFTS (SC)

Prior to any works commencing on the site (including demolition), written confirmation is to be obtained from Sydney Water confirming that the proposed development will not have any adverse impact on the pressure tunnel and shafts, which traverse the site in an east/west direction.

(Reason: To ensure that the proposed development will not have any adverse impact on the Pressure tunnel and shafts).

18. HERITAGE COUNCIL OF NSW APPROVAL - PRESSURE TUNNEL AND SHAFTS (SC)

Written approval is to be obtained from the Heritage Council of NSW for the proposed development.

(Reason: To ensure that the proposed excavation will not disturb an archaeological relic, or have any adverse impact on the significance of the State heritage listed pressure tunnel and shafts).

GENERAL CONDITIONS (GC)

19. APPROVED PLANS AND REFERENCE DOCUMENTATION (GC)

The development, except where modified by a condition(s) of consent, is to be carried out in accordance with the following plans and reference documentation:

Plans affixed with Council's 'Development Consent' stamp relating to Development Consent No. DA2018/046:

Drawing No.	Title/Description	Prepared by	Issue / Revision & Date	Date received by Council
DA01-A	Concept Data	ADG Architects	Issue 2 / 20/07/2018	20/07/2018
DA02	Site Plan			
DA04	Basement 1 Floor Plan			
DA05	Ground Floor Plan			
DA06	Level 1 Floor Plan			
DA07	Level 2 Floor Plan			
DA08	Roof Plan			
DA10	North and South Elevations			
DA11	East and West Elevations			
DA12	Sections			
L/01 'D'	Proposed Landscape Plan – Ground Floor			

Reference Documentation affixed with Council's 'Development Consent' stamp relating to Development Consent No. 2017/138:

Title / Description	Prepared by	Issue/Revision & Date	Date received by Council
Statement of Environmental Effects (Amended)	Shanahan Planning	July 2018	20/07/2018

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Traffic and Parking Assessment Report (Ref 18001)	Varga Traffic Planning Pty Ltd	03/04/2018	10/04/2018
Noise Assessment Report (Ref 180226)	DK Acoustics Pty Ltd	20/03/2018	10/04/2018
Arboricultural Impact Assessment	Advanced Treescape Consulting	09/03/2018	10/04/2018
Waste Management Plan	ADG Architects	Undated	10/04/2018
Geotechnical Investigation Report	Geo-Environmental Engineering	08/03/2018 / Rev 01	10/04/2018
Stage 1 Preliminary Site Investigation	Geo-Environmental Engineering	07/03/2018	10/04/2018
BASIX Cert No.916883M_02		dated	20 September 2018

In the event of any inconsistency, the conditions of this consent shall prevail.

(Reason: To ensure that the form of the development undertaken is in accordance with the determination of Council.)

20. CONSTRUCTION HOURS (GC)

No construction or any other work related activities shall be carried out on the site outside the hours of 7.00 am to 5.00 pm Mondays to Fridays and 8am to 1pm Saturdays.

No building activities are to be carried out at any time on a Sunday or public holiday.

Where the development involves the use of jackhammers/ rock breakers and the like or other heavy machinery, such equipment may only be used between the hours of 7.00 am - 5.00 pm Monday to Friday only.

(Reason: To maintain amenity to adjoining land owners.)

21. CONSTRUCTION WITHIN BOUNDARIES (GC)

The development including but not limited to footings, walls, roof barges and guttering must be constructed wholly within the boundary of the premises. No portion of the proposed structure shall encroach onto the adjoining properties. Gates must be installed so they do not open onto any footpath or adjoining land.

(Reason: Approved works are to be contained wholly within the subject site.)

22. DEMOLITION – GENERALLY (GC)

Alteration and demolition of the existing building is limited to that documented on the approved plans (by way of notation). No approval is given or implied for removal and/or rebuilding of any portion of the existing building which is not shown to be altered or demolished.

(Reason: To ensure compliance with the approved development.)

23. DEMOLITION - SITE SAFETY FENCING (GC)

Site fencing is to be erected to a minimum height of 1.8m complying with WorkCover Guidelines, to exclude public access to the site, prior to any demolition works occurring and maintained for the duration of the demolition works.

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If applicable, a separate Hoarding Application for the erection of an A class (fence type) or B class (overhead type) hoarding along the street frontage(s) complying with WorkCover requirements must be obtained including:

- payment to Council of a footpath occupancy fee based on the area of footpath to be occupied and Council's Schedule of Fees and Charges before the commencement of work; and
- provision of a Public Risk Insurance Policy with a minimum cover of \$20 million in relation to the occupation of and works within Council's road reserve, for the full duration of the proposed works, must be obtained with a copy provided to Council (Note: The policy is to note Council as an interested party).

(Reasons: Statutory requirement and health and safety.)

24. LANDSCAPING - IRRIGATION OF COMMON AND PRIVATE LANDSCAPE AREAS (GC)

All common and private landscape areas including all planters are to have full coverage by a fully automatic irrigation system. The design, materials and installation are to be in accordance with Sydney Water Codes and satisfy all relevant Australian Standards. Details demonstrating compliance shall be submitted to the Principal Certifying Authority, prior occupation of the premises.

(Reason: To ensure appropriate landscape maintenance.)

25. LANDSCAPING (GC)

All landscape works shall be undertaken in accordance with the approved landscape plan, arborist report, and tree management plan as applicable.

- a) The following trees shall be retained

Trees reference in Arborist Report	Species	Location
Tree 1	<i>Tristaniopsis laurina</i> (Water Gum)	Road reserve
Tree 4	<i>X Cupressocyparis leylandii</i> (Leyland Cypress)	Rear boundary of No. 251 Homebush Road
Tree 5	<i>Syzygium australe</i> (Brush Cherry)	Rear boundary of No. 251 Homebush Road
Tree 12	<i>Callistemon viminalis</i> (Weeping Bottlebrush)	Rear boundary of No. 253 Homebush Road
Tree 13	<i>Jacaranda mimosifolia</i> (Jacaranda)	Rear boundary of No. 253 Homebush Road

- b) The following trees on the subject site may be removed:

Tree reference in Arborist Report	Tree	Location
Tree 2	<i>Cupressus macrocarpa</i> (Monterey Cypress)	Front setback
Tree 3	<i>Juniperus spp.</i> (Juniper)	Adjacent southern side boundary of No. 251 Homebush Road at the rear.
Tree 6	<i>Cotoneaster glaucophyllus</i> (Grey-leaved Cotoneaster)	Rear boundary of No. 251 Homebush Road
Tree 7	<i>Waterhousea floribunda</i> (Weeping Lilly Pilly)	Rear boundary of No. 251 Homebush Road

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Tree 8	<i>Ligustrum lucidum</i> (Glossy Privet)	Adjacent northern side boundary of No. 251 Homebush Road at the rear.
Tree 9	<i>Leptospermum spp.</i> (Fine Leaf Tea Tree)	As above.
Tree 11	<i>Murraya paniculata</i> (Orange Jessamine)	Centre of site
Tree 14	<i>Casuarina glauca</i> (Swamp She-oak)	Rear boundary of No. 253 Homebush Road

(Reason: To ensure landscaping is in accordance with the approved Landscape Plan.)

26. LANDSCAPING - TREE PRESERVATION (GC)

All street trees and trees on private property that are protected under Council's controls, shall be retained except where Council's prior written consent has been obtained. The felling, lopping, topping, ringbarking, wilful destruction or removal of any tree/s unless in conformity with this approval or subsequent approval is prohibited.

All healthy trees and shrubs identified for retention on the approved drawings are to be suitably protected in accordance with Australian Standard AS4970 – Protection of Trees on Development, prior to the commencement of any works (including any demolition and excavation) and maintained for the duration of the works.

(Reason: To ensure the protection of trees to be retained on the site.)

27. LANDSCAPING (GC)

The principal contractor or owner must provide to PCA a works-as-executed landscape plan and certification from a qualified landscape architect/designer, horticulturist and/or arborist as applicable to the effect that the works comply with this consent.

(Reason: To ensure that all landscaping work is completed prior to issue of the Final Occupation Certificate.)

28. LIGHTING (GC)

Any lighting of the premises shall be installed and maintained in accordance with Australian Standard AS 4282-1997: Control of the Obtrusive Effects of Outdoor Lighting so as to avoid annoyance to the occupants of adjoining premises or glare to motorists on nearby roads.

No flashing, moving or intermittent lighting, visible from any public place may be installed on the premises or external signage associated with the development, without the prior approval of Council.

(Reason: To protect the amenity of surrounding development and protect public safety.)

29. PRINCIPAL CERTIFYING AUTHORITY (PCA) IDENTIFICATION SIGN (GC)

Prior to commencement of any work, signage must be erected in a prominent position on the work site identifying:

- i) the Principal Certifying Authority (PCA) by showing the name, address and telephone number of the PCA;
- ii) the Principal Contractor by showing the Principal Contractor's name, address and telephone number (outside of work hours) for that person; and

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- iii) the sign must state that unauthorised entry to the work site is prohibited.

Any such sign is to be maintained while the work is being carried out, but must be removed when the work has been completed.

This clause does not apply to building work, subdivision work or demolition work that is carried out inside an existing building that does not affect the external walls of the building.

(Reason: Statutory requirement.)

30. SITE MANAGEMENT - DURING DEMOLITION AND CONSTRUCTION WORKS (GC)

All of the following are to be satisfied/complied with during demolition, construction and any other site works:

- i) All demolition is to be carried out in accordance with Australian Standard AS 2601-2001.
- ii) Demolition must be carried out by a registered demolition contractor.
- iii) A single entrance is permitted to service the site for demolition and construction. The footway and nature strip at the service entrance must be planked out.
- iv) No blasting is to be carried out at any time during construction of the building.
- v) Care must be taken during demolition/ excavation/ building/ construction to prevent any damage to adjoining buildings.
- vi) Adjoining owner property rights and the need for owner's permission must be observed at all times, including the entering onto land for the purpose of undertaking works.
- vii) Any demolition and excess construction materials are to be recycled wherever practicable.
- viii) The disposal of construction and demolition waste must be in accordance with the requirements of the Protection of the Environment Operations Act 1997.
- ix) All waste on the site is to be stored, handled and disposed of in such a manner as to not create air pollution (including odour), offensive noise or pollution of land and/or water as defined by the Protection of the Environment Operations Act 1997. All excavated material should be removed from the site in an approved manner and be disposed of lawfully to a tip or other authorised disposal area.
- x) All waste must be contained entirely within the site.
- xi) Section 143 of the Protection of the Environment Operations Act 1997 requires waste to be transported to a place which can lawfully accept it. All non-recyclable demolition materials are to be disposed of at an approved waste disposal depot in accordance with legislation.
- xii) All materials on site or being delivered to the site are to generally be contained within the site. The requirements of the Protection of the Environment Operations Act 1997 must be complied with when placing/stockpiling loose material, disposing of concrete waste, or other activities likely to pollute drains or water courses.
- xiii) Details as to the method and location of disposal of demolition materials (weight dockets, receipts, etc.) should be kept on site as evidence of approved methods of disposal or recycling.
- xiv) Any materials stored on site must be stored out of view or in such a manner so as not to cause unsightliness when viewed from nearby lands or roadways.
- xv) Public footways, include nature strips and roadways adjacent to the site must be maintained and cleared of obstructions during construction. No building materials, waste containers or skips may be stored on the road reserve or footpath without prior separate approval from Council, including payment of relevant fees.
- xvi) Building operations such as brick-cutting, washing tools or paint brushes, and mixing mortar not be performed on the roadway or public footway or any other locations which could lead to the discharge of materials into the stormwater drainage system.
- xvii) All site waters during excavation and construction must be contained on site in an approved manner to avoid pollutants entering into waterways or Council's stormwater drainage system.
- xviii) Any work must not prohibit or divert any natural overland flow of water.

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- xix) Toilet facilities for employees must be provided in accordance with WorkCover NSW.
- xx) Protection pads are to be installed to the kerb and gutter where trucks and vehicles enter the site.

(Reason: To ensure that demolition, building and any other site works are undertaken in accordance with relevant legislation and policy and in a manner which will be non-disruptive to the local area.)

31. STORMWATER MANAGEMENT PLAN - CERTIFICATION REQUIREMENT (GC)

A detailed Stormwater Management Plan (SWMP) in line with Council's Stormwater Management Code is to be prepared and certified by a practicing Chartered Professional Engineer on the National Professional Engineer's Register (NPER) at Engineers Australia and submitted to the Principal Certifying Authority and Council, prior to the issue of a Construction Certificate. The SWMP is to be based on the approved development as modified by any conditions of consent.

(Reason: To ensure appropriate provision is made for the disposal and management of stormwater generated by the development, and to ensure public infrastructure in Council's care and control is not overloaded.)

32. SYDNEY WATER - STAMPED PLANS PRIOR TO COMMENCEMENT (GC)

The approved plans must be submitted to a Sydney Water Quick Check agent or Customer Centre to determine whether the development will affect Sydney Water's sewer and water mains, stormwater drains and/or easements, and if further requirements need to be met. Plans will be appropriately stamped.

For Quick Check agent details please refer to the web site www.sydneywater.com.au (see Building Developing and Plumbing then Quick Check) or telephone 13 20 92. The consent authority or a Certifying Authority must ensure that a Quick Check agent/Sydney Water has stamped the plans before the commencement of work.

(Reason: Compliance with Sydney Water requirements.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF A CONSTRUCTION CERTIFICATE (CC)

33. ACCESS - ACCESS FOR PEOPLE WITH DISABILITIES (CC)

Access for people with disabilities must be provided in accordance with the requirements of the Building Code of Australia, relevant Australian Standards and with regard to the Disability Discrimination Act 1992. Plans shall be notated demonstrating compliance and approved by the Principal Certifying Authority, prior to the issue of a Construction Certificate.

Note: Disability (Access to Premises - Buildings) Standards 2010 - As of 1 May 2011, if access is provided to the extent covered by this Standard, then such access cannot be viewed as unlawful under the Disability Discrimination Act 1992.

(Reason: To provide equitable access for people(s) with disabilities in accordance with the relevant legislation and Australian Standards.)

34. BASIX COMMITMENTS (CC)

The approved BASIX Certificate shall be submitted to the Principal Certifying Authority with the application for a Construction Certificate.

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Where a change or changes are proposed in the BASIX commitments, the applicant must submit a new BASIX Certificate to the Principal Certifying Authority and Council. If any proposed change in the BASIX commitments is inconsistent with the development consent the applicant will be required to submit a modification to the development consent to Council under Section 96 of the Environmental Planning and Assessment Act 1979.

All commitments in the BASIX Certificate must be shown on the plans accompanying the Construction Certificate.

(Reason: Statutory compliance.)

35. BUILDING CODE OF AUSTRALIA - COMPLIANCE WITH (CC)

All architectural drawings, specifications and related documentation shall comply with the Building Code of Australia (BCA). All work must be carried out in accordance with the requirements of the Building Code of Australia (BCA).

In the case of residential building work for which the Home Building Act 1989 requires there to be a contract of insurance in force in accordance with Part 6 of that Act, such a contract of insurance is to be in force before any building work authorised to be carried out by the consent commences.

Details demonstrating compliance with this condition are to be submitted to the Principle Certifying Authority, prior to issue of the Construction Certificate.

(Reason: This is a 'prescribed' condition under clause 98(1) of the Environmental Planning and Assessment Regulation 2000.)

36. COMMENCEMENT OF WORKS (NO WORKS UNTIL A CC IS OBTAINED)

Building work, demolition or excavation must not be carried out until a Construction Certificate has been issued by either Strathfield Council or a Principal Certifying Authority.

Demolition of any part of a building triggers 'commencement of erection of building' pursuant of section 81A(2) of the EP&A Act 1979. Accordingly, demolition works must not commence until a Construction Certificate has been issued, a Principal Certifying Authority has been appointed and a Notice of Commencement has been issued.

(Reason: To ensure compliance with statutory provisions.)

37. CONSTRUCTION AND ENVIRONMENTAL MANAGEMENT PLAN (CC)

The applicant must prepare and submit a Construction and Environmental Management Plan (CEMP) to the Principal Certifying Authority, including:

- i) Detailed information on any approvals required from other authorities prior to or during construction.
- ii) Traffic management, including details of:
 - ingress and egress of vehicles to the site;
 - management of loading and unloading of materials;
 - the location of heavy vehicle parking off-site; and
 - designated routes for vehicles to the site.
- iii) The proposed areas within the site to be used for a builder's site office and amenities, the storage of excavated material, construction materials and waste containers during the construction period.
- iv) Erosion and sediment control, detailing measures and procedures consistent with the requirements of Council's guidelines for managing stormwater, including:

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- the collection and treatment of stormwater and wastewater generated on site prior to discharge; and
- procedures to prevent run-off of solid material and waste from the site.
- v) Waste management, including:
 - details of the types and estimated volumes of waste materials that will be generated;
 - procedures for maximising reuse and recycling of construction materials; and
 - details of the off-site disposal or recycling facilities for construction waste.
- vi) Dust control, outlining measures to minimise the generation and off-site transmission of dust and fine particles, such as watering or damp cloth fences.
- vii) A soil and water management plan, which includes:
 - measures to minimise the area of soils exposed at any one time and conserve top soil;
 - identification and protection of proposed stockpile locations;
 - preservation of existing vegetation and revegetation;
 - measures to prevent soil, sand, sediments leaving the site in an uncontrolled manner;
 - measures to control surface water flows through the site in a manner that diverts clean run-off around disturbed areas, minimises slope gradient and flow distance within disturbed areas, ensures surface run-off occurs at non-erodible velocities, and ensures disturbed areas are promptly rehabilitated;
 - details of sediment and erosion control measures in place before work commences;
 - measures to ensure materials are not tracked onto the road by vehicles entering or leaving the site; and
 - details of drainage to protect and drain the site during works.
- viii) Asbestos management procedures:
 - Anyone who removes, repairs or disturbs bonded or a friable asbestos material must hold a current removal licence from Workcover NSW holding either a Friable (Class A) or a Non-Friable (Class B) Asbestos Removal License which ever applies and a current WorkCover Demolition License where works involve demolition. To find a licensed asbestos removalist please see www.workcover.nsw.gov.au
 - Removal of asbestos by a person who does not hold a Class A or Class B asbestos removal license is permitted if the asbestos being removed is 10m² or less of non-friable asbestos (approximately the size of a small bathroom). Friable asbestos materials must only be removed by a person who holds a current Class A asbestos license.
 - Before starting work, a work site-specific permit approving each asbestos project must be obtained from Workcover NSW. A permit will not be granted without a current Workcover licence. All removal, repair or disturbance of or to asbestos material must comply with the following:
 - The Work Health and Safety Act 2011;
 - The Work Health and Safety Regulation 2011;
 - How to Safety Remove Asbestos Code of Practice – WorkCover 2011; and
 - Safe Work Australia Code of Practice for the Management and Control of Asbestos in the Workplace.
 - Following completion of asbestos removal works undertaken by a licensed asbestos removalist re-occupation of a workplace must not occur until an independent and suitably licensed asbestos removalist undertakes a clearance inspection and issues a clearance certificate.
 - The developer or demolition contractor must notify adjoining residents at least two (2) working days (i.e. Monday to Friday exclusive of public holidays) prior to the commencement of asbestos removal works. Notification is to include, at a minimum:
 - the date and time when asbestos removal works will commence;
 - the name, address and business hours contact telephone number of the demolisher, contractor and/or developer;
 - the full name and license number of the asbestos removalist/s; and
 - the telephone number of WorkCover's Hotline 13 10 50
 - warning signs informing all people nearby that asbestos removal work is taking place in the area. Signs should be placed at all of the main entry points to the asbestos removal work area where asbestos is present. These signs should be weatherproof, constructed

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of light-weight material and adequately secured so they remain in prominent locations. The signs should be in accordance with AS 1319-1994 Safety signs for the occupational environment for size, illumination, location and maintenance; and

- appropriate barricades installed as appropriate to prevent public access and prevent the escape of asbestos fibres. Barricades must be installed prior to the commencement of asbestos removal works and remain in place until works are completed.

(Reason: Safety, amenity and protection of public infrastructure and the environment.)

38. COUNCIL PERMITS – FOR ALL ACTIVITIES ON COUNCIL LAND (CC)

Works Permit

(as per Section 68 of the Local Government Act 1993 and Section 138 and 139 of the Roads Act 1993)

A Works Permit is required for construction of a vehicular crossing (driveway), new stormwater down pipe connection to kerb and gutter, new footpath and/or stormwater connection. A Works Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Standing Plant Permit

This permit must be applied for where it is intended to park a concrete pump, crane or other plant on the roadway or footpath. A Standing Plant Permit Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Skip Bin Permit

This permit must be applied for if you intend to place a skip bin on the roadway or footpath. A Skip Bin Application Form is available from Council's Customer Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Temporary Full or Part Road Closure Permit

This permit must be applied for if you require a full or a part road closure to take place to assist in your construction works. Please use the Works Permit Application Form, which is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website. Please note a Road Closure Permit is not required for standing plant.

Hoarding Permit

This permit must be applied for if you intend to erect a Class A (fence type) or Class B (overhead type) hoarding along the street frontage(s). A Hoarding Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Work Zone Permit

This permit must be applied for if you require permanent parking along the kerbside at the front of the site during construction works. A Work Zone Permit Application Form is available from Council's Customer Service Centre or can be downloaded from Council's website. The applicable fees and charges are located on Council's website.

Ground Anchoring Permit

This permit must be applied for, for the installation of ground anchors under Council's footway/road reserve. It does not cover ground anchors under private properties. A separate approval is required to be obtained from Roads and Maritime Services (RMS) if it is proposed to install ground anchors under a State or Classified Regional Road (please refer to the end of this application form for more information).

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(Reason: Council requirement)

39. DILAPIDATION REPORT - PRE-COMMENCEMENT (CC)

Subject to access being granted, a pre-commencement Dilapidation Report is to be undertaken on all adjoining properties, which in the opinion of a suitably qualified engineer, could be potentially affected by the construction of the project. The Dilapidation Report shall be carried out prior to the issue of a Construction Certificate.

The Dilapidation Report is to be prepared by a suitably Qualified Engineer with current Corporate Membership with the Institution of Engineers, Australia or Geotechnical Practitioner. The report shall include a photographic survey of adjoining properties detailing the physical condition of those properties, both internally and externally, including walls, ceilings, roof, structural members and other such items.

If access for undertaking the dilapidation survey is denied by an adjoining owner, the applicant must demonstrate, in writing, to Council's satisfaction attempts have been made to obtain access and/or advise the affected property owner of the reason for the survey and these attempts have been unsuccessful. Written concurrence must be obtained from Council in such circumstances.

The Report shall cover structural and geotechnical factors likely to arise from the development. A copy of this Report shall be submitted to Council as a record. The person having the benefit of the development consent must, at their own cost, rectify any damage caused to other properties during the construction of the project.

(Reason: To ensure no damage to adjoining properties occurs.)

40. EROSION AND SEDIMENTATION CONTROL PLAN (CC)

An Erosion and Sediment Control Plan is to be prepared where construction or excavation activity requires the disturbance of the soil surface and existing vegetation. Details including drawings and specifications must provide adequate measures for erosion and sediment control to ensure:

- i) Compliance with the approved Soil and Water Management Plan.
- ii) Removal or disturbance of vegetation and top soil is confined to within 3m of the approved building area (no trees to be removed without approval).
- iii) All uncontaminated run-off is diverted around cleared or disturbed areas.
- iv) Silt fences or other devices are installed to prevent sediment and other debris escaping from the cleared or disturbed areas into drainage systems or waterways.
- v) All erosion and sediment controls are fully maintained for the duration of demolition/development works.
- vi) Controls are put into place to prevent tracking of sediment by vehicles onto adjoining roadways.
- vii) All disturbed areas are rendered erosion-resistant by turfing, mulching, paving or similar.
- viii) All water pumped or otherwise removed from excavations or basement areas is filtered to achieve suspended solids/non filterable residue levels complying with the Australian Water Quality guidelines for Fresh and Marine Waters.
- ix) Pumped or overland flows of water are discharged so as not to cause, permit or allow erosion before the commencement of work (and until issue of the occupation certificate).

Details of the proposed soil erosion and sedimentation controls are to be submitted to the Principal Certifying Authority with the Construction Certificate Application. Under no circumstances are any works to commence, prior to these details being approved by the Principal Certifying Authority and the controls being in place on the site.

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(Reason: Environmental protection.)

41. EXCAVATION - AFFECTING ADJOINING LAND (CC)

If an excavation associated with the approved development extends below the level of the base of the footings of a building on an adjoining allotment of land, the person having the benefit of the development consent must, at the person’s own expense, comply with the requirements of clause 98E of the Environmental Planning and Assessment Regulation 2000, including:

- i) protect and support the adjoining premises from possible damage from the excavation, and
- ii) where necessary, underpin the adjoining premises to prevent any such damage.

The condition referred to in subclause (i) does not apply if the person having the benefit of the development consent owns the adjoining land or the owner of the adjoining land has given consent in writing to that condition not applying. Details shall be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: Structural safety.)

42. FIRE SAFETY SCHEDULE (CC)

A Fire Safety Schedule specifying the fire safety measures which should be implemented in the building premises must be submitted with the Construction Certificate application, in accordance with Part 9 of Clause 168 of the Environmental Planning and Assessment Regulation 2000.

Note: A Construction Certificate cannot be issued until a Fire Safety Schedule is received.

(Reason: Compliance with the Environmental Planning and Assessment Act 1979.)

43. WATER SUSTAINABILITY - WATER SENSITIVE URBAN DESIGN (CC)

Details of the Water Sensitive Urban Design (WSUD) components (stormwater treatment measures) shall be submitted to and approved by the Principal Certifying Authority prior to the issue of a Construction Certificate. Details shall demonstrate compliance with the water conservation and stormwater quality targets set out under Sections 3.1 and 3.2 respectively under Part N of the SDCDP 2005, and be prepared by a suitably qualified professional engineer.

(Reason: To promote Water Sensitive Urban Design.)

44. Section 7.11 contribution payment (CC)

In accordance with the provisions of Section 7.11 of the Environmental Planning and Assessment Act 1979 and the Strathfield Direct Development Contributions Plan 2010-2030, a contribution in the form of cash, cheque or credit card (financial transaction fee applies) shall be paid to Council for the following purposes:

Provision of Community Facilities	\$47,558.18
Provision of Major Open Space	\$216,375.47
Provision of Local Open Space	\$47,615.13
Provision Roads and traffic Management	\$11,002.47
Administration	\$4,100.82
TOTAL	\$326,652.07

The total amount of the contribution is valid as at the date of determination and is subject to quarterly indexation. Contributions shall be indexed at the time of payment in accordance with clause 2.14 of the Strathfield Direct Development Contributions Plan 2010-2030.

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Contributions must be received by Council and submitted to the Accredited Certifier, prior to the issue of any Construction Certificate.

Please present a copy of this condition when paying the contribution at the Customer Service Centre so that it can be recalculated.

Note: A copy of Strathfield Council's Section 94 Direct Development Contributions Plan may be downloaded from Council's website.

(Reason: To enable the provision of public amenities and services required/anticipated as a consequence of increased demand resulting from the development.)

45. SECURITY PAYMENT - DAMAGE DEPOSIT FOR COUNCIL INFRASTRUCTURE (CC)

A security (damage deposit) calculated in accordance with Council's adopted Fees and Charges) shall be paid to Council, prior to the issue of a Construction Certificate.

The deposit is required as security against any damage to Council property during works on the site. The applicant must bear the cost of all restoration works to Council's property damaged during the course of this development. All building work must be carried out in accordance with the Building Code of Australia.

Payment may be accepted in the form of cash, bank guarantee, cheque or credit card (financial transactions fees apply). Note: Additional fees apply for the lodgement of a bank guarantee in lieu of cash bond applies in accordance with Council's adopted Fees and Charges.

Any costs associated with works necessary to be carried out to rectify any damages caused by the development, shall be deducted from the Damage Deposit.

Note: Should Council property adjoining the site be defective e.g. cracked footpath, broken kerb etc., this should be reported in writing, or by photographic record, submitted to Council at least seven (7) days prior to the commencement of any work on site. This documentation will be used to resolve any dispute over damage to infrastructure. It is in the applicant's interest for it to be as full and detailed as possible.

The damage deposit shall be refunded upon completion of all works upon receipt of a Final Occupation Certificate stage and inspection by Council.

(Reason: Protection of Council infrastructure.)

46. TRAFFIC - CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CC)

A Construction Traffic Management Plan (CTMP) is to be prepared by an appropriately qualified Traffic Management Consultant and submitted to and approved by Council's Engineering Section, prior to the commencement of any works including demolition.

The following matters should be addressed in the CTMP (where applicable):

- i) description of the demolition, excavation and construction works;
- ii) site plan/s showing the site, roads, footpaths, site access points and vehicular movements;
- iii) size, type and estimated number of vehicular movements (including removal of excavated materials, delivery of materials and concrete to the site);
- iv) proposed route(s) from the arterial (state) road network to the site and the proposed route from the site back to the arterial road network;
- v) impacts of the work and vehicular movements on the road network, traffic and pedestrians

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- and proposed methods to safely manage pedestrians and construction related vehicles in the frontage roadways;
- vi) any Traffic Control Plans (TCP's) proposed to regulate traffic and pedestrian movements for construction activities (such as concrete pours, crane installation/removal etc.);
 - vii) proposed hours of construction related activities and vehicular movements to and from the site;
 - viii) current/proposed approvals from other Agencies and Authorities (including Roads and Maritime Services, Police and State Transit Authority);
 - ix) any activities proposed to be located or impact upon Council's road, footways or any public place;
 - x) measures to maintain public safety and convenience;
 - xi) any proposed road and/or footpath closures;
 - xii) turning areas within the site for construction and spoil removal vehicles, allowing a forward egress for all construction vehicles on the site;
 - xiii) locations of work zones (where it is not possible for loading/unloading to occur on the site) in the frontage roadways accompanied by supporting documentation that such work zones have been approved by the Local Traffic Committee and Council;
 - xiv) location of any proposed crane and concrete pump and truck standing areas on and off the site (and relevant approvals from Council for plant on road);
 - xv) a dedicated unloading and loading point within the site for all construction vehicles, plant and deliveries;
 - xvi) material, plant and spoil bin storage areas within the site, where all materials are to be dropped off and collected;
 - xvii) on-site parking area for employees, tradespersons and construction vehicles as far as possible;
 - xviii) proposed areas within the site to be used for the storage of excavated material, construction materials and waste and recycling containers during the construction period; and
 - xix) how it is proposed to ensure that soil/excavated material is not transported onto surrounding footpaths and roadways.

(Reason: To mitigate traffic impacts on the surrounding area during the construction period.)

47. VENTILATION SYSTEMS (CC)

Any natural or mechanical ventilation systems shall be designed, constructed and installed in accordance with the following:

- i) The Building Code of Australia;
- ii) Protection of the Environment Operations Act 1997; and
- iii) Australian Standard AS1668-1991.

Details demonstrating compliance with the above are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: To ensure any proposed ventilation system complies with the relevant requirements/standards.)

48. CAR PARKING - VEHICULAR ACCESS RAMPS (CC)

Vehicular access ramps shall comply with the provisions of AS/NZS 2890.1:2004. Plans to be submitted shall contain the following details:

- i) Longitudinal section along the extreme wheel paths of each driveway/access ramp at a scale of 1:25 demonstrating compliance with the scraping provisions of AS/NZS 2890.1:2004. It shall include all levels and grades, including those levels stipulated as boundary levels, both existing and proposed. It shall extend from the centre-line of the

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- roadway through to the parking area.
- ii) Sections showing the clearance to the underside of any overhead structure (minimum headroom requirements 2200mm minimum for standard headroom clearance or 2400mm where disabled access provisions are to be provided) demonstrating compliance with the provisions of AS/NZS 2890.1:2004.
 - iii) Longitudinal section along the gutter line showing how it is intended to transition the vehicular crossing into the existing kerb and gutter. Boundary levels shall generally run parallel with the kerb levels.
 - iv) Location of verge trees, street furniture and service installations.
 - v) Superimposition of vehicle turning circles for access into parking spaces.
 - vi) Certification that the design complies with AS/NZS 2890.1:2004 by a Qualified Engineer.

The certification referred to in (vi) above shall be submitted to the Principal Certifying Authority prior to the issue of a Construction Certificate (for the design) and to the Principal Certifying Authority prior to the issue of any Occupation Certificate for the 'as-built works'.

(Reason: To ensure adequate vehicular access can be achieved.)

49. CAR PARKING - COMPLIANCE WITH AS/NZS 2890.1:2004 (CC)

Car parking dimensions must comply with the Australian/New Zealand Standard AS/NZS 2890.1:2004 - Parking facilities - Off-street car parking. Details demonstrating compliance are to be submitted to the Principal Certifying Authority, prior to the issue of a Construction Certificate.

(Reason: To ensure compliance with Australian Standards relating to parking of vehicles.)

50. CAR PARKING - VEHICULAR CIRCULATION, AISLE AND RAMP WIDTHS (CC)

The aisle widths, internal circulation, ramp widths and grades of the car park shall comply with the Roads and Traffic Authority Guidelines and AS/NZS 2890.1:2004 - Off-Street Car Parking Code.

Vehicular manoeuvring paths are to be prepared demonstrating that all vehicles can enter or depart the site in a forward direction without encroaching on required car parking spaces.

(Reason: To ensure compliance with Australian Standards relating to manoeuvring, access and parking of vehicles.)

51. WASTE - GARBAGE ROOMS OR GREASE ARRESTOR ROOMS (CC)

Garbage rooms or grease arrester rooms must be constructed of solid material: cement rendered and steel trowelled to a smooth even surface. The door to the garbage room is to be designed and constructed to ensure the room is vermin proof and can be opened from the inside at all times. The garbage room is to be ventilated to the external air by natural ventilation or an approved air handling exhaust system.

(Reason: To keep garbage rooms in a clean and sanitary condition to protect public health.)

52. WORKS WITHIN THE ROAD RESERVE (CC)

Detailed drawings including long and short sections and specifications of all works within existing roads (including but not limited to structures, road works, driveway crossings, footpaths and stormwater drainage) shall be submitted to and approved by Council under Section 138 of the Roads Act 1993 and all fees and charges paid, prior to the issue of a Construction Certificate.

Detailed drawings and specifications are to be prepared and certified by an appropriately qualified Civil Engineer.

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Upon completion of the works, the Applicant is to provide to Council with electronic copies of 'Work as Executed Plans'. The plans are to show relevant dimensions and finished levels and are to be certified by a surveyor.

The Applicant is to provide to Council, in an approved format, details of all public infrastructure created as part of the works.

All civil engineering works adjacent/near/outside 4-10 Albert Road are to be fully supervised by Council. A maintenance period of six (6) months shall apply to the work, after it has been completed and approved. In that period the Applicant shall be liable for any part of the work which fails to perform in the manner outlined in Council's specifications, or as would reasonably be expected under the design conditions.

(Reason: To ensure compliance of engineering works/Council assets are constructed to acceptable standards for engineering works.)

53. WORKS ZONE - APPROVAL BY COUNCIL'S TRAFFIC COMMITTEE (CC)

An application for a 'Works Zone' must be submitted to and approved by the Strathfield Council Traffic Committee prior to the commencement of any site work (including demolition).

The suitability of the proposed length and duration of the Works Zone is to be demonstrated in the application for the Works Zone. The application for the Works Zone must be submitted to Council at least six (6) weeks prior to the commencement of work on the site to allow for assessment and tabling of agenda for the Strathfield Council Traffic Committee.

The requirement for a Works Zone may be varied or waived only if it can be demonstrated in the Construction Traffic Management Plan (to the satisfaction of Council) that all construction related activities (including all loading and unloading operations) can and will be undertaken wholly within the site. The written approval of Council must be obtained to provide a Works Zone or to waive the requirement to provide a Works Zone prior to the commencement of any site work.

(Reason: Council requirement.)

CONDITIONS TO BE SATISFIED PRIOR TO THE COMMENCEMENT OF WORKS (CW)

54. APPOINTMENT OF A PRINCIPAL CERTIFYING AUTHORITY (PCA) (CW)

No work shall commence in connection with this Development Consent until:

- i) A construction certificate for the building work has been issued by the consent authority or a Principal Certifying Authority.
- ii) The person having the benefit of the development consent has appointed a principal certifying authority for the building work, and notified the principal certifying authority that the person will carry out the building work as an owner/builder, if that is the case.
- iii) The principal certifying authority has, no later than 2 days before the building work commences:
 - notified the Council of his or her appointment, and
 - notified the person having the benefit of the development consent of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- iv) The person having the benefit of the development consent, if not carrying out the work as an owner-builder, has:
 - appointed a principal contractor for the building work who must be the holder of a contractor licence if any residential building work is involved;
 - notified the principal certifying authority of such appointment; and

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- unless that person is the principal contractor, notified the principal contractor of any critical stage inspections and other inspections that are to be carried out in respect of the building work.
- v) The person having the person having the benefit of the development consent has given at least 2 days notice to the Council of the person's intention to commence the erection of the building.

Note: If the principal certifying authority is the Council, the nomination will be subject to the payment of a fee for the service to cover the cost of undertaking all necessary inspections and the issue of the appropriate certificates.

Under the Environment Planning and Assessment (Quality of Construction) Act, 2003, a sign must be erected in a prominent position on the work site showing the name, address and telephone number of the principal certifying authority; the name of the principal contractor (if any) for the building work and a telephone number at which that person may be contacted outside working hours. That sign must also state that unauthorised entry is prohibited. The sign must not be removed until all work has been completed.

(Reason: Statutory requirement.)

55. NOTICE OF COMMENCEMENT (CW)

No work shall commence until the following details are submitted to Council:

- i) a Notice of Commencement (form will be attached with issue of a Construction Certificate or available from our website) within two (2) days of the date on which it is proposed to commence works associated with the Development Consent;
- ii) details of the appointment of a Principal Certifying Authority (either Council or another Principal Certifying Authority); and
- iii) details of the name, address and licence details of the Builder.

(Reason: Statutory requirement.)

CONDITIONS TO BE SATISFIED DURING DEMOLITION AND BUILDING WORKS (DW)

56. CONTAMINATED LAND UNEXPECTED FINDS (DW)

In the instance works cause the generation of odours or uncovering of unexpected contaminants works are to immediately cease, Council is to be notified and a suitably qualified environmental scientist appointed to further assess the site.

The contaminated land situation is to be evaluated by the supervising environmental consultant and an appropriate response determined in consultation with the applicant, which is agreed to by Strathfield Council's Environmental Services Manager.

Note: Council may also request that a NSW EPA accredited site auditor is involved to assist with the assessment of the contaminated land situation and review any new contamination information. The applicant must also adhere to any additional conditions which may be imposed by the accredited site auditor.

(Reason: To ensure compliance with statutory requirements.)

57. FILL MATERIAL (DW)

The only waste derived material which may be received at the development site is:

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- i) Virgin excavated material (within the meaning of the Protection of the Environment Operations Act 1997), and
- ii) any other waste-derived material the subject of a resource recovery exemption under cl.51A of the Protection of the Environment Operations (Waste) Regulation 2005 that is permitted to be used as fill material.

Any (b)-type material received at the development site must be accompanied by documentation certifying by an appropriately qualified environmental consultant the materials compliance with the exemption conditions; and this documentation must be provided to the Principal Certifying Authority on request.

(Reason: To ensure imported fill is of an acceptable standard for environmental protection purposes.)

58. OBSTRUCTION OF PUBLIC WAY NOT PERMITTED DURING WORKS (DW)

The public way must not be obstructed by any materials, vehicles, refuse, skips or the like, under any circumstances, without the prior approval of Council.

(Reason: To maintain public access and safety.)

59. PUBLIC INFRASTRUCTURE AND SERVICES (DW)

The applicant must comply with the requirements (including financial costs) of any relevant utility provider (e.g. Energy Australia, Sydney Water, Telstra, RMS, Council etc.) in relation to any connections, works, repairs, relocation, replacements and/or adjustments to public infrastructure or services affected by the development.

(Reason: To maintain public infrastructure and/or services.)

60. SITE REQUIREMENTS DURING DEMOLITION AND CONSTRUCTION (DW)

All of the following are to be satisfied/complied with during demolition, construction and any other site works:

- i) All demolition is to be carried out in accordance with Australian Standards AS 2601-2001.
- ii) Demolition must be carried out by a registered demolition contractor.
- iii) A single entrance is permitted to service the site for demolition and construction. The footway and nature strip at the service entrance must be planked out with close boarded, hardwood timber footpath protection pads. The pad shall cover the entire width of the footpath opening for the full width of the fence.
- iv) No blasting is to be carried out at any time during construction of the building.
- v) Care must be taken during demolition/ excavation/ building/ construction to prevent any damage to adjoining buildings.
- vi) Adjoining owner property rights and the need for owner's permission must be observed at all times, including the entering onto land for the purpose of undertaking works.
- vii) Any demolition and excess construction materials are to be recycled wherever practicable.
- viii) The disposal of construction and demolition waste must be in accordance with the requirements of the Protection of the Environment Operations Act 1997.
- ix) All waste on the site is to be stored, handled and disposed of in such a manner as to not create air pollution (including odour), offensive noise or pollution of land and/or water as defined by the Protection of the Environment Operations Act 1997.
- x) All excavated material should be removed from the site in the approved manner and be disposed of lawfully to a tip or other authorised disposal area.
- xi) Section 143 of the Protection of the Environment Operations Act 1997 requires waste to be transported to a place which can lawfully accept it. All non-recyclable demolition materials

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- are to be disposed of at an approved waste disposal depot in accordance with legislation.
- xii) All materials on site or being delivered to the site are to generally be contained within the site. The requirements of the Protection of the Environment Operations Act 1997 must be complied with when placing/stockpiling loose material, disposing of concrete waste, or other activities likely to pollute drains or water courses.
 - xiii) Details as to the method and location of disposal of demolition materials (weight docket, receipts etc.) should be kept on site as evidence of approved methods of disposal and recycling.
 - xiv) Any materials stored on site must be stored out of view or in such a manner so as not to cause unsightliness when viewed from nearby lands or roadways.
 - xv) Public footways and roadways adjacent to the site must be fully maintained and cleared of obstructions during construction unless prior separate approval from Council is obtained including payment of relevant fees.
 - xvi) Building operations such as brick cutting, washing tools or paint brushes, and mixing mortar not be performed on the roadway or public footway or any other locations which could lead to the discharge of materials into the stormwater drainage system.
 - xvii) All site waters during excavation and construction must be contained on site in an approved manner to avoid pollutants entering into waterways or Council's stormwater drainage system.
 - xviii) Stamped plans, specifications, documentation and the consent shall be available on site at all times during construction.

(Reason: To ensure that demolition, building and any other site works are undertaken in accordance with relevant legislation and policy and in a manner which will be non-disruptive to the local area.)

61. SURVEY REPORT OF APPROVED LEVELS DURING AND POST CONSTRUCTION (DW)

A Survey Certificate to Australian Height Datum shall be prepared by a Registered Surveyor as follows:

- i) At the completion of the first structural floor level prior to the pouring of concrete indicating the level of that floor and the relationship of the building to the boundaries.
- ii) At the completed height of the building, prior to the placement of concrete in form work, or the laying of roofing materials.
- iii) At the completion of the development.

Progress certificates in response to points (1) through to (3) shall be provided to the Principal Certifying Authority at the time of carrying out relevant progress inspections. Under no circumstances is work allowed to proceed until such survey information is submitted to and approved by the Principal Certifying Authority.

(Reason: To ensure compliance with the approved plans.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF AN OCCUPATION CERTIFICATE (OC)**62. CAR PARKING - SURPLUS VEHICULAR CROSSINGS (OC)**

All surplus vehicular crossings and/or kerb laybacks must be removed and reinstated with concrete kerb and gutter or to the existing edging profile as specified by Council. The nature strip is to be restored and the footpath area reinstated. All of the above are to be restored to the satisfaction of Council's Engineer and at full cost to the applicant, prior to the issue of any Occupation Certificate.

(Reason: Maintenance of public infrastructure.)

DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)

63. ENGINEERING WORKS (CERTIFICATION OC)

Prior to occupation of the premises, a Work As Executed (WAE) Plan of all engineering and/or drainage works is to be submitted to the Principal Certifying Authority. The WAE Plan is to be certified by a suitably Qualified Engineer, with Corporate Membership standing in the Institution of Engineers Australia and registered on the National Professional Engineers Register (NPER) under the appropriate professional category, demonstrating that:

- i) the stormwater drainage system; and/or
- ii) the car parking arrangement and area; and/or
- iii) any related footpath crossing works; and/or
- iv) the proposed basement pump and well system; and/or
- v) the proposed driveway and layback; and/or
- vi) any other civil works

have been constructed in accordance with the approved plans and any relevant Standards and Council policies/specifications.

For major works, such as subdivisions, works within the road reserve (requiring separate S138 approval) and as where specified by Council, a Part 4A Certificate will be required. It is strongly recommended that an Engineer supervise all engineering related works.

Where Council is not the Principal Certifying Authority, an electronic copy of the above documents is to be provided to Council, prior to the issue of any Occupation Certificate.

(Reason: Asset management.)

64. FIRE SAFETY CERTIFICATION (OC)

A fire safety certificate shall be obtained in accordance with Part 9, Division 4 of the Environmental Planning and Assessment Regulation 2000, prior to the issue of any Occupation Certificate.

An fire safety certificate is a certificate issued by the owner of a building to the effect that each essential fire safety measure specified in the current fire safety schedule for the part of the building to which the certificate relates:

- i) has been assessed by a properly qualified person; and
- ii) was found, when it was assessed, to be capable of performing to at least the standard required by the current fire safety schedule for the building for which the certificate is issued.

An interim fire safety certificate must be provided before an interim occupation certificate can be used for a building under Clause 153(2) of the Environmental Planning & Assessment Regulation 2000.

A final fire safety certificate must be provided before an interim occupation certificate can be used for a building under Clause 153(1) of the Environmental Planning & Assessment Regulation 2000.

A copy of the fire safety certificate and fire safety schedule shall be:

- i) submitted to Strathfield Council;
- ii) submitted to the Commissioner of the New South Wales Fire Brigade; and
- iii) prominently displayed in the building.

(Reason: Fire safety and statutory requirement.)

DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)

65. LANDSCAPING - ARBORIST'S FOLLOW UP REPORT OF TREE/S TO BE RETAINED (OC)

As part of the on-going assessment of the tree/s to be retained on the site and road reserve, the consulting arborist engaged by the applicant is to assess their health and any impacts suffered by them as a result of the proposed approved development, prior to the issue of an Occupation Certificate. Findings are to be compiled in a detailed report and submitted to the Principle Certifying Authority, which documents the following:

- i) methods of excavation or construction used to carry out the works;
- ii) any damage sustained by the tree/s as a result of the works;
- iii) any subsequent remedial works required to be carried out by the consulting arborist as a result of the damage; and
- iv) any future or on-going remedial work required to be carried out to ensure the long term retention of the tree/s.

(Reason: To ensure the survival of trees to be retained.)

66. OCCUPATION OF BUILDING (OC)

A person must not commence occupation or use (or change of use where an existing building) of the whole or any part of a new building (within the meaning of section 109H (4) of the Act) unless an Interim Occupation Certificate or Final Occupation Certificate has been issued in relation to the building or part.

The Principal Certifying Authority is required to be satisfied, amongst other things, that:

- i) all required inspections (including each applicable mandatory critical stage inspection) have been carried out; and
- ii) any preconditions to the issue of the certificate required by a development consent have been met.

Note: New building includes an altered portion of, or an extension to, an existing building.

(Reason: Statutory requirement.)

67. STORMWATER - CERTIFICATION OF THE CONSTRUCTED DRAINAGE SYSTEM (OC)

The constructed stormwater system shall be certified by a suitably qualified person, in accordance with Council's Stormwater Management Code, prior to the issue of any Occupation Certificate.

(Reason: Adequate stormwater management.)

68. STORMWATER - COVENANT AND RESTRICTION AS TO USER FOR STORMWATER CONTROLLED SYSTEMS (OC)

Prior to the issue of any Occupation Certificate, the applicant shall register a Positive Covenant and a Restriction as to User under section 88E and or section 88B of the Conveyancing Act as appropriate in favour of Council, ensuring the ongoing retention, maintenance and operation of the stormwater facility (on-site detention, pump-out, charged lines, etc.).

(Reason: To ensure the on-site detention and/or pump system is maintained to an appropriate operational standard.)

CONDITIONS TO BE SATISFIED PRIOR TO THE ISSUE OF A SUBDIVISION CERTIFICATE (SC)

DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)

69. ENGINEERING WORKS - CERTIFICATION OF WORK AS EXECUTED (SC)

Prior to the issue of the Subdivision Certificate, a Work As Executed (WAE) plan of all engineering and/or drainage works is to be submitted to the Principal Certifying Authority. The WAE Plan is to be certified by a suitably Qualified Engineer, with Corporate Membership standing in the Institution of Engineers Australia and registered on the National Professional Engineers Register (NPER) under the appropriate professional category, demonstrating that all engineering works including the provision of services, drainage, driveways and earthworks have been constructed in accordance with the approved plans and any relevant Standards and Council policies/specifications.

Where Council is not the Principal Certifying Authority, an electronic copy of the above documents is to be provided to Council.

(Reason: To ensure adequate access and services have been provided for the new lots.)

CONDITIONS TO BE SATISFIED DURING ONGOING USE OF THE PREMISES (OU)

70. FIRE SAFETY ANNUAL STATEMENT (OU)

Pursuant to Part 9, Division 5 of the Environmental Planning and Assessment Regulation (as amended) the owner of the building shall provide to Council an Annual Fire Safety Statement from an appropriately qualified person certifying the essential fire safety measures in the building. The Annual Fire Safety Statement shall be submitted within 12 months of the issue of the fire safety certificate, and then on an annual basis.

A copy of the Fire Safety Statement obtained and Fire Safety Schedule shall also be:

- i) Forwarded to the Commissioner of the New South Wales Fire Brigade; and
- ii) Prominently displayed in the building.

(Reason: Fire safety)

71. BOARDING HOUSES - OPERATIONAL PLAN OF MANAGEMENT (OU)

An Operational Plan of Management is to be formulated and submitted to Council for approval, prior to the issue of any Occupation Certificate.

The Plan of Management must include the minimum criteria as stipulated in Schedule 2 (Standards for Places of Shared Accommodation) of the Local Government (General) Regulation, 2005 under the Local Government Act 1993, the Public Health Act, 1991, Boarding Houses Act 2012 and Boarding Houses Regulation 2013.

The plan shall be dated and the name and signature of the author of the document shall be included. The Plan of Management shall be displayed in prominent locations within the premises and a copy of the Plan shall be provided to all residents of the boarding house. The on-site Caretaker shall be responsible for ensuring that the terms of the Plan are adhered to at all times.

This Operational Plan of Management must incorporate the following:

- i) All the measures to be implemented on the premises in terms of safety & security, amenity and health including but not limited to matters relating to curfews and after-hours access, bringing of visitors on to the site, drug and alcohol policy (including smoking), and use of communal areas etc.
- ii) All the responsibilities of the Caretaker/Manager.
- iii) An Incident Register to be maintained by the Caretaker. This Register must be produced

**DA2018/046 - 251-253 Homebush Road, Strathfield South
Lot 38 & 39 DP 2043 (Cont'd)**

upon demand by any Council officer or NSW Police Officer. The Register must contain a direction that all incidents of a criminal nature are to be reported to the Police immediately.

- iv) Prior to commencement of the boarding house use, a notification letter shall be forwarded to Council, the Police and neighbours providing contact details for the Caretaker/Manager so that any issues regarding the operation of the premises etc. can be addressed promptly. Evidence of this letter being forwarded as required in this condition shall be provided to the Principal Certifying Authority, prior to the issue of any Occupation Certificate and commencement of the use.

The Plan of Management referred to in this condition shall be reviewed by Council annually from the date of issue of an Occupation Certificate by the Principal Certifying Authority. The operator of the boarding house shall contact Council annually to carry out this review and the review shall include the Incident Register referred to in point (iii) above. Should amendments be made to the Plan of Management an updated version of same shall be provided to Council and the residents of the boarding house within one (1) month of the annual review being completed.

(Reason: To minimise the impact of the use on surrounding residences.)

72. BOARDING HOUSE - ONGOING USE OF (OU)

Ongoing operation of the approved use for the purpose of a boarding house shall comply with the following:

- i) Use of the premises shall remain consistent with the definition of 'boarding house' as defined under Strathfield Local Environmental Plan 2012 as a building that:
- is wholly or partly let in lodgings, and
 - provides lodgers with a principal place of residence for 3 months or more, and
 - may have shared facilities, such as a communal living room, bathroom, kitchen or laundry, and
 - has rooms, some or all of which may have private kitchen and bathroom facilities, that accommodate one or more lodgers but does not include backpackers' accommodation, a group home, a serviced apartment, seniors housing or hotel or motel accommodation.
- ii) Subdivision of the approved boarding house under strata or community title is prohibited.
- iii) The use and operation of the boarding house shall comply with all applicable legislation/regulation and standards, including:
- Boarding Houses Act 2012;
 - Boarding Houses Regulation 2013;
 - Local Government Act 1993; and
 - Schedule 2 (Standards for Places of Shared Accommodation) of the Local Government (General) Regulation 2005.
- iv) The maximum number of persons accommodated in a bedroom, or in a cubicle of a dormitory, must not exceed the number determined by allowing a minimum floor area within the bedroom or cubicle in accordance with the relevant provisions under the Public Health Act 1991 for each person.
- v) No speakers or music is permitted outdoors or in the communal areas

(Reason: To minimise the impact of the boarding house on surrounding residences.)

ATTACHMENTS

1. [↓](#) Architectural Plans

PROPOSED AFFORDABLE HOUSING
 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH


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PERSPECTIVE ALONG HOMEBUSH ROAD

<p>2 REVISED DA ISSUE 08.07.18 RW JS 1 DA ISSUE 08.04.2018 YS JS</p>	<p>ADG architects</p> <p>Suite 3.04, Level 3, 507-509 Marris St, Goswell NSW 2250 Postal Address P.O. BOX 437 Goswell NSW 2250 tel: 02 2512 5153 fax: 02 2512 5112 info@adgarchitects.com.au</p>	 <p>Member Australian Institute of Architects</p>	<p>Project PROPOSED AFFORDABLE HOUSING</p> <p>Client HONG AND HA PHAM</p>	<p>Drawing COVER PAGE</p> <p>Location 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH (LOT 38 DP 2043 & LOT 39 DP 2043)</p>	<p><small>Do not scale drawings. Use stated dimensions only. Check and verify levels and dimensions on site prior to commencement of any work. The liability of any drawings is the liability of the drafter.</small></p> <p><small>This drawing is the copyright of ADG Architects and is prepared under the Copyright Act 1968. It may not be altered, reproduced or transmitted in any form or by any means without the express permission of ADG Architects.</small></p> <p>FOR DA</p>	<p>Project Number 17_021</p> <p>Scale As indicated @ A3</p>	<p>Drawing Number DA01</p> <p>Date Printed 20/07/2018 10:00:39 AM</p> <p>Issue 2</p>
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SITE CONSTRAINTS & DEVELOPMENT INFORMATION:

INFORMATION OBTAINED FROM STRATHFIELD COUNCIL & AFFORDABLE RENTAL HOUSING SEPP 2009. BUILDING SETBACKS BASED ON PRE-DA MEETING

DESIGN CRITERIA	ALLOWABLE	PROPOSED DEVELOPMENT
SITE ZONING:	B4 (MIXED-USE)	
SITE AREA:	901.7m ²	
FSR:	1.4+ 0.5 (BONUS 0.5 AHSEPP)	1.21:1
TOTAL GFA	1713m ² (PERMISSIBLE)	1,093m ²
MAX HEIGHT	11m	COMPLIES
LANDSCAPE	30% OF SITE AREA (270.5m ²)	256m ² (28%)
TOTAL PARKING SPACES (FROM ARHSEPP 2009 -)	18 CAR SPACES (INCLUDING 1 FOR MANAGER) 8 MOTORCYCLE SPACES 8 BICYCLE SPACES	17 SPACES (INCLUDING 2 ACC.) 9 SPACES 12 SPACES

SETBACKS OBTAINED FROM PRE-DA MEETING:

FRONT SETBACK	3.5m TO 249 HOMEBUSH ROAD 5.2m TO 255 HOMEBUSH ROAD	3.5m 5.2m
SIDE SETBACK	4m FACING 249 HOMEBUSH ROAD 6m FACING 255 HOMEBUSH ROAD	minor part exceeding minor part exceeding
REAR SETBACK	6m	minor part exceeding

Screening devices have been used to alleviate privacy concerns and increase amenity to residents

MINIMUM ROOM SIZE: 12m² SINGLE (EXCLUDING ENS & KITCHENETTE)
16m² DOUBLE (EXCLUDING ENS & KITCHENETTE)
ALL ROOMS TO INCLUDE A FRONT LOADER WASHING MACHINE

ROOMS:

GROUND FLOOR:	LEVEL 1:	LEVEL 2:
9 ROOMS (1 SINGLE ROOM + 8 DOUBLE ROOMS, INCLUDES 1 MANAGER ROOM (ACC.) AND 1 ACC. ROOM)	13 ROOMS (3 SINGLE ROOMS, 10 DOUBLE ROOMS, INCLUDES 2 ACC.ROOM)	13 ROOMS (3 SINGLE ROOMS, 10 DOUBLE ROOMS, INCLUDES 2 ACC.ROOM)

TOTAL: 36 ROOMS (MANAGER ROOM IS INCLUDED)
6 ACCESSIBLE ROOMS (ACCESSIBLE = ACC.)
MAX. CAPACITY : 64 OCCUPANTS

GROSS FLOOR AREA (EXCLUDES VERTICAL CIRCULATION AND BALCONY)

GF:	351m ²
L1:	371m ²
L2:	371m ²
TOTAL:	1,093m ²

SCHEDULE OF ROOMS

NUMBER	AREA	ACCESSIBLE ROOMS
GROUND FLOOR PLAN		
1	18.1 m ²	
2	17.3 m ²	
3	17.3 m ²	
4	16.8 m ²	
5	20.7 m ²	ACCESSIBLE
6	13.8 m ²	
7	17.3 m ²	
8	18.3 m ²	
9	18.3 m ²	
MANAGER	20.5 m ²	ACCESSIBLE
GROUND FLOOR PLAN: 10	178.5 m ²	
LEVEL 01 FLOOR PLAN		
10	22.8 m ²	ACCESSIBLE
11	17.3 m ²	
12	17.3 m ²	
13	20.7 m ²	ADAPTABLE
14	16.5 m ²	
15	17.3 m ²	
16	19.2 m ²	
17	13.8 m ²	
18	13.8 m ²	
19	13.8 m ²	
20	17.4 m ²	
21	18.3 m ²	
22	18.3 m ²	
LEVEL 01 FLOOR PLAN: 13	226.6 m ²	
LEVEL 2 FLOOR PLAN		
23	22.8 m ²	ADAPTABLE
24	17.3 m ²	
25	17.3 m ²	
26	20.7 m ²	ADAPTABLE
27	16.5 m ²	
28	17.3 m ²	
29	19.2 m ²	
30	13.8 m ²	
31	13.8 m ²	
32	13.8 m ²	
33	17.4 m ²	
34	18.3 m ²	
35	18.3 m ²	
LEVEL 2 FLOOR PLAN: 13	226.6 m ²	
TOTAL: 36	631.6 m ²	

AREA EXCLUDES KITCHENETTE AND BATHROOM

COMMUNAL	45.2 m ²
REC.	3.3 m ²
TOTAL	48.5 m ²



SITE LOCATION

DRAWING LIST

- DA01 COVER PAGE
- DA01-A CONCEPT DATA
- DA02 SITE PLAN
- DA03 SITE ANALYSIS
- DA04 BASEMENT 1 FLOOR PLAN
- DA05 GROUND FLOOR PLAN
- DA06 LEVEL 1 FLOOR PLAN
- DA07 LEVEL 2 FLOOR PLAN
- DA08 ROOF PLAN
- DA10 NORTH AND SOUTH ELEVATIONS
- DA11 EAST AND WEST ELEVATIONS
- DA12 SECTIONS
- DA13 SHADOW DIAGRAMS
- DA14 SHADOW DIAGRAMS - ELEVATION
- DA15 11M HEIGHT PLANE

MATERIAL SCHEDULE



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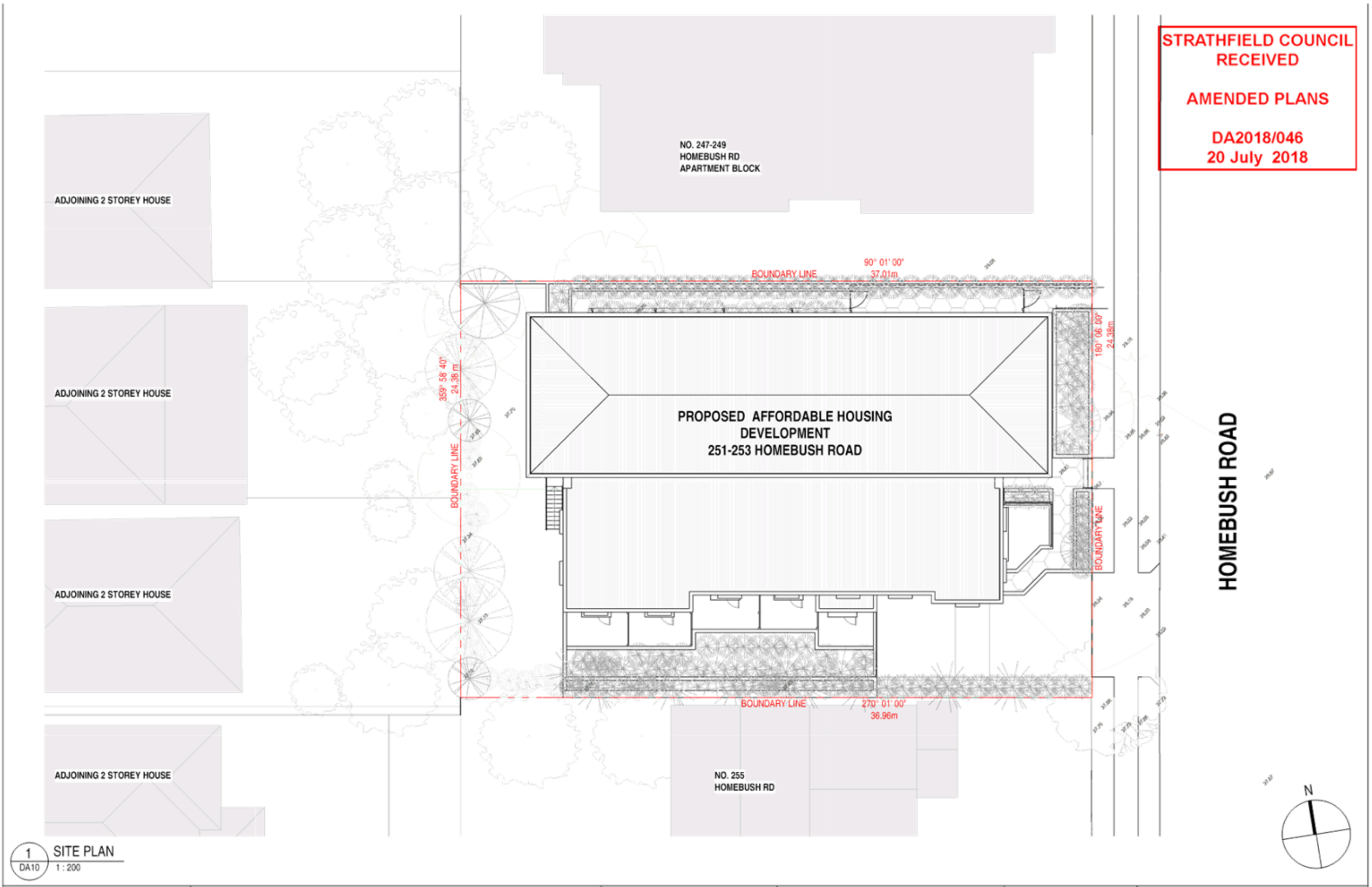
**DA2018/046
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<p>2 REVISED DA ISSUE 06.07.18 RW JB</p> <p>1 DA ISSUE 08.04.2018 YS JS</p>	<p>ADG architects</p> <p><small>Suite 3.04, Level 3, 505-506 Mann St, Gosford NSW 2250 Postal Address P.O. BOX 437 Gosford NSW 2250 tel: 02 2512 5153 fax: 02 2512 5112 a.r@adgarchitects.com.au</small></p>	<p>Member Australian Institute of Architects</p>	<p>Project PROPOSED AFFORDABLE HOUSING</p> <p>Client HONG AND HA PHAM</p>	<p>Drawing CONCEPT DATA</p> <p>Location 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH (LOT 38 DP 2043 & LOT 39 DP 2043)</p>	<p>Do not scale drawings. Use stated dimensions only. Check and verify levels and dimensions on site prior to commencement of any work. The liability of any drawings is the liability of the drafter.</p> <p>This drawing is the copyright of ADG Architects and is prepared under the Copyright Act 1968. It may not be altered, reproduced or transmitted in any form or by any means without the express permission of ADG Architects.</p> <p>FOR DA</p>	<p>Project Number 17_021</p> <p>Scale As indicated @ A3</p> <p>File Name C:\Users\jw@adg\Documents\17_021_251-253 HOMEBUSH</p>	<p>Drawing Number DA01-A</p> <p>Date Printed 20/07/2018 10:00:42 AM</p> <p>Issue 2</p>
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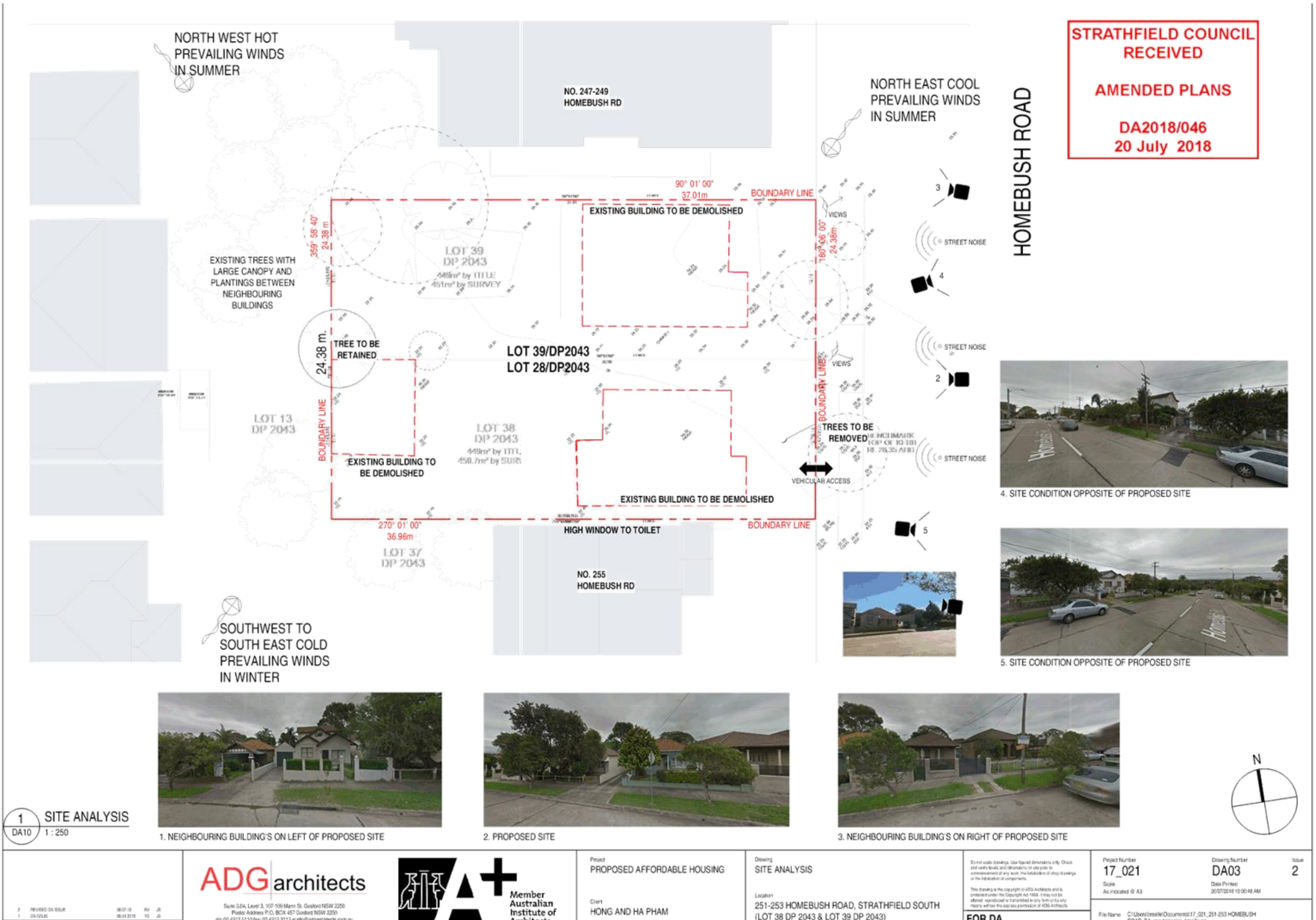
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1 SITE PLAN
DA10 1 : 200

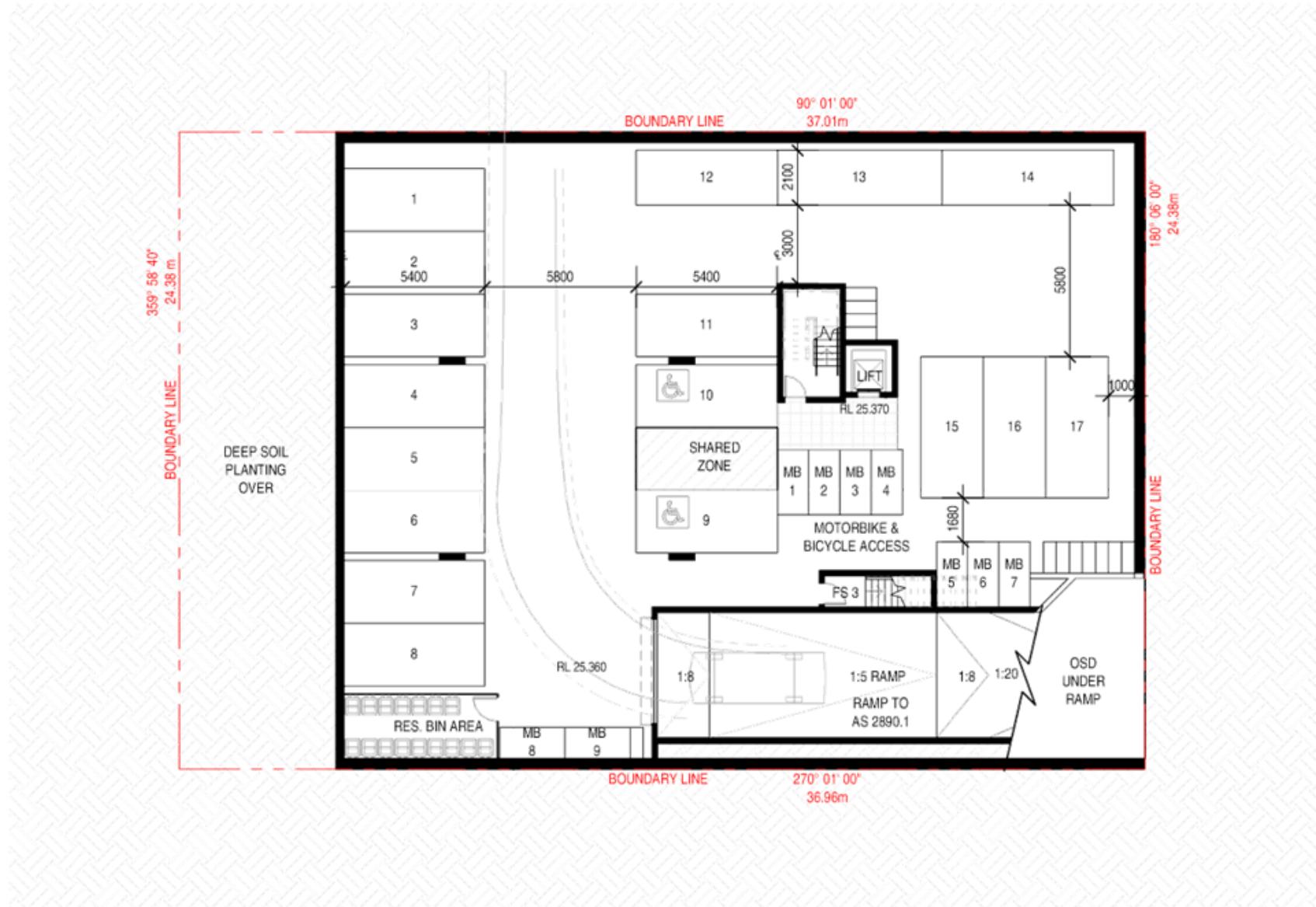
<p>2 REVISED DA ISSUE 06.07.18 RV JS</p> <p>1 ORIGIN 08.04.2018 YS JS</p>	<p>ADG architects</p> <p>Suite 3.04, Level 3, 107-109 Mares St, Gosford NSW 2250 Postal Address P.O. BOX 457 Gosford NSW 2250 tel: 02 2512 5153 fax: 02 2512 5113 info@adgarchitects.com.au</p>	 <p>Member Australian Institute of Architects</p>	<p>Project PROPOSED AFFORDABLE HOUSING</p> <p>Client HONG AND HA PHAM</p>	<p>Drawing SITE PLAN</p> <p>Location 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH (LOT 38 DP 2043 & LOT 39 DP 2043)</p>	<p>Do not scale drawings. Use stated dimensions only. Check and verify levels and dimensions on site prior to commencement of any work. The liability of any drawings is the fabrication of components.</p> <p>This drawing is the copyright of ADG Architects and is prepared under the Copyright Act 1968. It may not be altered, reproduced or transmitted in any form or by any means without the express permission of ADG Architects.</p> <p>FOR DA</p>	<p>Project Number 17_021</p> <p>Scale 1 : 200 @ A3</p>	<p>Drawing Number DA02</p> <p>Issue 2</p> <p>Date Printed 20/07/2018 10:00:45 AM</p> <p>File Name C:\Users\jw@adg\Documents\17_021_251-253 HOMEBUSH</p>
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20 July 2018**



1 BASEMENT 1
DA12 1:200

REFER TO STORMWATER ENGR.
DRAWINGS FOR STORMWATER DETAIL

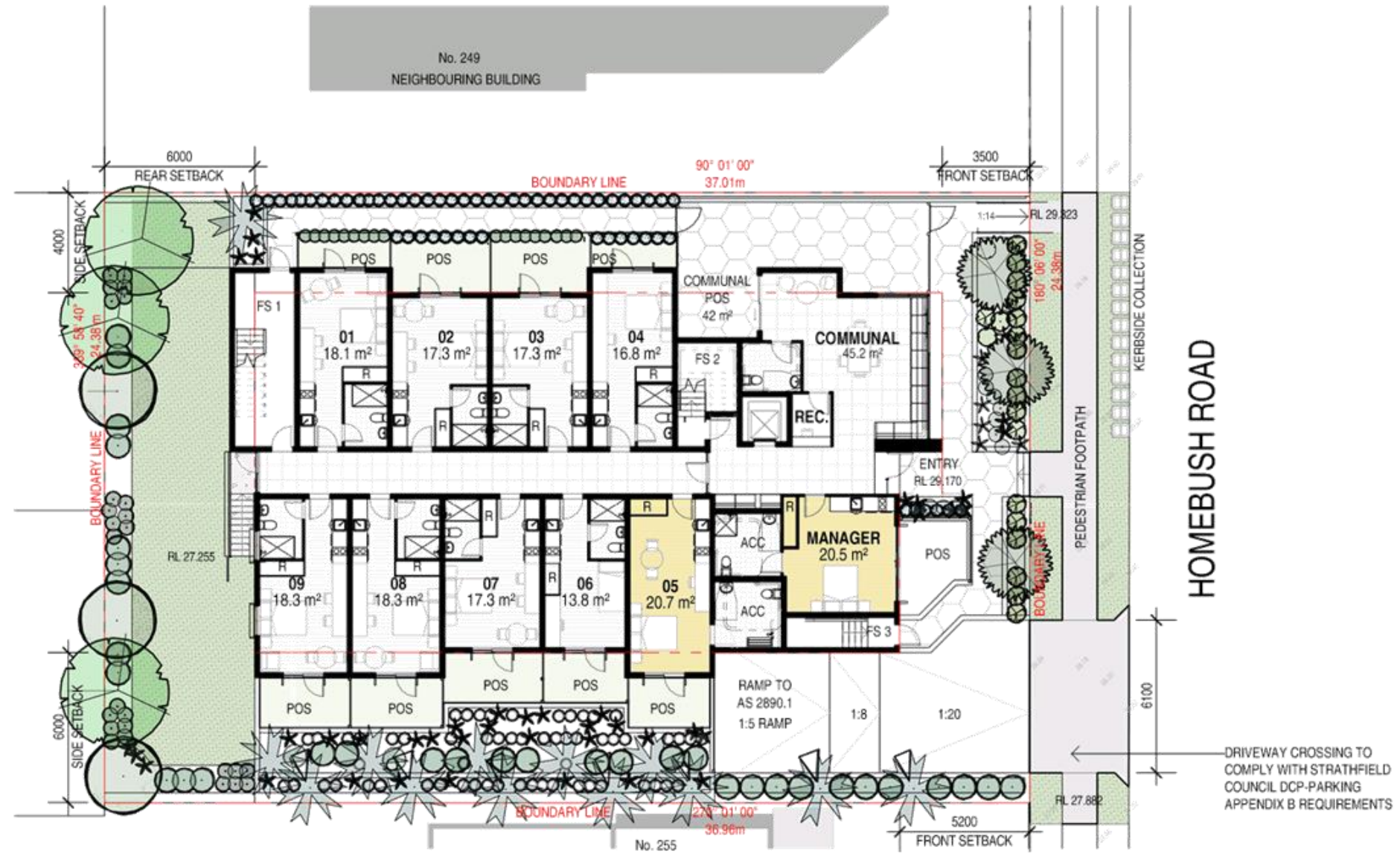


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20 July 2018**



1 GROUND FLOOR PLAN
DA10 1:200

R - ROBE
REFER TO STORMWATER ENGR.
DRAWINGS FOR STORMWATER DETAIL
REFER TO LANDSCAPE ARCHITECTS
DRAWINGS FOR LANDSCAPE DETAILS



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20 July 2018**

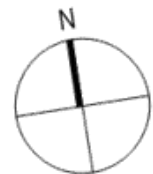


1 LEVEL 01 FLOOR PLAN
DA10 1:200

R - ROBE

REFER TO STORMWATER ENGR.
DRAWINGS FOR STORMWATER DETAIL

REFER TO LANDSCAPE ARCHITECTS
DRAWINGS FOR LANDSCAPE DETAILS



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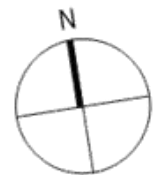
AMENDED PLANS

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20 July 2018**



1 LEVEL 2 FLOOR PLAN
DA10 1:200

R - ROBE
REFER TO STORMWATER ENGR.
DRAWINGS FOR STORMWATER DETAIL
REFER TO LANDSCAPE ARCHITECTS
DRAWINGS FOR LANDSCAPE DETAILS

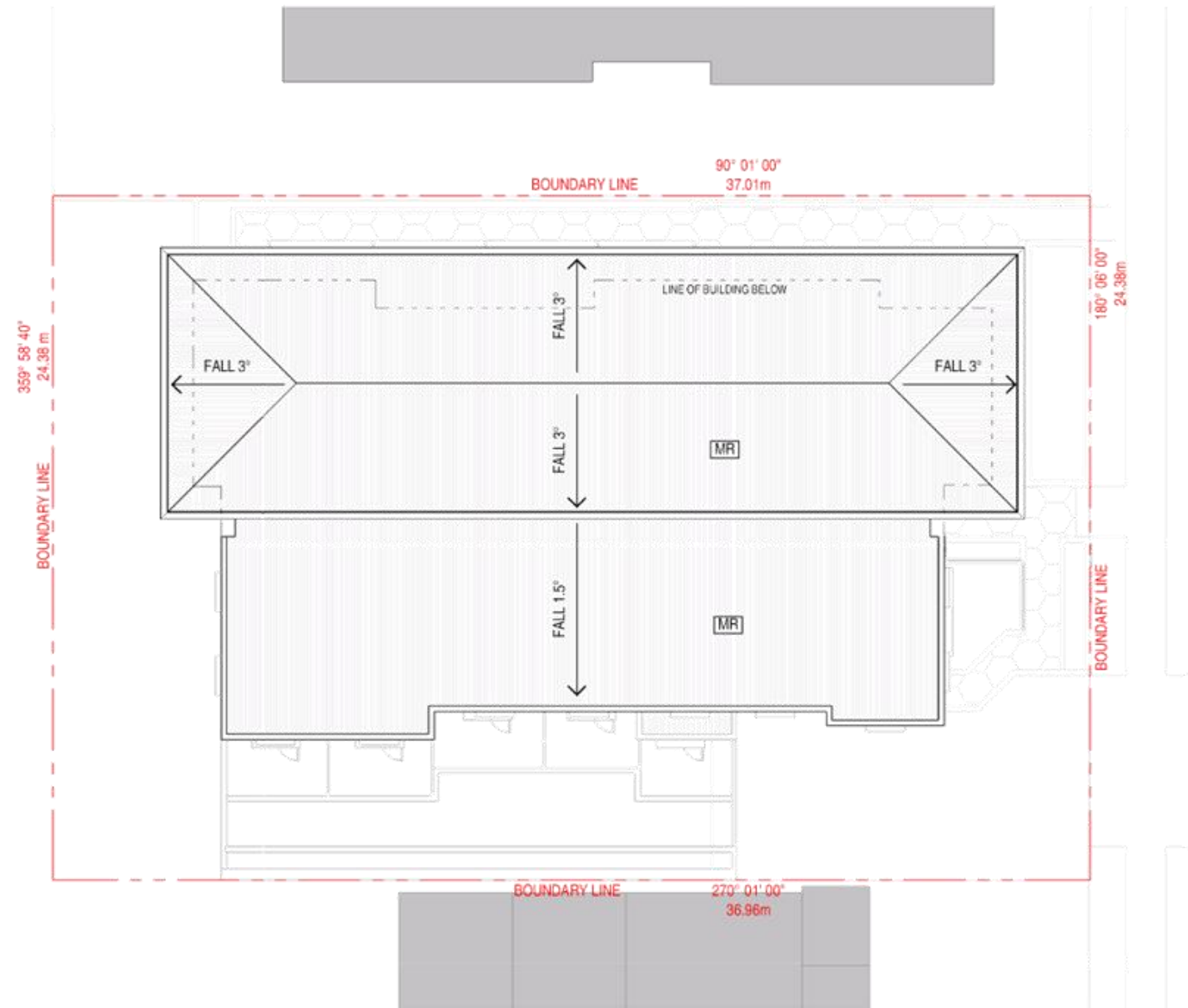


<p>2 REVISED DA ISSUE 08.07.18 RW JS 1 DA ISSUE 08.04.2018 YS JS</p>	<p>ADG architects Suite 3.04, Level 3, 107-109 Macquarie Street, Gosford NSW 2250 Postal Address P.O. BOX 437 Gosford NSW 2250 tel: 02 2512 5153 fax: 02 2512 5172 a.h@adgarchitects.com.au</p>	<p>Member Australian Institute of Architects</p>	<p>Project: PROPOSED AFFORDABLE HOUSING Client: HONG AND HA PHAM</p>	<p>Drawing: LEVEL 2 FLOOR PLAN Location: 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH (LOT 38 DP 2043 & LOT 39 DP 2043)</p>	<p><small>Do not scale drawings. Use stated dimensions only. Check and verify levels and dimensions on site prior to commencement of any work. The liability of shop drawings is the fabrication of components. This drawing is the copyright of ADG Architects and is prepared under the Copyright Act 1968. It may not be altered, reproduced or transmitted in any form or by any means without the express permission of ADG Architects.</small></p> <p>FOR DA</p>	<p>Project Number: 17_021 Scale: 1:200 @ A3</p>	<p>Drawing Number: DA07 Issue: 2 Date Printed: 20/07/2018 10:00:57 AM File Name: C:\Users\jw@adg\Documents\17_021_251-253 HOMEBUSH</p>
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20 July 2018**



R - ROBE

REFER TO STORMWATER ENGR.
DRAWINGS FOR STORMWATER DETAIL

REFER TO LANDSCAPE ARCHITECTS
DRAWINGS FOR LANDSCAPE DETAILS



1 ROOF PLAN
DA10 1 : 200

2	REVISED DA ISSUE	06.07.18	RJ	JS
1	DA ISSUE	05.04.2015	YS	JS

ADG architects

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tel: 02 2512 5153 fax: 02 2512 5112 info@adgarchitects.com.au



Project
PROPOSED AFFORDABLE HOUSING

Client
HONG AND HA PHAM

Drawing
ROOF PLAN

Location
251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH
(LOT 38 DP 2043 & LOT 39 DP 2043)

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FOR DA

Project Number 17_021	Drawing Number DA08	Issue 2
Scale 1 : 200 @ A3	Date Printed 20/07/2018 10:01:33 AM	
File Name C:\Users\robert\Documents\17_021_251-253 HOMEBUSH		


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1 NORTH ELEVATION
1 : 200



2 SOUTH ELEVATION
1 : 200

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20 July 2018**



1 EAST ELEVATION
1 : 200



2 WEST ELEVATION
1 : 200

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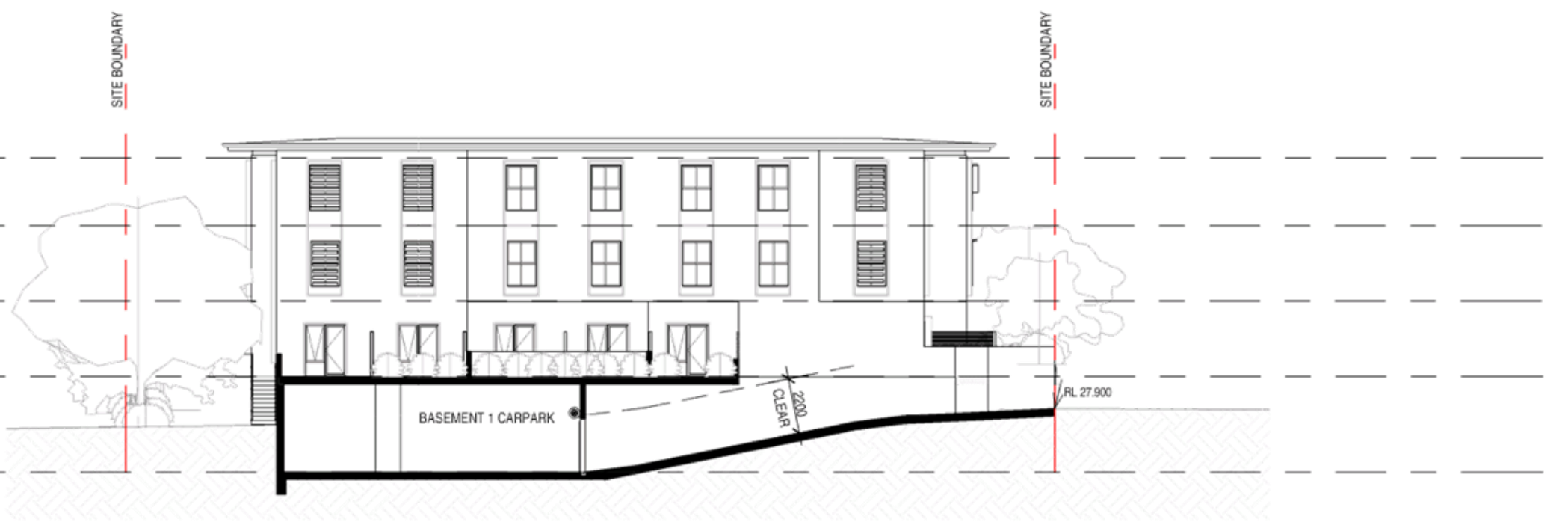
DA2018/046
20 July 2018

- ROOF PLAN
RL 37.86
- LEVEL 2 FLOOR PLAN
RL 35.16
- LEVEL 01 FLOOR PLAN
RL 32.16
- GROUND FLOOR PLAN
RL 29.16
- BASEMENT 1
RL 25.36



1 SECTION 1
1 : 200

- ROOF PLAN
RL 37.86
- LEVEL 2 FLOOR PLAN
RL 35.16
- LEVEL 01 FLOOR PLAN
RL 32.16
- GROUND FLOOR PLAN
RL 29.16
- BASEMENT 1
RL 25.36



2 SECTION 2
1 : 200

<p>2 REVISED DA ISSUE 06.07.18 RV JS</p> <p>1 DA ISSUE 05.04.2018 YS JS</p>	<p>ADG architects</p> <p>Suite 3.04, Level 3, 507-509 Marris St, Gosford NSW 2250 Postal Address P.O. BOX 457 Gosford NSW 2250 tel: 02 2512 5153 fax: 02 2512 5113 info@adgarchitects.com.au</p>	<p>Member Australian Institute of Architects</p>	<p>Project PROPOSED AFFORDABLE HOUSING</p> <p>Client HONG AND HA PHAM</p>	<p>Drawing SECTIONS</p> <p>Location 251-253 HOMEBUSH ROAD, STRATHFIELD SOUTH (LOT 38 DP 2043 & LOT 39 DP 2043)</p>	<p><small>Do not scale drawings. Use stated dimensions only. Check and verify levels and dimensions on site prior to commencement of any work. The liability of any drawings is the fabrication of components.</small></p> <p><small>This drawing is the copyright of ADG Architects and is produced under the Copyright Act 1968. It may not be altered, reproduced or transmitted in any form or by any means without the express permission of ADG Architects.</small></p> <p>FOR DA</p>	<p>Project Number 17_021</p> <p>Scale 1 : 200 @ A3</p>	<p>Drawing Number DA12</p> <p>Issue 2</p> <p>Date Printed 20/07/2018 10:01:35 AM</p> <p>File Name C:\Users\jw@adg\Documents\17_021_251-253 HOMEBUSH</p>
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1 ELEVATIONAL SHADOW DIAGRAM 9AM
1 : 300



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**DA2018/046
20 July 2018**

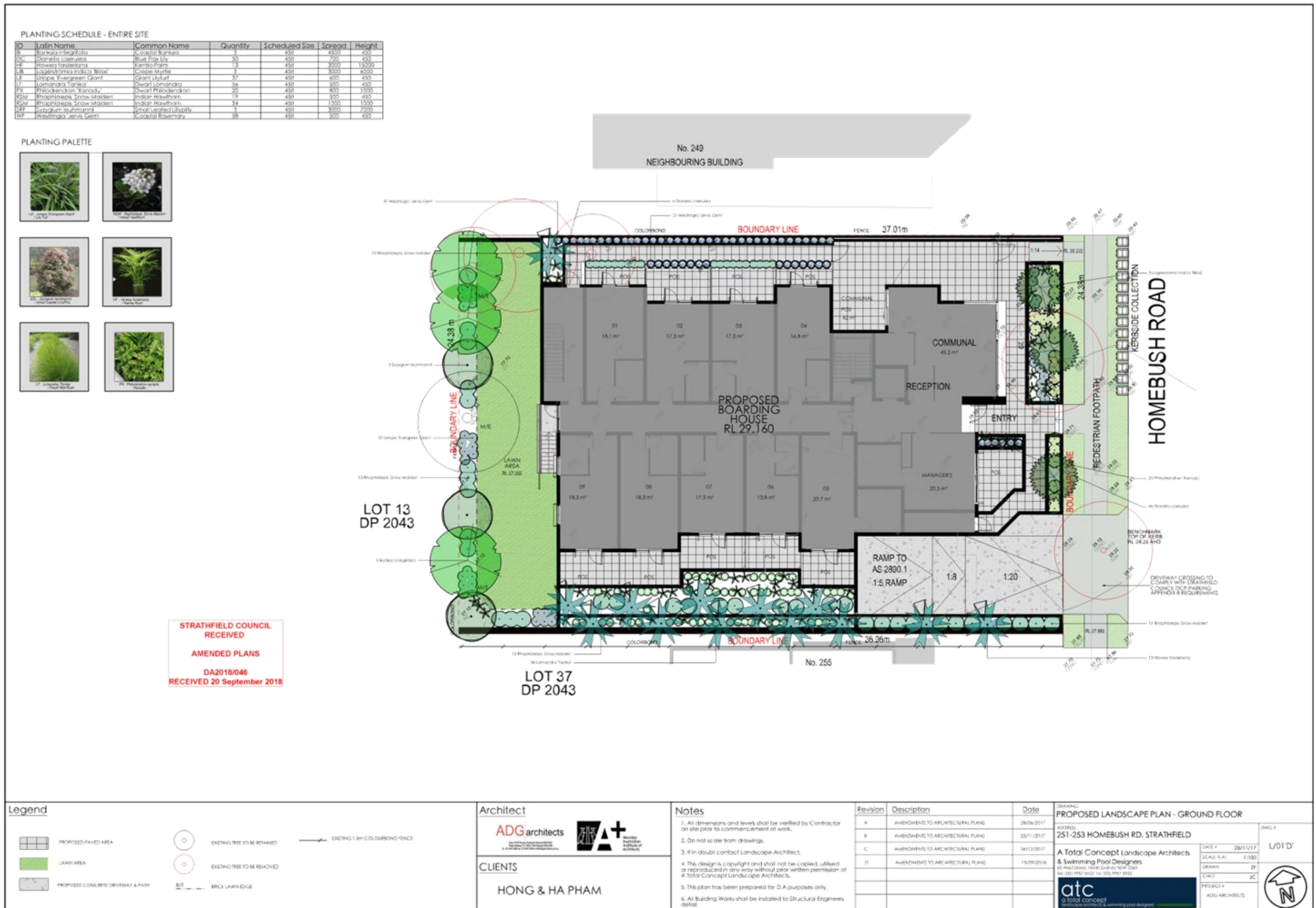
2 ELEVATIONAL SHADOW DIAGRAM 12PM
1 : 300



3 ELEVATIONAL SHADOW DIAGRAM 3PM
1 : 300



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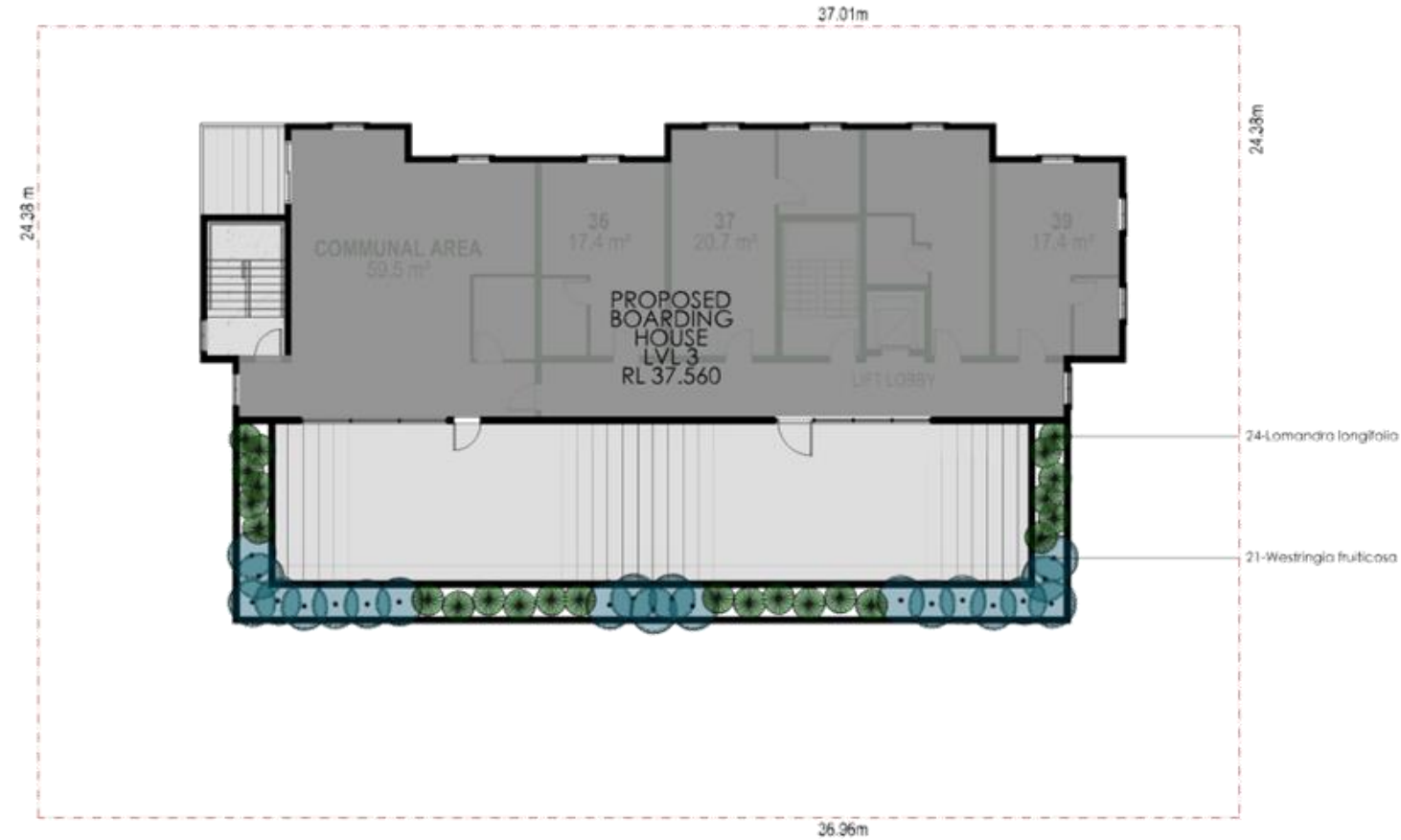


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PLANTING SCHEDULE - ENTIRE SITE

ID	Latin Name	Common Name	Quantity	Scheduled Size	Spread	Height
B	Coratola integrifolia	Coastal Banksia	2	450	450	450
DC	Coratola caespitosa	Blue Fire Lily	14	450	700	450
HF	Hoveia laetevirens	Kemira Palm	7	450	3000	15000
LB	Lobelia indica	Cape Matte	3	450	3000	4000
LE	Liriodie Evergreen Giant	Giant Lilylily	33	450	600	450
LT	Lomandra laevicaulis	Dwarf Lomandra	23	450	500	450
LI	Lomandra longifolia	Rescue Grass	34	450	1000	400
PK	Phacelia grandiflora	Dwarf Phacelia	18	450	800	1000
SA	Phoradendron laevicaulis	Indian Hawthorn	23	450	1000	1000
SA	Phoradendron laevicaulis	Indian Hawthorn	18	450	500	450
SP	Phoradendron laevicaulis	Indian Hawthorn	3	450	3000	2000
WF	Westringia fruticosa	Coastal Rosemary	25	450	500	450
WF	Westringia fruticosa	Coastal Rosemary	21	450	1500	1500

PLANTING PALETTE



Legend



Architect
ADG architects
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CLIENTS
HONG & HA PHAM

- Notes**
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 - Do not scale from drawings.
 - If in doubt contact Landscape Architect.
 - This design is copyright and shall not be copied, utilized or reproduced in any way without prior written permission of A Total Concept Landscape Architects.
 - This plan has been prepared for D.A purposes only.
 - All Building Works shall be installed to Structural Engineers detail.

Revision	Description	Date
A	AMENDMENTS TO ARCHITECTURAL PLAN	28/06/2017
B	AMENDMENTS TO ARCHITECTURAL PLAN	03/11/2017

DRAWING
PROPOSED LANDSCAPE PLAN - LEVEL 3

ADDRESS
251-253 HOMEBUSH RD, STRATHFIELD

A Total Concept Landscape Architects & Swimming Pool Designers
45 West Street, Homebush NSW 1585
Tel: (002) 9557 9222 Fax: (002) 9557 9922

DATE: 28/11/17
SCALE: E.A.I: 1:100
DRAWN: JF
CHECK: JC
PROJECT: #
AUG ARCHITECTS

DWG # L/01 B